

Tuesday, April 8, 2025, 5:00 p.m.

Council Chambers

3rd Floor City Hall, 2580 Shaughnessy Street, Port Coquitlam, BC

Pages

1. CALL TO ORDER

2. ADOPTION OF THE AGENDA

2.1 Adoption of the Agenda

Recommendation:

That the Tuesday, April 8, 2025, Council Meeting Agenda be adopted as circulated.

3. CONFIRMATION OF MINUTES

3.1 Minutes of Council Meetings

3

Recommendation:

That the minutes of the following Council Meetings be adopted:

- *March 11, 2025.*

4. PROCLAMATIONS

None.

5. DELEGATIONS

None.

6. PUBLIC HEARINGS

None.

7. BYLAWS

7.1 Zoning Amendment Bylaw (Minor & Housekeeping) - First Three Readings

8

Recommendation:

That "Zoning Bylaw, 2008, No. 3630, Amendment Bylaw, 2025, No. 4404" be given first three readings.

7.2 Zoning Amendment Bylaw for 1776 Jensen Avenue - First Three Readings 27

Recommendation:

That "Zoning Bylaw, 2008, No. 3630, Amendment Bylaw, 2025, No. 4405", be given first three readings.

7.3 Road Closure and Removal of Highway Dedication Bylaw for Prince Street Road Allowance - First Three Readings 35

Recommendation:

That "Road Closure and Removal of Highway Dedication Bylaw for Prince Street Road Allowance, 2025, No.4386", be given first three readings.

8. REPORTS

8.1 Utility Rate Relief - 2471 Gately Avenue 156

Recommendation:

That Council waive the Water utility and Sewer utility charges for 2471 Gately Avenue until the earlier of:

- 1. *December 21, 2025; or*
- 2. *Occupancy Permit Issuance by the City.*

8.2 Utility Rate Relief - 2442, 2444, 2446 Wilson Avenue 157

Recommendation:

That Council:

- 1. *waive the Water utility and Sewer Utility charges for units in 2442, 2444, and 2446 Wilson Avenue, which are uninhabitable due to a structure fire; and*
- 2. *that the term of the waiver be from January 1, 2025, until the existing buildings are demolished.*

9. NEW BUSINESS

10. OPEN QUESTION PERIOD

11. ADJOURNMENT

11.1 Adjournment of the Meeting

Recommendation:

That the Tuesday, April 8, 2025, Council Meeting be adjourned.



Council Minutes

Tuesday, March 11, 2025

Council Chambers

3rd Floor City Hall, 2580 Shaughnessy Street, Port Coquitlam, BC

Council Present: Chair - Mayor West
Councillor Darling
Councillor McCurrach
Councillor Penner
Councillor Petriw
Councillor Pollock

Absent: Councillor Washington

Staff Present: R. Bremner, CAO
K. Grommada, Deputy CAO
B. Clarkson, Fire Chief
C. Deakin, Corporate Officer
J. Frederick, Director Engineering & Public Works
B. Irvine, Director Development Services
D. Long, Director Community Safety & Corporate Services
J. Lovell, Director Finance
G. Mitzel, Director Recreation

1. CALL TO ORDER

The meeting was called to order at 5:00 p.m.

2. ADOPTION OF THE AGENDA

2.1 Adoption of the Agenda

Moved-Seconded:

That the Tuesday, March 11, 2025, Council Meeting Agenda be adopted as circulated.

In Favour (6): Mayor West, Councillor Darling, Councillor McCurrach, Councillor Penner, Councillor Petriw, and Councillor Pollock

Absent (1): Councillor Washington

Carried

3. CONFIRMATION OF MINUTES

3.1 Minutes of Council Meetings

Moved-Seconded:

That the minutes of the following Council Meetings be adopted:

- *January 28, 2025*
- *February 25, 2025.*

In Favour (6): Mayor West, Councillor Darling, Councillor McCurrach, Councillor Penner, Councillor Petriw, and Councillor Pollock

Absent (1): Councillor Washington

Carried

SPECIAL PRESENTATION

Member of Parliament, Ron McKinnon presented Mayor West with the King Charles III Coronation Medal and noted this award is a testament to Mayor West's service and unwavering dedication to local government.

4. PROCLAMATIONS

4.1 World Down Syndrome Day - March 21, 2025

Mayor West proclaimed March 21, 2025, as World Down Syndrome Day and introduced Mr. Feng, Board of Directors for the Lower Mainland Down Syndrome Society, to say a few words.

5. DELEGATIONS

None.

6. PUBLIC HEARINGS

None.

7. BYLAWS

7.1 Fire and Emergency Services Bylaw - Adoption

Moved-Seconded:

That "Fire and Emergency Services Bylaw, 2025, No. 4395", be adopted.

In Favour (6): Mayor West, Councillor Darling, Councillor McCurrach, Councillor Penner, Councillor Petriw, and Councillor Pollock

Absent (1): Councillor Washington

Carried

7.2 Bylaw Notice Enforcement Amendment Bylaw - Adoption

Moved-Seconded:

That "Bylaw Notice Enforcement Bylaw, 2013, No. 3814, Amendment Bylaw, 2025, No. 4398", be adopted.

In Favour (6): Mayor West, Councillor Darling, Councillor McCurrach, Councillor Penner, Councillor Petriw, and Councillor Pollock

Absent (1): Councillor Washington

Carried

7.3 Ticket Information Utilization Amendment Bylaw - Adoption

Moved-Seconded:

That "Ticket Information Utilization Bylaw, 1992, No. 2743, Amendment Bylaw, 2025, No. 4399", be adopted.

In Favour (6): Mayor West, Councillor Darling, Councillor McCurrach, Councillor Penner, Councillor Petriw, and Councillor Pollock

Absent (1): Councillor Washington

Carried

7.4 Fees and Charges Amendment Bylaw - Adoption

Moved-Seconded:

That "Fees and Charges Bylaw, 2024, No. 4390, Amendment Bylaw, 2025, No. 4400", be adopted.

In Favour (6): Mayor West, Councillor Darling, Councillor McCurrach, Councillor Penner, Councillor Petriw, and Councillor Pollock

Absent (1): Councillor Washington

Carried

7.5 Zoning Amendment Bylaw for 3630 and 3638 Westwood Street and 2803 Anson Avenue - Adoption

Moved-Seconded:

That “Zoning Bylaw, 2008, No. 3630, Amendment Bylaw, 2025, No. 4394”, be adopted for 3630 and 3638 Westwood Street and 2803 Anson Avenue.

In Favour (6): Mayor West, Councillor Darling, Councillor McCurrach, Councillor Penner, Councillor Petriw, and Councillor Pollock

Absent (1): Councillor Washington

Carried

8. REPORTS

8.1 Development Permit Amendment Application for 3638 Westwood Street and 2803 Anson Avenue

Moved-Seconded:

That Council approve an amendment to Development Permit DP000500, which will regulate the development of a 32-storey high-rise building and landscaping at 3638 Westwood Street and 2803 Anson Avenue.

In Favour (6): Mayor West, Councillor Darling, Councillor McCurrach, Councillor Penner, Councillor Petriw, and Councillor Pollock

Absent (1): Councillor Washington

Carried

8.2 Development Variance Permit for 2472 Chilcott Avenue - Further Information

Moved-Seconded:

That the Development Variance Permit for 2472 Chilcott Avenue be referred back to staff for further consultation with the neighbours relating to access for Lot 3 of the subdivision and then a further report be brought forward to Council after that consultation.

In Favour (2): Mayor West, and Councillor Darling

Opposed (4): Councillor McCurrach, Councillor Penner, Councillor Petriw, and Councillor Pollock

Absent (1): Councillor Washington

Defeated

8.3 Development Variance Permit for 2472 Chilcott Avenue - Issuance

Moved-Seconded:

That Development Variance Permit DVP00103 for 2472 Chilcott Avenue, be issued.

In Favour (5): Mayor West, Councillor McCurrach, Councillor Penner, Councillor Petriw, and Councillor Pollock

Opposed (1): Councillor Darling

Absent (1): Councillor Washington

Carried

9. NEW BUSINESS

Council provided updates related to community events.

10. OPEN QUESTION PERIOD

The floor was opened up to anyone in attendance and one member of the public posed questions to Council.

11. ADJOURNMENT

11.1 Adjournment of the Meeting

Moved-Seconded:

That the Tuesday, March 11, 2025, Council Meeting be adjourned. (6:05 p.m.)

In Favour (6): Mayor West, Councillor Darling, Councillor McCurrach, Councillor Penner, Councillor Petriw, and Councillor Pollock

Absent (1): Councillor Washington

Carried

Mayor

Corporate Officer

Zoning Amendment Bylaw (Minor & Housekeeping) - First Three Readings

RECOMMENDATION:

That “Zoning Bylaw, 2008, No. 3630, Amendment Bylaw, 2025, No. 4404” be given first three readings.

PREVIOUS COUNCIL/COMMITTEE ACTION

On April 1, 2025, Committee of Council recommended to Council that:

- 1. The Zoning Bylaw be amended as described in this report; and*
- 2. That the requirement for a Public Hearing be waived as the proposed amendments are consistent with the Official Community Plan.*

OPTIONS (✓ = Staff Recommendation)

	#	Description
<input checked="" type="checkbox"/>	1	Give first three readings to the Bylaw.
<input type="checkbox"/>	2	Defer first three readings of the Bylaw and request staff to provide further information (to be specified).
<input type="checkbox"/>	3	Decline first three readings of the Bylaw, which will retain the existing Bylaw.

Attachment 1 – Bylaw 4404

Attachment 2 – Report to Committee, April 1, 2025

CITY OF PORT COQUITLAM
ZONING BYLAW AMENDMENT BYLAW, 2025

Bylaw No. 4404

The Council of the Corporation of the City of Port Coquitlam enacts as follows:

1. CITATION

This Bylaw is cited as “Zoning Bylaw, 2008, No. 3630, Amendment Bylaw, 2025, No. 4404”.

2. ADMINISTRATION

2.1 Section I Definitions is amended as follows:

2.1.1 Replace the definition of Agriculture with the following:

Agriculture means a use defined as a farm use in the *Agricultural Land Commission Act*, whether in the Agricultural Land Reserve or not, excluding any cannabis production use unless this use takes place outdoors in a field or is located inside a structure that has a base consisting entirely of soil.

2.1.2 Insert the definition of City as follows:

City means the Corporation of the City of Port Coquitlam.

2.1.3 Insert the definition of Bylaw Enforcement Officer as follows:

Bylaw Enforcement Officer means any person employed by the City or appointed by City Council to enforce the City’s bylaws and includes members of the Royal Canadian Mounted Police.

2.1.4 Replace the definition of Boarding with the following:

Boarding means the use of a dwelling unit for the provision of rooming accommodation for a period of 90 consecutive days or more, with or without meals, to persons other than members of the family occupying the dwelling unit as a residence.

2.1.5 Replace the definition of Farm residence accessory facility with the following:

Farm residence accessory facility means an accessory building or structure such as a detached garage or carport, greenhouse, pergola, workshop, shed, or sunroom, or an improvement associated with a farm residence such as ornamental landscaping, an artificial pond, swimming pool, or tennis court.

2.1.6 Replace the definition of Impervious surface area with the following:

Impervious surface area means any hard-surfaced, man-made area that does not readily absorb or retain rainwater, including but not limited to roofs, driveways, parking spaces, patios, sidewalks, grouted pavers or those with joints filled with polymeric or poly sand, sport courts, ornamental pools, swimming pools or any other hard surface. For clarity, green roofs and driveways, parking spaces and patios constructed of gravel, ungrouted pavers less than 0.37m² in size, grasscrete or similar porous materials, do not constitute impervious surfaces.

2.1.7 Replace the definition of Rowhouse with the following:

Rowhouse means the residential use of a building containing four or more principal dwelling units each having a private entrance from the exterior of the building, a private outdoor space, and being separated from the other dwelling units by vertical walls only.

2.1.8 Replace the definition of Single residential with the following:

Single residential means the residential use of a building containing only one principal dwelling unit.

2.1.9 Replace the definition of Short-term rental accommodation with the following:

Short-term rental accommodation means the use of a principal residence for the provision of rooming accommodation for a period of less than 90 consecutive days in exchange for a fee, with or without meals, to persons other than members of the family occupying the dwelling unit as a residence.

2.1.10 Replace the definition of Townhouse with the following:

Townhouse means the residential use of a building containing multiple principal dwelling units each having a private entrance from the exterior of the building, private outdoor space, and sharing common walls.

2.1.11 Replace the definition of Triplex with the following:

Triplex means the residential use of a building containing three principal dwellings each having a private entrance from the exterior of the building and, if the building contains secondary suites, having each principal dwelling and associated secondary suite being separated from the other dwelling units by vertical walls only.

2.1.12 Insert the definition of Type 1 agricultural structure as follows:

Type 1 agricultural structure means the farm use of a building or structure or use as a farm education and research facility, but excludes a type 2 agricultural structure.

2.1.13 Insert the definition of Type 2 agricultural structure as follows:

Type 2 agricultural structure means the farm use of a building or structure for the keeping of swine or fur bearing animals or the production of cannabis.

2.2 Section II Zones and Zone Regulations is amended as follows:

2.2.1 Replace subsection 8 with the following:

Where a table in this Bylaw specifies a building height, no building or structure may be constructed that exceeds the height specified for the zone in which the building or structure is located, other than communications antennas and towers, flagpoles, elevator penthouses, stair towers, guardrails, and rooftop-mounted mechanical equipment that is visually screened.

2.3 Section II Zones and Zone Regulations, Part 1. Agricultural Zones is amended as follows:

2.3.1 Replace subsection 1.4 with the following:

1.4. REGULATIONS

Table 1.4: Agricultural Zones Regulation

Building or Structure	Building Height	Lot Coverage	Floor Area	Setback				
				Front	Interior Side	Exterior Side	Rear	Other
Farm residence	11 m	10%	500m ² Note 5	7.5 m	1.8 m	3.5 m	7.5 m	-
Type 1 agricultural structure	1 storey	35%/75% Note 1	Note 2	7.5 m	1.8 m	7.5 m	7.5 m	Note 3
Type 2 agricultural structure	1 storey	20%		30 m	15 m	30 m	15 m	Note 3 Note 4

Notes to Table 1.4

Note 1. The lot coverage for all Type 1 agricultural structures may be increased by up to 40%, up to a maximum of 75%, for greenhouses.

Note 2. The floor area of a farm education and research facility is limited to 100 m².

Note 3. Agricultural structures must be sited:

- a. A minimum of 30 m from all sand points, wells and streams;
- b. For a type 2 agricultural structure, at least 45 m from lots in a residential zone; and
- c. Or a Type 2 agricultural structure that involves a cannabis production facility, at least 150 m from all lots zoned P1 – Civic Institutional or P2 – Parks and Natural Area or designated in the Official Community Plan as Park or Park Reserve.

Note 4. The production surface area in a structure for cannabis production with a base consisting entirely of soil is limited to 200 m².

Note 5. Floor area comprising a crawl space for a farm residence may be excluded from the floor area calculation.

1.5 ADDITIONAL REGULATIONS

1. A farm residence and all farm residence accessory facilities must be located within the farm home plate.
2. The area of the farm home plate is limited to the greater of 10% of the lot area or 1000 m² up to a maximum of 2000 m².
3. A farm residence must be entirely located within an area no more than 50 m from the front lot line or the exterior side lot line, whichever provides access to the farm residence.
4. Farm residence accessory facilities on lots with a lot width of more than 33 m must be entirely located within an area no more than 60 m from the front lot line or the exterior side lot line, whichever provides access to the farm residence.
5. Within a farm residence, portions of floor other than stairwells that are more than 4.3 m measured from the floor to the ceiling shall be counted twice in the calculation of the floor area ratio.
6. Cooking and sanitary facilities in a Type 1 or Type 2 agricultural structure cannot be located above the first storey of the building or in a mezzanine, and, in the case of sanitary facilities, are limited to one sink and a one toilet unless it can be demonstrated to the satisfaction of the building official that additional facilities are required for specific farm purposes.
7. A crawl space shall not include any windows or external door openings.

2.4 Section II Zones and Zone Regulations, Part 2. Residential Zones is amended as follows:

2.4.1 Replace Note 3 in the Notes to Table 2.4 with the following:

In the calculation of floor area ratio in RS and RRh zones:

- a. Up to 46 m² of floor area may be excluded for an attached garage or carport per single residential use or per rowhouse dwelling unit;
- b. Up to 23 m² of floor area may be excluded for an attached garage or carport per accessory dwelling unit or per principal dwelling unit for a duplex or triplex use;
- c. Up to 90 m² of floor area may be excluded for the dwelling unit of an Accessory Dwelling Unit;
- d. Floor area comprising a crawl space may be excluded;

- e. In RS zones portions of floor area other than stairwells that a more than 4.3 m measured from the floor to the ceiling shall be counted twice.

2.4.2 Delete Regulation 1 of 2.5 Additional Regulations Driveway Access and replace with the following:

Vehicle access to a lot in RS zones for parking and garage access purposes is restricted to a lane where the lot abuts a lane opened for and in vehicular use by the public.

2.5 Section II Zones and Zones Regulations, Part 3. Commercial Zones is amended as follows:

2.5.1 Delete civic address “1360 Kingsway Avenue” and replace with “Building 5, 1320 Kingsway Avenue” in Note 7.a. in the Notes to Table 3.3.

2.6 Section II Zones and Zones Regulations, Part 5. Institutional and Park Zones is amended as follows:

2.6.1 Table 5.3: Institutional and Park Zones Permitted Uses is amended as follows:

Use	Zone	
	P1	P3
Accessory liquor lounge	▪ Note 11	▪ Note 11

2.6.2 Replace Note 11 in the Notes to Table 5.3 with the following:

An accessory liquor lounge in the P1 or P3 zone is permitted when combined with a restaurant or accessory restaurant use.

2.7 Section II Zones and Zones Regulations, Part 6. Comprehensive Development Zones is amended as follows:

2.7.1 Delete 6.20.3 1. in CD20 Comprehensive Development Zone 20 and renumber subsequent regulations accordingly.

2.8 Section III Supplementary Regulations is amended as follows:

2.8.1 Insert item d. of Screening, Landscaping, Outdoor Storage and Fences 4-2 as follows:

All parking areas, loading areas and outdoor display areas must be separated from any abutting street or lot designated for residential use in the Official Community Plan by a landscaped area not less than 2 m wide.

2.8.2 Insert item e. to Screening, Landscaping, Outdoor Storage and Fences 4-2 as follows:

Parking and loading areas must be separated from abutting lots designated for residential use in the Official Community Plan, and from lanes separating the areas from such lots, by a landscape screen of at least 2 m high.

2.8.3 Delete Screening, Landscaping, Outdoor Storage and Fences 4-2 and 4-3 and reorder subsequent notes accordingly.

2.8.4 Replace Screening, Landscaping, Outdoor Storage and Fences 4-6 (formerly 4-8) with the following:

That portion of a fence, wall, landscape screen or any other landscaping that is located within a triangular area measured 5 m each way from the point of intersection of a street or lane with any other street or lane must not exceed a height of 1 m. For clarity, parking spaces and outdoor storage are not permitted within this area.

2.8.5 Delete 14-8 d) and replace with the following:

6.0 m of a principal dwelling located on the same lot as measured from the exterior face of any wall or structure of either building, except for projections permitted in Section II of this Bylaw.

2.8.6 Insert f) to Accessory Dwelling Unit 14-8 as follows:

A triangular area measured 5m each from the point of intersection of any lane with any street or other lane.

2.8.7 Replace 14-9 c) with the following:

Exterior stair cases providing access to upper storeys or access between storeys.

2.7 Section IV is Enforcement is inserted as follows after Section III and the subsequent sections and table of contents are renumbered accordingly:

SECTION IV ENFORCEMENT

1. The provisions of this Bylaw may be enforced by any Bylaw Enforcement Officer.
2. Any Bylaw Enforcement Officer may enter, in accordance with the *Community Charter*, on or into any property subject to this Bylaw in order to inspect and determine whether all regulations, restrictions and requirements are being met.
3. No person shall obstruct a Bylaw Enforcement Officer who is conducting an inspection or enforcement action in relation to this Bylaw.

2.9 Section VII [formerly V] Schedule A - Zoning Map is amended as follows:

2.9.1 The zone of the parcel identified as

Civic Address: NONE
Legal Description: LOT 28 OF DISTRICT LOT 231 GROUP 1 AND OF THE FRACTIONAL SECTION 19 BLOCK 6 NORTH RANGE 1 EAST NEW WESTMINSTER DISTRICT PLAN 69963
PID: 001-974-050

is amended

From: A (Agriculture)
To: P1 (Civic Institutional)

2.9.2 The zone of the parcel known as

Civic Address: NONE
Legal Description: LOT 12 SECTION 18 BLOCK 6 NORTH RANGE 1 EAST NEW WESTMINSTER DISTRICT PLAN LMP5051
PID: 017-846-528

is amended

From: A (Agriculture)
To: P1 (Civic Institutional)

2.9.3 The zone of the parcel known as

Civic Address: NONE
Legal Description: DISTRICT LOT 7311, NEW WESTMINSTER DISTRICT
PID: 012-816-361

is amended

From: A (Agriculture)
To: P1 (Civic Institutional)

2.9.4 The zone of the parcel known as

Civic Address: NONE
Legal Description: PARCEL A, BLOCK 6N, RANGE 1E, NEW WESTMINSTER DISTRICT, PLAN 14789F
PID: 003-034-399

is amended

From: A (Agriculture)
To: P1 (Civic Institutional)

2.9.5 The zone of the parcel known as

Civic Address: NONE
Legal Description: NWP846RX
PID: 013-182-331

is amended

From: Multiple zoning designations
To: P1 (Civic Institutional)

2.9.6 The zone of the parcel known as

Civic Address: NONE
Legal Description: LOT H, BLOCK 6N, SECTION 9,16, RANGE 1E,
NEW WESTMINSTER DISTRICT, PLAN NWP8989
PID: 011-369-396

is amended

From: A (Agriculture)
To: P1 (Civic Institutional)

2.9.7 The zone of the parcel known as

Civic Address: NONE
Legal Description: PARCEL "J" (REFERENCE PLAN 9831) LOT 31
SECTION 9 BLOCK 6 NORTH RANGE 1 EAST
NEW WESTMINSTER DISTRICT PLAN 4318
PID: 011-070-366

is amended

From: A (Agriculture)
To: P1 (Civic Institutional)

2.9.8 The zone of the parcel known as

Civic Address: NONE
Legal Description: LOT 23 EXCEPT: PART DEDICATED ROAD ON
PLAN LMP5051, SECTIONS 17, 18 AND 19
BLOCK 6 NORTH RANGE 1 EAST NEW
WESTMINSTER DISTRICT PLAN 66549
PID: 003-891-232

is amended

From: M2 (Heavy Industrial)

To: P1 (Civic Institutional)

2.9.9 The zone of the parcel known as

Civic Address: NONE
Legal Description: PARCEL 2, DISTRICT LOT 231, NEW WESTMINSTER DISTRICT, PLAN NWP69614
PID: 001-677-900

is amended

From: M1 (General Industrial)
To: P3 (Parks and Natural Areas)

2.9.10 The zone of the parcel known as

Civic Address: NONE
Legal Description: DISTRICT LOT 7313 GROUP 1 NEW WESTMINSTER DISTRICT
PID: 001-724-495

is amended

From: M2 (Heavy Industrial)
To: P1 (Parks and Natural Areas)

2.9.11 The zone of the parcel known as

Civic Address: NONE
Legal Description: LOT 5, DISTRICT LOT 380, NEW WESTMINSTER DISTRICT, PLAN NWP1106, EXCEPT: FIRSTLY: PT SUBDIVIDED BY PL NWP2392 SECONDLY: PCL "B" (REFERENCE PL NWP21658)
PID: 011-993-979

is amended

From: A (Agriculture)
To: P3 (Parks and Natural Areas)

2.9.12 The zone of the parcel known as

Civic Address: NONE
Legal Description: PARCEL 1, DISTRICT LOT 380, NEW WESTMINSTER DISTRICT, PLAN LMP36959, GROUP 1, DEDICATED AS HIGHWAY ON PL 39103
PID: 024-052-990

is amended

From: A (Agriculture)
To: P3 (Parks and Natural Areas)

2.9.13 The zone of the parcel known as

Civic Address: 1269 RIVERSIDE DRIVE [Terry Fox Park]
Legal Description: PARCEL 2, BLOCK 6N, SECTION B6N, RANGE 1E, NEW WESTMINSTER DISTRICT, PLAN LMP33923
PID: 024-052-990

is amended

From: P1 (Civic Institutional)
To: P3 (Parks and Natural Areas)

2.9.14 The zone of the parcel known as

Civic Address: No address
Legal Description: BCP30589
PID: No PID

is amended

From: P1 (Civic Institutional)
To: P3 (Parks and Natural Areas)

2.9.15 The zone of the parcel known as

Civic Address: 1282 LYNWOOD AVENUE
Legal Description: PARCEL 517, SECTION 7, TOWNSHIP 40, NEW WESTMINSTER DISTRICT, PLAN NWP66660, PART SW 1/4
PID: 003-892-743

is amended

From: P1 (Civic Institutional)
To: P3 (Parks and Natural Areas)

2.9.16 The zone of the parcel known as

Civic Address: NONE
Legal Description: LOT 325, SECTION 6/7, TOWNSHIP 40, NEW WESTMINSTER DISTRICT, PLAN NWP49145, EXCEPT PLAN 75780,86586
PID: 006-399-975

is amended

From: Multiple zoning designations

To: P3 (Parks and Natural Areas)

2.9.17 The zone of the parcel known as

Civic Address: 2773 SHAUGHNESSY STREET [Lions Park/Railside]
Legal Description: LOT A, DISTRICT LOT 379, NEW WESTMINSTER DISTRICT, PLAN NWP15873
PID: 003-127-664

is amended

From: P1 (Civic Institutional)
To: P3 (Parks and Natural Areas)

2.9.18 The zone of the parcel known as

Civic Address: 2781 SHAUGHNESSY STREET [Lions Park/Railside]
Legal Description: LOT A, DISTRICT LOT 379, NEW WEST DISTRICT, PLAN NWP15873
PID: 003-127-664

is amended

From: P1 (Civic Institutional)
To: P3 (Parks and Natural Areas)

2.9.19 The zone of the parcel known as

Civic Address: 2265 PRAIRIE AVENUE [Shaughnessy Park]
Legal Description: PARCEL B, DISTRICT LOT 380, NEW WESTMINSTER DISTRICT, PLAN NWP65646
PID: 003-657-442

is amended

From: P3 (Parks and Natural Areas)
To: P1 (Civic Institutional)

2.9.20 The zone of the parcel known as

Civic Address: 940 PRAIRIE AVENUE
Legal Description: LOT 438, SECTION 6, TOWNSHIP 40, NEW WESTMINSTER DISTRICT, PLAN NWP53887
PID: 005-180-104

is amended

From: P1 (Civic Institutional)

To: P3 (Parks and Natural Areas)

2.9.21 The zone of the parcel known as

Civic Address: NONE
Legal Description: BLOCK 6N, RANGE 1E, NEW WESTMINSTER DISTRICT, PLAN BCP9506
PID: No PID

is amended

From: P1 (Civic Institutional)
To: P3 (Parks and Natural Areas)

2.9.22 The zone of the parcel known as

Civic Address: NONE
Legal Description: LOT 100, DISTRICT LOT 379, NEW WESTMINSTER DISTRICT, PLAN NWP58743, EXCEPT PLAN 68311
PID: 000-734-926

is amended

From: P1 (Civic Institutional)
To: P3 (Parks and Natural Areas)

2.9.23 The zone of the parcel known as

Civic Address: 2611 MCALLISTER AVENUE
Legal Description: LOT 99, DISTRICT LOT 379, NEW WESTMINSTER DISTRICT, PLAN NWP58743
PID: 000-734-918

is amended

From: P1 (Civic Institutional)
To: P3 (Parks and Natural Areas)

2.9.24 The zone of the parcel known as

Civic Address: NONE
Legal Description: DISTRICT LOT 380, NEW WESTMINSTER DISTRICT, PLAN BCP36658, GROUP 1, DEDICATED PARK
PID: No PID

is amended

From: RS2 (Residential Small Scale 2)
To: P3 (Parks and Natural Areas)

2.9.25 The zone of the parcel known as

Civic Address: NONE
Legal Description: LOT 3, BLOCK 6N, RANGE 1E, NEW
WESTMINSTER DISTRICT, PLAN EPP27932
PID: 029-139-601

is amended

From: RS2 (Residential Small Scale 2)
To: P3 (Parks and Natural Areas)

2.9.26 The zone of the parcel known as

Civic Address: 2012 SASKATCHEWAN AVENUE
Legal Description: PARCEL H, DISTRICT LOT 340, NEW
WESTMINSTER DISTRICT, GROUP 1, (PLAN
WITH FEE DEPOSITED 25132E) EXC PT
SUBDIVIDED BY PL 71468
PID: 003-802-604

is amended

From: RS2 (Residential Small Scale 2)
To: P3 (Parks and Natural Areas)

2.9.27 The zone of the parcel known as

Civic Address: NONE
Legal Description: NWP2111
PID: 012-545-031

is amended

From: Multiple
To: P3 (Parks and Natural Areas)

2.9.28 The zone of the parcel known as

Civic Address: NONE
Legal Description: NWP2111
PID: 012-545-023

is amended

From: RS2 (Residential Small Scale 2)

To: P3 (Parks and Natural Areas)

2.9.29 The zone of the parcel known as

Civic Address: NONE
Legal Description: PARCEL D, DISTRICT LOT 380, NEW WESTMINSTER DISTRICT, PLAN NWP6234, EXCEPTPLAN 69280
PID: 001-454-030

is amended

From: DC (District Commercial)
To: P3 (Parks and Natural Areas)

2.9.30 The zone of the parcel known as

Civic Address: NONE
Legal Description: LOT 246, SECTION 7, TOWNSHIP 40, NEW WESTMINSTER DISTRICT, PLAN NWP38481
PID: 008-509-859

is amended

From: RS1 (Residential Small Scale 1)
To: P3 (Parks and Natural Areas)

2.9.31 The zone of the parcel known as

Civic Address: NONE
Legal Description: LOT 161, SECTION 6, TOWNSHIP 40, NEW WESTMINSTER DISTRICT, PLAN NWP39730
PID: 008-651-060

is amended

From: RS1 (Residential Small Scale 1)
To: P3 (Parks and Natural Areas)

2.9.32 The zone of the parcel known as

Civic Address: NONE
Legal Description: LOT 161, SECTION 6, TOWNSHIP 40, NEW WESTMINSTER DISTRICT, PLAN NWP39730
PID: 008-651-060

is amended

From: RS1 (Residential Small Scale 1)
To: P3 (Parks and Natural Areas)

2.9.33 The zone of the parcel known as

Civic Address: 1214 ELLIS DRIVE
Legal Description: LOT 211, SECTION 6, TOWNSHIP 40, NEW
WESTMINSTER DISTRICT, PLAN NWP34229
PID: 007-000-511
is amended

From: Multiple zoning designations
To: P3 (Parks and Natural Areas)

3.0 Section VII [*formerly V*] Schedule D – Prescribed is amended as follows:

2.9.34 Remove 2043, 2039, 2035, 2031, 2023, 2019, and 2015 Suffolk Ave from the Impacted Parcels designation within the 400 m prescribed bus stop area depicted on the map.

RECOMMENDATION:

That Committee of Council recommend to Council that:

- 1. the Zoning Bylaw be amended as described in this report; and*
- 2. that the requirement for a Public Hearing be waived as the proposed amendments are consistent with the Official Community Plan.*

PREVIOUS COUNCIL/COMMITTEE ACTION

March 12, 2024 – Council adopted Zoning Amendment Bylaw No.4360 to provide for minor housekeeping changes.

REPORT SUMMARY

This report outlines a number of recommended changes to the Zoning bylaw (No. 3630) that are minor or of a housekeeping nature. These amendments are intended to ensure consistency with the City's own regulations and those of senior levels of government, address mapping or typographical errors and omissions, and clarify the intent of regulations or correct misinterpretations or inconsistencies.

BACKGROUND & DISCUSSION

The following amendments are recommended by staff to provide greater clarity and consistency to existing Zoning bylaw regulations, align with senior government legislation, or correct minor errors and omissions:

1. Insert a definition of "Bylaw Enforcement Officer" and associated provisions and ticketing to enforce the rights of an Officer to inspect a property in accordance with the *Community Charter*. This authority was included in prior iterations of the Zoning bylaw, but has been omitted from the most recent version.
2. Improving the Agricultural zone layout, along with updates to regulations and definitions to clarify provisions related to permitted farm residence accessory facility buildings and structures and to ensure floor area regulations pertaining to "open to above" and crawl spaces to align with other zones.
3. Clarify regulations that were mis-aligned or missed when the City instituted amendments to address Bill 44, including:
 - a. aligning floor area ratio exclusions and accessory building and structure floor area limits for detached and attached garages, carports, and Accessory Dwelling Units (ADUs);

- b. updating the definitions of “Single Residential”, “Townhouse”, and “Rowhouse” to clarify the intent and reference to the number of permitted principal dwellings;
 - c. updating the definition of “Triplex” to clarify the siting of secondary suites is in keeping with BC Building Code requirements;
 - d. ensuring ADU’s are sited to maintain road safety view corridors; and
 - e. aligning the 6 m separation required between an ADU and a principal dwelling with BC Building Code fire separation requirements.
4. Minor changes to meet the intent of regulations throughout the bylaw such as:
- a. including guardrails as a building feature excluded from height calculations (alongside other such similar rooftop projections already exempted);
 - b. deleting the industrial floor area limits in CD20 (Comprehensive Zone 20), this was originally intended to regulate the siting of the industrial buildings but has unintended consequences for mezzanines and does not fit with current industrial policies to support intensification;
 - c. provide for cafes and concessions at municipal parks and recreation facilities to serve alcohol; and
 - d. updates to ambiguous, missed, or confusing wording, civic addresses, legal descriptions, notes, figures, and errors where needed.
5. Amend Schedule A – Zoning Map to apply P1 (Civic Intuitional) and (P3 (Park and Natural Area) zoning to incorrectly zoned City owned land to reflect their P (Park and Recreation) or PR (Park Reserve) designation in the Official Community Plan. These parcels have either retained historic zoning, such as Agriculture, or have had the zoning of an adjacent parcel applied through mapping error; this amendment will ensure all lands designated for park uses within the City are zoned appropriately.
6. Amend Schedule D – Prescribed bus stop map to remove lots included by error.


Staff recommend that the requirement for a Public Hearing be waived as the proposed amendments are generally minor, corrective, or administrative in nature and are in keeping with the Official Community Plan.

FINANCIAL IMPLICATIONS

None.

Zoning Bylaw Minor and Housekeeping Amendments 2025

OPTIONS (✓ = Staff Recommendation)

	#	Description
	1	Recommend to Council that the Zoning Bylaw be amended as described in this report and that the requirement for a Public Hearing be waived.
	2	Recommend to Council that the Zoning Bylaw only be amended for selected items or request additional information before making a decision.
	3	Determine that no changes should be made at this time pending receipt of further information.

Lead author(s): Paul Cloutier

Zoning Amendment Bylaw for 1776 Jensen Avenue – First Three Readings

RECOMMENDATION:


That “Zoning Bylaw, 2008, No. 3630, Amendment Bylaw, 2025, No. 4405”, be given first three readings.

PREVIOUS COUNCIL/COMMITTEE ACTION

On March 11, 2025, Committee of Council recommended to Council that:

- 1. The zoning of 1776 Jensen Avenue be amended from RS1 (Residential Small-Scale 1) to RS4 (Residential Small-Scale 4); and*
- 2. Prior to adoption of the amendment bylaw, the following conditions be met to the satisfaction of the Director of Development Services:*
 - a) Demolition of the existing buildings and structures;*
 - b) Preparation of subdivision plans to the satisfaction of the approving officer; and*
 - c) Completion of the design and submission of fees and securities for off-site works and services.*

OPTIONS (✓ = Staff Recommendation)

	#	Description
	1	Give first three readings to the Bylaw.
	2	Defer first three readings and request staff to provide further information (to be specified).
	3	Decline first three readings of the Bylaw, which will retain the existing Bylaw.

Attachment 1 – Bylaw 4405

Attachment 2 – Report to Committee, March 11, 2025

CITY OF PORT COQUITLAM
ZONING AMENDMENT BYLAW, 2025
Bylaw No. 4405

The Council of the Corporation of the City of Port Coquitlam enacts as follows:

1. CITATION

This Bylaw may be cited for all purposes as “Zoning Bylaw, 2008, No. 3630, Amendment Bylaw, 2025, No. 4405”.

2. ADMINISTRATION

2.1 The Zoning Map of the “Zoning Bylaw, 2008, No. 3630” is hereby amended to reflect the following rezoning:

Civic Address: 1776 Jensen Avenue

Legal Descriptions: Lot 17, District Lot 479, New West District, Plan NWP20629

From: RS1 (Residential Small-Scale 1)

To: RS4 (Residential Small-Scale 4)

as shown on Schedule 1 attached to and forming part of this Bylaw.

READ A FIRST TIME this _____ day of _____, 2025

READ A SECOND TIME this _____ day of _____, 2025

READ A THIRD TIME this _____ day of _____, 2025

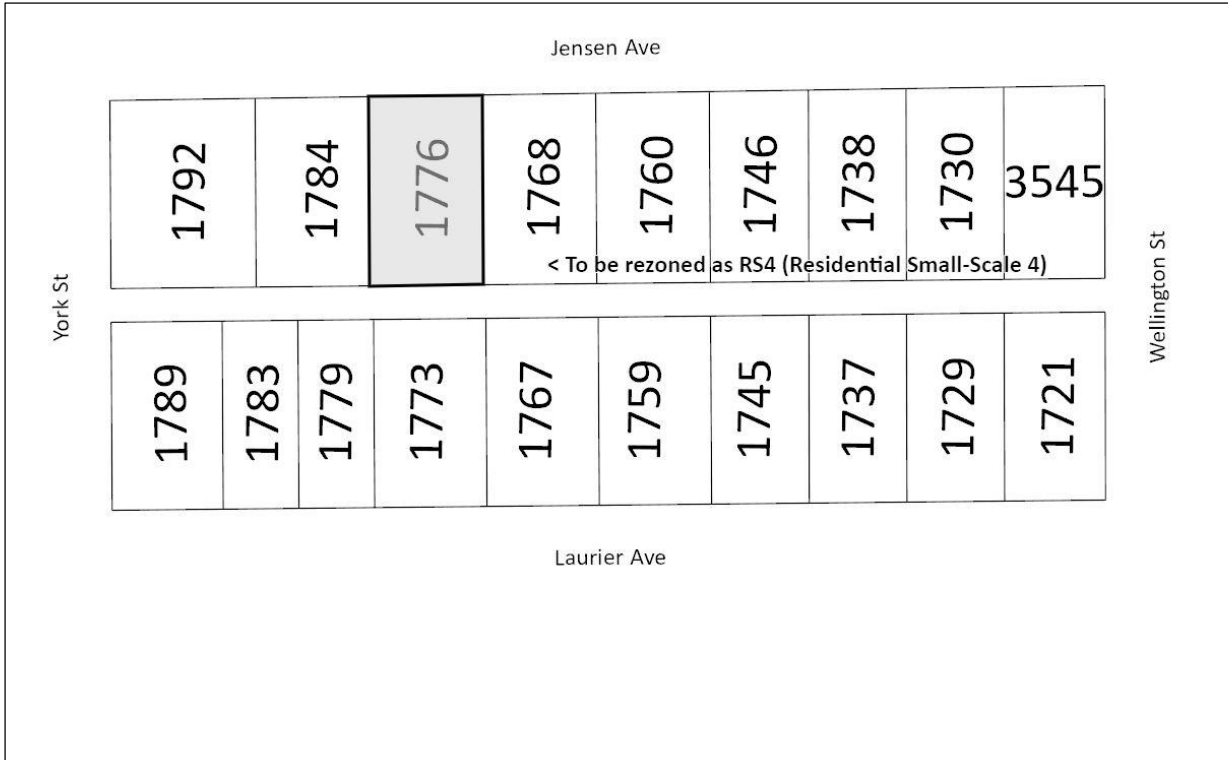
ADOPTION TIME this _____ day of _____, 2025

Mayor

Corporate Officer

SCHEDULE 1

BYLAW 4405



RECOMMENDATION:

That Committee of Council recommend to Council that:

- 1. The zoning of 1776 Jensen Avenue be amended from RS1 (Residential Small-Scale 1) to RS4 (Residential Small-Scale 4); and*
- 2. Prior to adoption of the amendment bylaw, the following conditions be met to the satisfaction of the Director of Development Services:*
 - a) Demolition of the existing buildings and structures;*
 - b) Preparation of subdivision plans to the satisfaction of the approving officer; and*
 - c) Completion of the design and submission of fees and securities for off-site works and services.*

PREVIOUS COUNCIL/COMMITTEE ACTION

None.

REPORT SUMMARY

This report describes an application to amend the zoning of 1776 Jensen Avenue from RS1 (Residential Small-Scale 1) to RS4 (Residential Small-Scale 4) to facilitate a two-lot subdivision. The proposal generally conforms with the housing policies of the Official Community Plan and the subdivision requirements of the RS4 zone. Approval is recommended.

BACKGROUND

Proposal: The applicant is proposing to amend the zoning of 1776 Jensen Street from RS1 (Residential Small-Scale 1) to RS4 (Residential Small-Scale 4) to enable subdivision of the site into two smaller lots.

Context: The property is approximately 707m² (7610ft²) in size and located on the south side of Jensen Avenue between York Street and Wellington Street. The property is currently developed with an older detached one-storey house. Neighbouring lots are of similar size and developed with single detached houses of varying ages. The lot is not in the floodplain and has existing rear lane access.

Rezoning Application for 1776 Jensen Avenue



Site Context

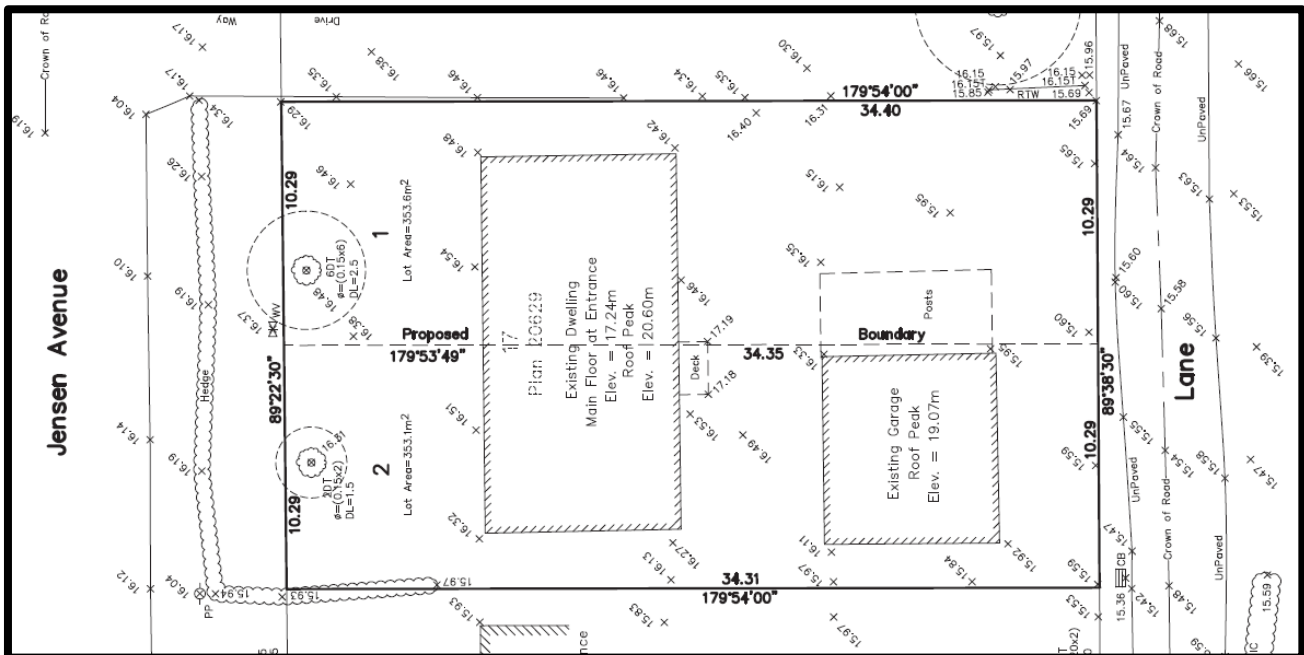
Policy and Regulations: The land use designation in the Official Community Plan for the site is Small Lot Residential (RSL). This designation supports the consideration of RS4 zones which provides for small-scale dwelling units in forms that include single residential, duplex and accessory dwelling units.

The RS4 zone is included in the Small-Scale Multi-Unit Housing and Environmental Conservation Development Permit Areas and any future development would need to meet the associated design guidelines.

Proposed Subdivision: The applicant has provided a preliminary plan of subdivision to demonstrate the proposed lots would meet the subdivision requirements of the Zoning Bylaw for RS4 zones. Both proposed lots exceed the minimum requirements for lot area, width, frontage and depth.

Rezoning Application for 1776 Jensen Avenue

	RS4 Zone Minimum Requirements	Proposed Lot 1	Proposed Lot 2
Lot Area	300.00 m ²	353.8 m ²	353.3 m ²
Lot Width	9.50 m	10.29 m	10.29 m
Lot Frontage	9.50 m	10.29 m	10.29 m
Lot Depth	28.00 m	34.38 m	34.33 m



Preliminary Subdivision Plan

There are two non-significant trees that will be removed as they conflict with future buildings footprints or required servicing works. Tree removal and replacement will be through issuance of a tree cutting permit.

Off-site Infrastructure and Services: The proposed subdivision would be subject to off-site works and services requirements of the Subdivision Servicing Bylaw, including road and lane improvements, sidewalk, drainage, and street lighting, as well as water, sanitary, storm, and third-party services.

Rezoning Application for 1776 Jensen Avenue

DISCUSSION

The proposed rezoning and subdivision would help meet the demand for ground-oriented housing in the community and will result in infrastructure upgrades to services and the transportation network. The proposed lots exceed the Zoning Bylaw's minimum subdivision requirements and complies with OCP housing polices that permit small lot, single-detached residential uses and duplexes in areas designated Small Lot Residential.

Staff recommend approval of the rezoning subject to the specified conditions.

FINANCIAL IMPLICATIONS

The subdivision and construction of new dwellings is anticipated to increase the assessed value of the lands resulting in increased property tax revenue for the City.

PUBLIC CONSULTATION

The applicant has posted a development sign on the site. Staff visited the site on March 5, 2025 and confirm the sign is in good condition. No comments have been received by staff to date.



Development Sign in Place

Rezoning Application for 1776 Jensen Avenue

OPTIONS (✓ = Staff Recommendation)

	#	Description
<input checked="" type="checkbox"/>	1	Recommend to Council that the zoning of 1776 Jensen Avenue be amended from RS1 (Residential Small-Scale 1) to RS4 (Residential Small-Scale 4) subject to the specified conditions being met prior to adoption of the rezoning bylaw.
	2	Request additional information or amendments to the application or recommended conditions to address specified issues prior to deciding on the application.
	3	Recommend to Council that the rezoning application be refused.

Lead author(s): Ben Ricketts

Road Closure and Removal of Highway Dedication Bylaw for Prince Street Road Allowance – First Three Readings

RECOMMENDATION:

That “Road Closure and Removal of Highway Dedication Bylaw for Prince Street Road Allowance, 2025, No.4386”, be given first three readings.

PREVIOUS COUNCIL/COMMITTEE ACTION

On September 10, 2024, Council approved:

1. *“Zoning Bylaw, 2008, No. 3630, Amendment Bylaw, 2024, No. 4385” be given first three readings to amend 1884, 1904, 1912, 1920 and 1930 Harbour Street; 1887, 1893 and 1911 Prince Street; and 1155 Pitt River Road from RS1 (Residential Small-Scale 1) to RTh3 (Residential Townhouse 3); and*
2. *Prior to adoption of the amendment bylaw, the following conditions be met to the satisfaction of the Director of Development Services:*
 - a. *Installation of tree protection fencing for retained trees;*
 - b. *Registration of legal agreements to ensure buildings are designed to incorporate recommendations of the Acoustical Evaluation and for the construction of a sound attenuation fence along Mary Hill Road;*
 - c. **Completion of road closure and sale of Prince Street;**
 - d. *Consolidation of the site and dedication of corner cuts; and*
 - e. *Submission of plans, fees and securities for off-site works and services including relocation of the existing Prince Street sanitary main and installation of flashing pedestrian beacons and streetlighting at Pitt River Road and Harbour Street crosswalks.*

On July 9, 2024, Committee of Council approved to recommend to Council:

1. *The zoning of 1884, 1904, 1912, 1920, and 1930 Harbour Street; 1887, 1893, and 1911 Prince Street; and 1155 Pitt River Road be amended from RS1 (Residential Small Scale to Rth3 (Residential Townhouse 3).*
2. *Prior to adoption of the amending bylaw, the following conditions be met to the satisfaction of the Director of Development Services:*
 - a. *Installation of tree protection fencing for retained trees;*
 - b. *Registration of legal agreements to ensure buildings are designed to incorporate recommendations of the Acoustical Evaluation and for the construction of a sound attenuation fence along Mary Hill Road;*
 - c. **Completion of road closure and sale of Prince Street;**
 - d. *Consolidation of the site and dedication of corner cuts; and*
 - e. *Submission of plans, fees and securities for off-site works and services including relocation of the existing Prince Street sanitary main and installation of flashing pedestrian beacons and streetlighting at Pitt River Road and Harbour Street crosswalks.*

Road Closure and Removal of Highway Dedication Bylaw for Prince Street Road Allowance – First Three Readings

OPTIONS (✓ = Staff Recommendation)

	#	Description
<input checked="" type="checkbox"/>	1	Give first three readings to the Bylaw.
<input type="checkbox"/>	2	Defer first three readings and request staff to provide further information (to be specified).
<input type="checkbox"/>	3	Decline first three readings of the Bylaw, which will retain the existing Bylaw.

Attachment 1 – Bylaw 4386

Attachment 2 – Report to Committee, July 9, 2024

CITY OF PORT COQUITLAM

ROAD CLOSURE AND REMOVAL OF HIGHWAY DEDICATION BYLAW FOR
PRINCE STREET ROAD ALLOWANCE, 2025

Bylaw No. 4386

The Council of the Corporation of the City of Port Coquitlam enacts as follows:

1. CITATION

This Bylaw is cited as “Road Closure and Removal of Highway Dedication Bylaw for Prince Street Road Allowance Bylaw, 2025, No. 4386.

2. ADMINISTRATION

2.1 The portion of road shown in heavy outlined areas on Reference Plan EPP141598 which was prepared on the 3rd day of October, 2024, by Matthew C. Onderwater, a certified British Columbia Land Surveyor, and is included in Schedule 1 of this Bylaw, is permanently closed to traffic.

2.2 The dedication of ‘Highway’ is now removed from the said portion of road included within the heavy outlined area shown on Reference Plan EPP141598.

READ A FIRST TIME this	day of	, 2025
READ A SECOND TIME this	day of	, 2025
READ A THIRD TIME this	day of	, 2025
ADOPTED this	day of	, 2025

Mayor

Corporate Officer

RECOMMENDATIONS:

That Committee of Council recommend to Council that:

- 1. The zoning of 1884, 1904, 1912, 1920, and 1930 Harbour Street; 1887, 1893, and 1911 Prince Street; and 1155 Pitt River Road be amended from RS1 (Residential Small Scale 1) to Rth3 (Residential Townhouse 3).*
- 2. Prior to adoption of the amending bylaw, the following conditions be met to the satisfaction of the Director of Development Services:*
 - a. Installation of tree protection fencing for retained trees;*
 - b. Registration of legal agreements to ensure buildings are designed to incorporate recommendations of the Acoustical Evaluation and for the construction of a sound attenuation fence along Mary Hill Road;*
 - c. Completion of road closure and sale of Prince Street;*
 - d. Consolidation of the site and dedication of corner cuts; and*
 - e. Submission of plans, fees and securities for off-site works and services including relocation of the existing Prince Street sanitary main and installation of flashing pedestrian beacons and streetlighting at Pitt River Road and Harbour Street crosswalks.*

PREVIOUS COUNCIL/COMMITTEE ACTION

February 23, 2021 – Council granted Third Reading to Official Community Plan and Zoning Bylaw amendments to enable a mixed-use townhouse/apartment and commercial development on the site. *This amending bylaw expired on February 23, 2023.*

REPORT SUMMARY

This report provides for consideration of an application to amend the zoning of 1884, 1904, 1912, 1920, and 1930 Harbour Street; 1887, 1893, and 1911 Prince Street; and 1155 Pitt Harbour Road from RS1 (Residential Small Scale 1) to Rth3 (Residential Townhouse 3) to permit the development of a 43-unit townhouse development. The proposed change in land use is in keeping with the sites Residential Townhouse land use designation and policies of the Official Community Plan and the development is designed to generally comply with the regulations of the proposed RTh3 zone. The report recommends a set of conditions be met as part of the rezoning approval.

BACKGROUND

Proposal: The applicant has proposed to rezone nine RS1 (Residential Small Scale 1) properties at the juncture of Harbour Street, Pitt River Road and Mary Hill Bypass to Rth3 (Residential Townhouse 3) to enable the development of a 43-unit townhouse project.

Rezoning Application for 1884 Harbour Street

The site was previously subject to an Official Community Plan and Zoning Bylaw amendment application to permit construction of a mixed-use development containing ground floor commercial space, low rise apartment buildings, and ground-oriented townhomes. This application received third reading in 2021. The site subsequently sold to new owners who determined they wished to proceed with a townhouse development.

Context: The proposed 9,595 m² (103,279.7 ft²) site is located to the east Pitt River Road between Harbour Street and the Mary Hill Bypass. The nine lots (1884, 1904, 1912, 1920, and 1930 Harbour Street; 1887, 1893, and 1911 Prince Street; and 1155 Pitt Harbour Road) have been vacant for a number of years and are in a naturalized state. The site slopes downward north to south from Harbour Street to the Mary Hill Bypass. 1155 Pitt River Road and 1911 and 1893 Prince Street are located within the floodplain and have a minimum flood construction level of 5.15m elevation.



Location Map

Surrounding land uses include houses along the north edge of Harbour Street, a gas station and Marian Kroeker Park to the west of Pitt River Road, and existing townhouse complexes east of the site.

Policy and Regulations:

OCP Land Use Designation and Policies: The land use designation in the Official Community Plan (OCP) for the site is Townhouse Residential and the properties are currently zoned RS1. OCP housing policies allow for Council consideration of rezoning to a townhouse zone, including the Residential Townhouse 3 (RTh3) zone, and encourage a variety of housing types to accommodate the needs of Port Coquitlam's population and demand for multi-family housing.

Rezoning Application for 1884 Harbour Street

Zoning: The proposed Residential Townhouse 3 (RTh3) zone is intended to accommodate and regulate attached townhouses accessed from an internal driveway with a maximum permit density of one townhouse per 220m² of land area.

Housing Needs Report: The 2022 Housing Needs Report identified the need for additional ground-oriented dwelling units with three or more bedrooms.

Development Permit: Development of the site for townhouse uses would be subject to the Intensive Residential and Environmental Conservation development permit area designations of the OCP. The Intensive Residential objectives and design guidelines promote coordination of siting and building design; use of high-quality cladding materials; consideration of the relationship between buildings and open areas; and the overall visual impact of buildings and landscaping. The Environmental Conservation objectives and guidelines encourage sustainable development and building design; efficient use of energy, water and other resources; and reduction of waste and pollution.

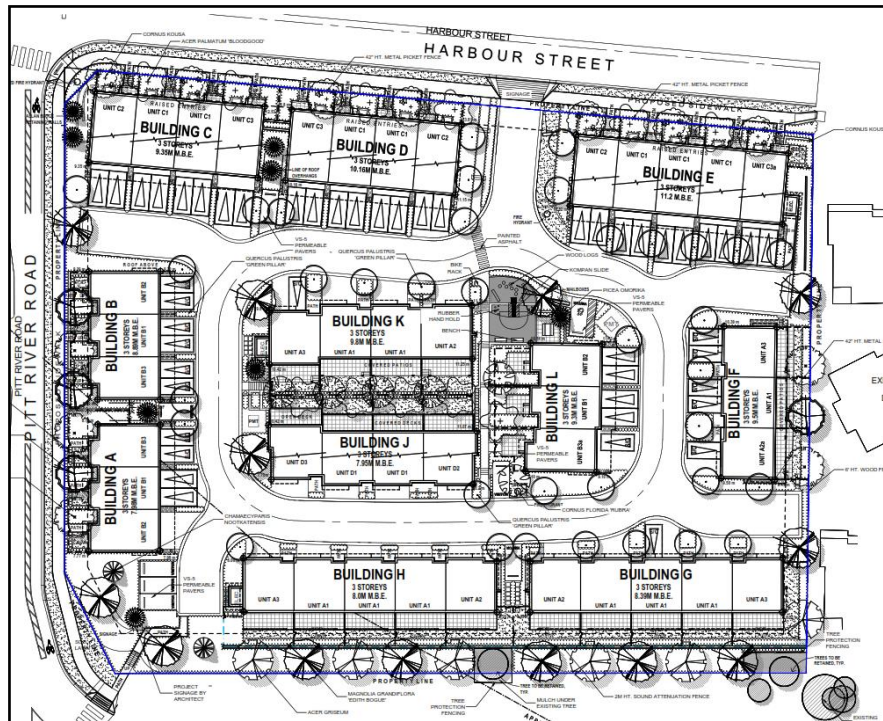
Archeology: The BC Archeology Branch has identified the site as being in proximity to a known archaeological site. The applicant has advised they have met archaeological assessment and permit requirements from the Branch; confirmation from the Branch will be required prior to issuance of a Building Permit.

Project Description: The proposal includes 43 3-storey townhouse units within 11 buildings. The unit mix included 21 3-bedroom and 27 4-bedroom units, ranging in size from 141.5 m² (1,524 ft²) to 180.6 m² (1,945 ft²).

The development is designed with townhomes fronting onto both Pitt River Road and Harbour Street; these units will have pedestrian entrances accessed from the sidewalk, with small landscaped entries and rooftop decks oriented south. The units along the Mary Hill Bypass are designed with rear entry off an internal driveway; these units have a small back yard (separated from the bypass with a sound attenuating fence) and an enclosed balcony on the second level. Three units face the adjoining townhouse development; these units have small patios and second floor balconies with a fence and trees to help provide for privacy. Three buildings are clustered within the interior of the site, with entrances from the interior driveway.

Pedestrian access to the interior of the site is provided by a series of walkways from surrounding roads. Vehicle access to the site is provided from Harbour Street with a circular driveway allowing circulation throughout the site. Each townhouse unit has an attached 2-car garage with some units having additional one or two parking spaces on their driveway aprons. An accessible parking stall is located near the entrance to the site and visitor parking is provided between units and a cluster of three spaces located near the southwest corner of the site.

Rezoning Application for 1884 Harbour Street



Site Plan

Project Profile

Bylaw Regulation	Requirement	Proposed	Variance
Minimum lot area	1,000m ²	9,582.5 m ²	-
Density (units per area)	1 unit per 220m ² (43)	43 units	-
Building Lot Coverage	40%	39.2%	-
Front setback (Harbour Street)	7.5 m	3.1 m	4.4 m
Rear setback (Mary Hill Bypass)	7.5 m	6.6 m	0.9 m
Interior side setback (1872 Harbour Street)	1.8/3.5 m (window to a habitable roof)	1.8/3.5 m	-
Exterior side setback (Pitt River Road)	3.5 m	3.5 m	-
Useable open space	30 m ² per unit	46.3 m ² per unit	-
Family friendly units	25% (10 units)	100% (43)	-
Building height	10.5 m	9.3 m – 11.56 m	0.92 m – 1.56 m
Parking:	-	-	-
Residential Off-street spaces	2 spaces per unit (86)	134	-
Visitor	1 space per 5 units (9)	7 spaces	2 spaces
Accessible	1 per 100 stalls or part (1)	1 space	-
Small car	25% of provided (33)	16 spaces	-

Rezoning Application for 1884 Harbour Street

The townhouses are designed in a contemporary style that incorporates flat roofs and geometric massing. The material palette is a mix of cementitious panels and horizontal siding in white, greys, and wood tones, with black trim. The design provides an articulated façade with variable setbacks and vertical expression.



Pitt River Road Elevation



View from Pitt River Road / Mary Hill Bypass

The applicant confirm the design of the buildings would incorporate recommendations of an acoustical consultant (see Attachment 2) to ensure the townhouses meet the Canadian Mortgage and Housing Corporation's (CMHC) maximum acceptable road noise levels for dwellings. These recommendations include upgraded window and exterior door assemblies and double layers of drywall. In addition, the applicants have included a sound attenuation fence along the Mary Hill Bypass as to the acoustical consultant recommended by the Ministry of Transportation and Infrastructure (MOTI).

The proposal includes a landscape plan containing a variety of trees, shrubs, grasses, and ground cover plants intended to beautify and provide screening between adjacent uses and soften the edges of the development. This includes a line of magnolias and paperbark maple trees and the sound attenuating fence along the Mary Hill Bypass. The landscape plan also includes a playground, benches and a picnic table (located to the north of Building L) as on-site amenities for the future residents.

The project also includes a number of measures to conform with the requirements of the Environmental Conservation DPA, incorporating a high level of mechanical and envelope efficiency in line with the City's Step Code 3 requirements. Thermally broken window frames and high-performance glazing, along with operable windows allowing for cross-ventilation, are included. The on-site landscaping is designed to promote opportunities for passive cooling. Water conservation is promoted through low-flow fixtures and drought tolerant planting. A rain sensor is proposed to be included with the irrigation system.

Rezoning Application for 1884 Harbour Street

A full description of the building and landscape design and environmental conservation measures will be provided at development permit stage if the rezoning proceeds.

Trees: The project arborist report (Attachment 3) identifies 86 on-site trees, 80 of which are proposed for removal. The majority of these trees either conflict with the locations of the proposed buildings, pathways, driveways, and other site improvements, or are otherwise in poor condition or dead. Two trees, a black walnut and bitter cherry, along south side of the site along Mary Hill Bypass are to be retained, along with four trees within the municipal boulevard/MOTI right-of-way. In accordance with the Tree bylaw, 91 replacement trees are required. The landscape plans include 98 replacement trees.

Variance Requests: To achieve the proposed design, the applicant is requesting several variances. Shorter setbacks along Harbour Street and Mary Hill Bypass are requested so that the site has enough depth for units along Harbour Street to have both garage and parking pads (providing for increased off-street resident parking). A reduction in two visitor parking spaces requested in order to provide space for a larger, more functional play space; the applicants note the additional off-street resident spaces will help off-set visitor demand to the site.

Variances to the maximum townhouse height requirements (ranging from 0.92m to 1.56m) are requested for several of the buildings. The applicants note the variances are due to accommodate the sloping site. The applicants further note the screening being proposed along the east side of the site will help mitigate the requested height variance and, due to the site sloping downward from Harbour Street, the three buildings located along the south side of Harbour Street do not require variances.

All requested variances would be confirmed through issuance of a development permit.

Off-site Works and Services: The applicant is required to provide a number of off-site works and services in accordance with the Subdivision Servicing Bylaw. Road and frontage improvements, including sidewalk, boulevard landscaping, drainage, and street lighting are required along Pitt River Road and Harbour Street. Dual pedestrian controlled flashing beacons at the Pitt River Road/Harbour Street intersection are also required to create safer pedestrian crossings similar to other intersections along the Pitt River Road corridor. The proposed sidewalks and RRFBs are consistent with the draft 2024 Master Transportation Plan. A 5m x 5m corner-cut road dedication is required to accommodate the off-site infrastructure.

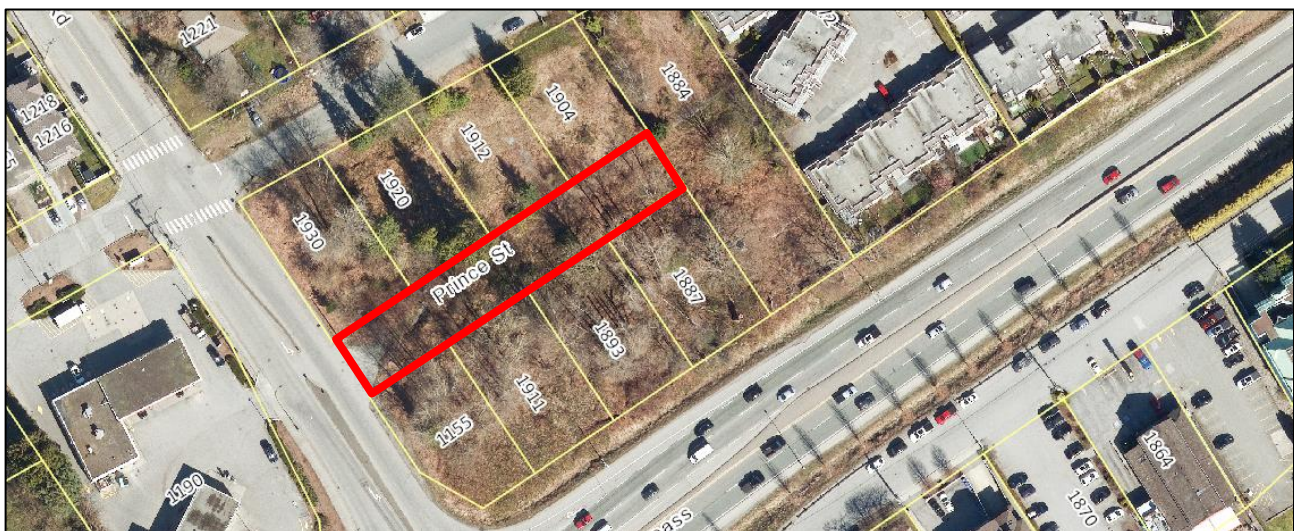
Required utility works include the capping off and abandonment of existing water, sanitary, and storm services; replacement of the Pitt River Road water main and provision of a fire hydrant on Harbour Street at the entrance to the development; and rerouting of the existing sanitary main within Prince Street should Prince Street be closed and sold. This Prince Street sanitary main provides service to

Rezoning Application for 1884 Harbour Street

the neighbouring developments east of the site. Storm service and third-party utilities are also required.

A traffic impact assessment provided by the applicant noted the existing road network can accommodate the increased traffic demands without requiring any significant upgrades.

Road Closure and Land Purchase: The applicant has requested to purchase Prince Street, an unopened road, and incorporate that land into the proposed development. The inclusion of Prince Street in the proposal would increase the site area by approximately 1,350 m². In accordance with normal process, the road closure, purchase and sale would be completed prior to bylaw adoption.



Prince Street

DISCUSSION

The OCP establishes how the community is intended to develop, designates lands for uses in keeping with these policies, and provides guidance on the types of housing the City should encourage. The proposed rezoning aligns with the site's OCP townhouse land use designation and policies to encourage housing choice and options and reflects findings of the Housing Needs Report which recommends the construction of more ground and family-oriented housing. Staff note that recent Federal and Provincial direction is for municipalities to enable construction of more "missing middle" or ground-oriented housing.

Staff note the proposed project fits well into the existing form and character of the neighbourhood and is designed to provide for an attractive lively street presence. The development exceeds the City's requirements for family friendly units and the required number of resident parking space, and the requested variances are be minor and not to have a detrimental effect on the overall form, character or function of the development. Offsite works will support the development and include improvements to the pedestrian network in the area.

Rezoning Application for 1884 Harbour Street

Staff recommend that Committee of Council forward the rezoning application to Council with a recommendation to support consideration of the rezoning with specified conditions to ensure tree protection, registration of legal agreements to ensure adherence to acoustical measures, closure and purchase of Prince Street, and all required off-site works.

FINANCIAL IMPLICATIONS

It is anticipated that in addition to revenue from the sale of the lane, there will be an increase in property tax and utility revenue with the addition of the new development.

PUBLIC ENGAGEMENT

The applicant hosted an open house on in July 2023 at the TriCity Church; the meeting was attended by 17 members of the community and comment forms were collected from eight attendees. The comments received were generally supportive of the proposal (see Attachment 5). A few concerns were raised about the proposal which included a desire for commercial space within the development, traffic issues resulting from a potential access to the site off of Pitt River, and pedestrian safety at the Pitt River Road/Harbour crossings.

Several verbal and written submission were received by staff noting a desire to see commercial space within the new development (concern that the area does not currently have a grocery store within walking distance), concerns regarding sufficient parking and rooftop decks proposed for the buildings fronting Harbour Street.


On July 2, 2024, staff visited the site to confirm the development signs were in good order.



Site Sign

Rezoning Application for 1884 Harbour Street

OPTIONS (✓ = Staff Recommendation)

	#	Description
	1	Recommend to Council that the zoning of 1884, 1904, 1912, 1920, and 1930 Harbour Street; 1887, 1893, and 1911 Prince Street; and 1155 Pitt Harbour Road be amended from RS1 (Residential Small Scale 1) to Rth3 (Residential Townhouse 3) and that specified conditions be met prior to adoption of the rezoning bylaw.
	2	Request additional information or amendments to the application to address specified issues prior to making a decision on the application.
	3	Recommend to Council that the rezoning application be refused.

ATTACHMENTS

Attachment 1: Architectural and Landscape Drawings

Attachment 2: Acoustical Evaluation, Brown Strachan Associates

Attachment 3: Arborist Report. VDZ+A

Attachment 4: Public Information Meeting Summary, Comments Forms, and Emails (Redacted)

Lead author(s): Paul Cloutier



GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

VIEW FROM MARY HILL BYPASS



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COVER SHEET

DP0.0

SCALE

1/26



VIEW FROM PITT RIVER ROAD



VIEW FROM MARY HILL BYPASS



VIEW FROM ENTRY ON HARBOUR STREET



VIEW FROM PEDESTRIAN ENTRANCE ON MARY HILL BYPASS



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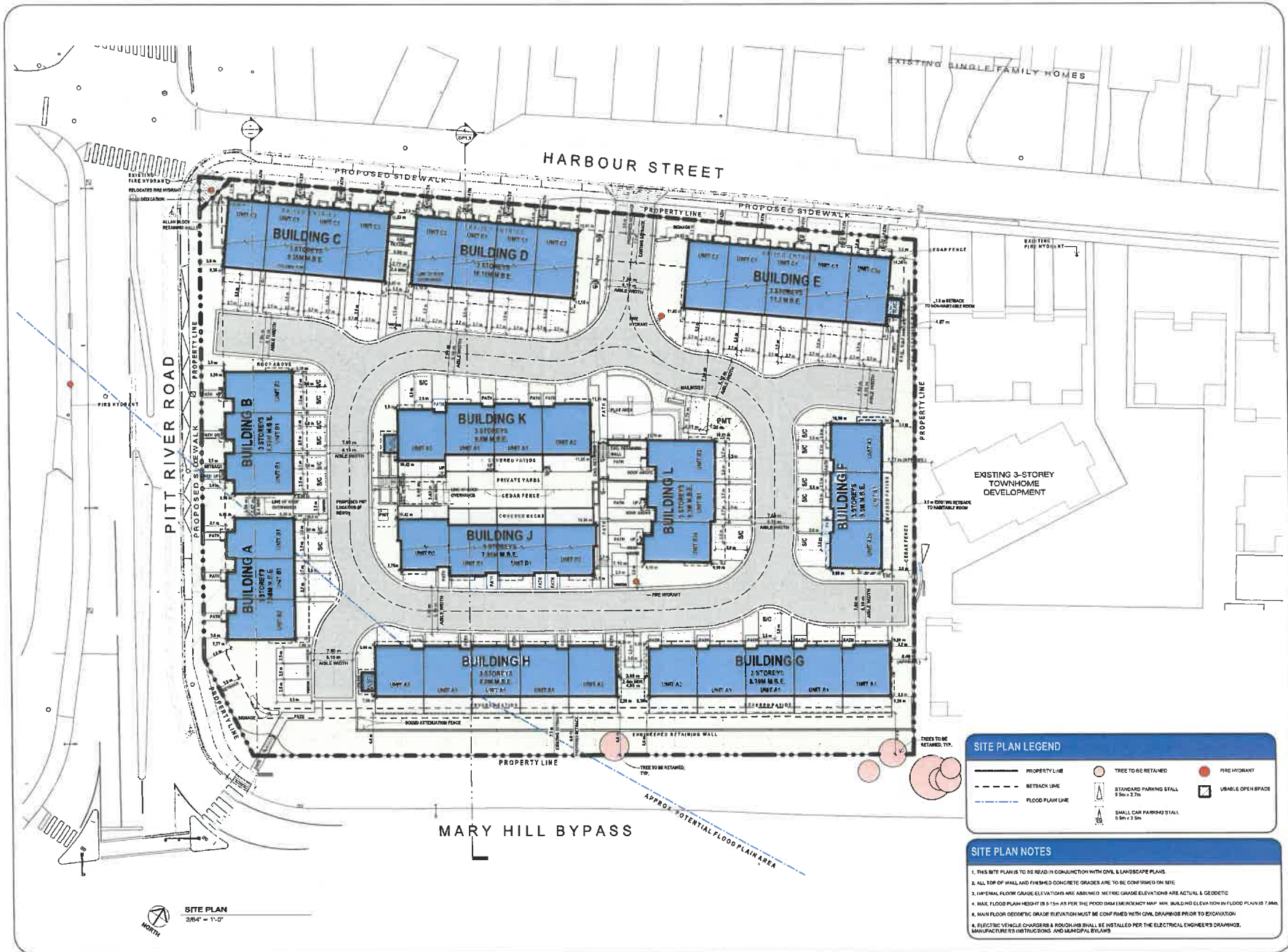
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B	2024.04.04	DP REVISION
C	2024.04.29	DP REVISION

RENDERINGS

DP0.1

SCALE

1:27



SITE PLAN
3/64" = 1'-0"

SITE PLAN LEGEND

—	PROPERTY LINE	○	TREE TO BE RETAINED	●	FIRE HYDRANT
- - -	SETBACK LINE	⊞	STANDARD PARKING STALL 9.5m x 5.7m	□	VEHICLE OPEN SPACE
- · - · -	FLOOD PLAIN LINE	⊞	SMALL CAR PARKING STALL 5.5m x 3.5m		

- SITE PLAN NOTES**
1. THIS SITE PLAN IS TO BE READ IN CONJUNCTION WITH CIVIL & LANDSCAPE PLANS.
 2. ALL TOP OF WALL AND FINISHED CONCRETE GRADES ARE TO BE CONFIRMED ON SITE.
 3. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED. METRIC GRADE ELEVATIONS ARE ACTUAL & GEODETIC.
 4. MAX. FLOOD PLAIN HEIGHT IS 9.15m AS PER THE FLOOD DAM EMERGENCY PLAN. BUILDING ELEVATION IN FLOOD PLAIN IS 7.6m.
 5. MAIN FLOOR GEOMETRIC GRADE ELEVATION MUST BE CONFIRMED WITH CIVIL DRAWINGS PRIOR TO EXCAVATION.
 6. ELECTRICAL VEHICLE CHARGERS & POUCHES SHALL BE DETAILED PER THE ELECTRICAL ENGINEER'S DRAWINGS, MANUFACTURER'S INSTRUCTIONS, AND MUNICIPAL BYLAWS.



FOR ALL UNRESOLVED ISSUES, THE CLIENT SHALL BE ADVISED BY THE ARCHITECT. THE ARCHITECT'S RESPONSIBILITY IS LIMITED TO THE DESIGN AND CONSTRUCTION OF THE BUILDING. THE ARCHITECT IS NOT RESPONSIBLE FOR THE DESIGN OR CONSTRUCTION OF ANY OTHER WORKS OR SERVICES. THE ARCHITECT'S LIABILITY IS LIMITED TO THE DESIGN AND CONSTRUCTION OF THE BUILDING. THE ARCHITECT'S LIABILITY IS LIMITED TO THE DESIGN AND CONSTRUCTION OF THE BUILDING.

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DATE
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2361



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A	2024.02.08	DP REVISION
B	2024.04.04	DP REVISION
C	2024.04.28	DP REVISION

SITE PLAN

DP1.1

SCALE
As Indicated



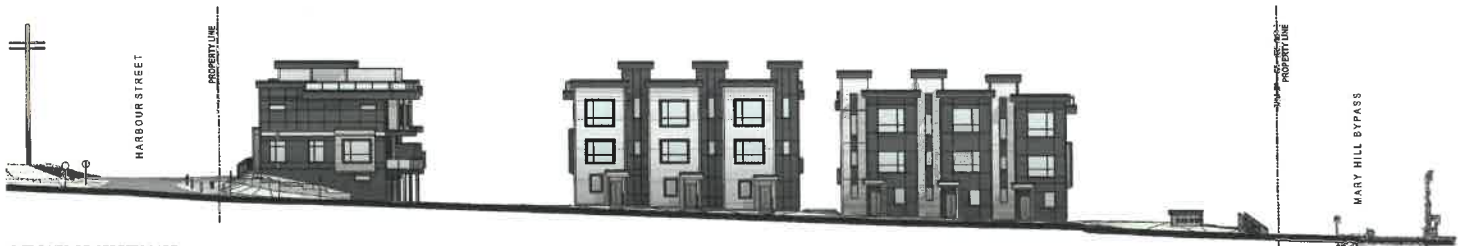
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MARY HILL BYPASS STREETSCAPE
1/16" = 1'-0"



PITT RIVER RD STREETSCAPE
1/16" = 1'-0"

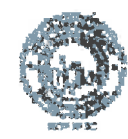


HARBOUR STREET STREETSCAPE
1/16" = 1'-0"

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2301



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C	2024.04.20	DP REVISION

SITE STREETSCAPES

DP1.2

SCALE
1/16" = 1'-0"
130



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2024.02.26

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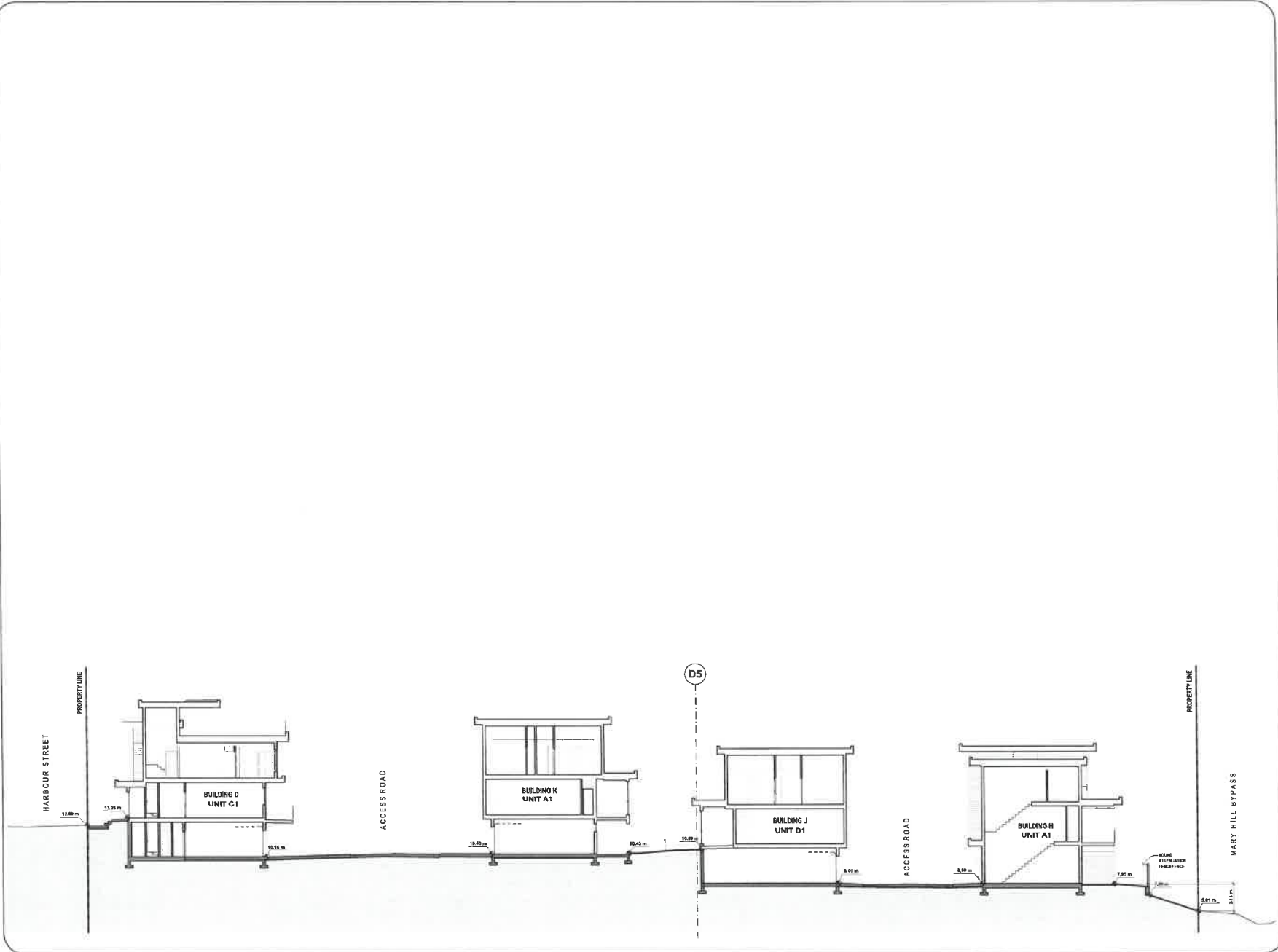
SITE SECTIONS

DP1.3

SCALE

1/32" = 1'-0"

131





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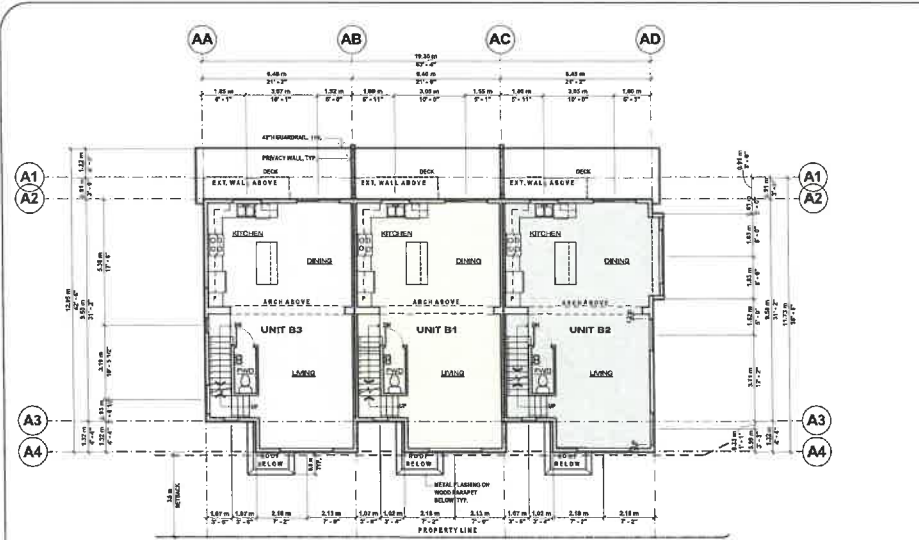


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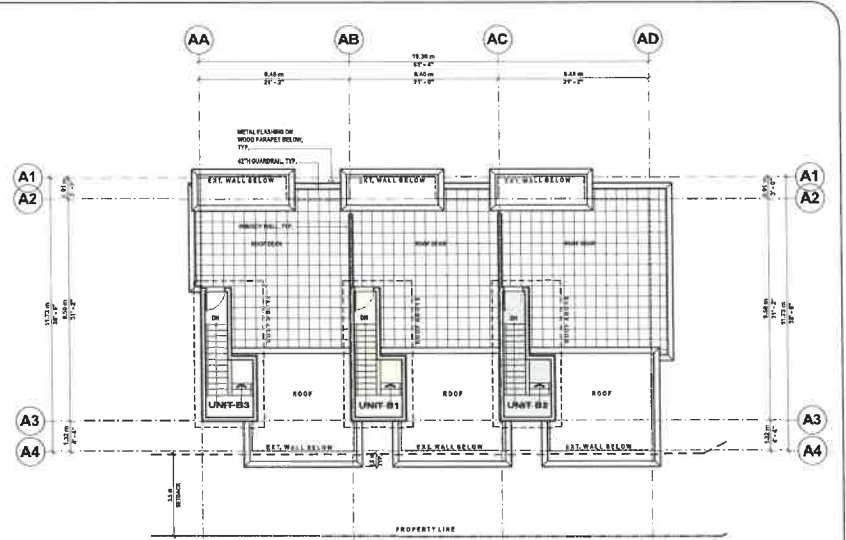
FLOOR PLANS

DP2.1-A

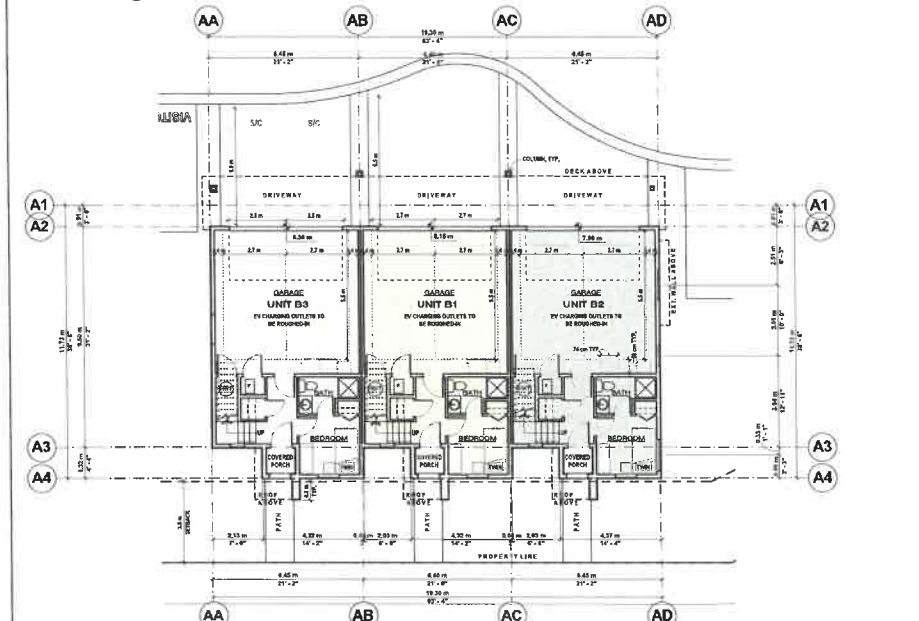
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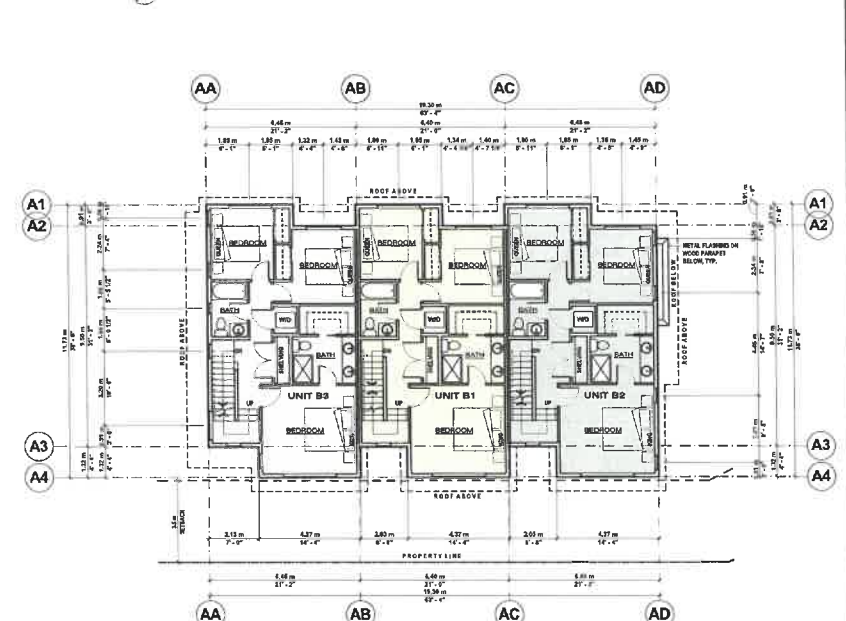
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ROOF DECK PLAN
1/8" = 1'-0"



1ST FLOOR PLAN
1/8" = 1'-0"



3RD FLOOR PLAN
1/8" = 1'-0"



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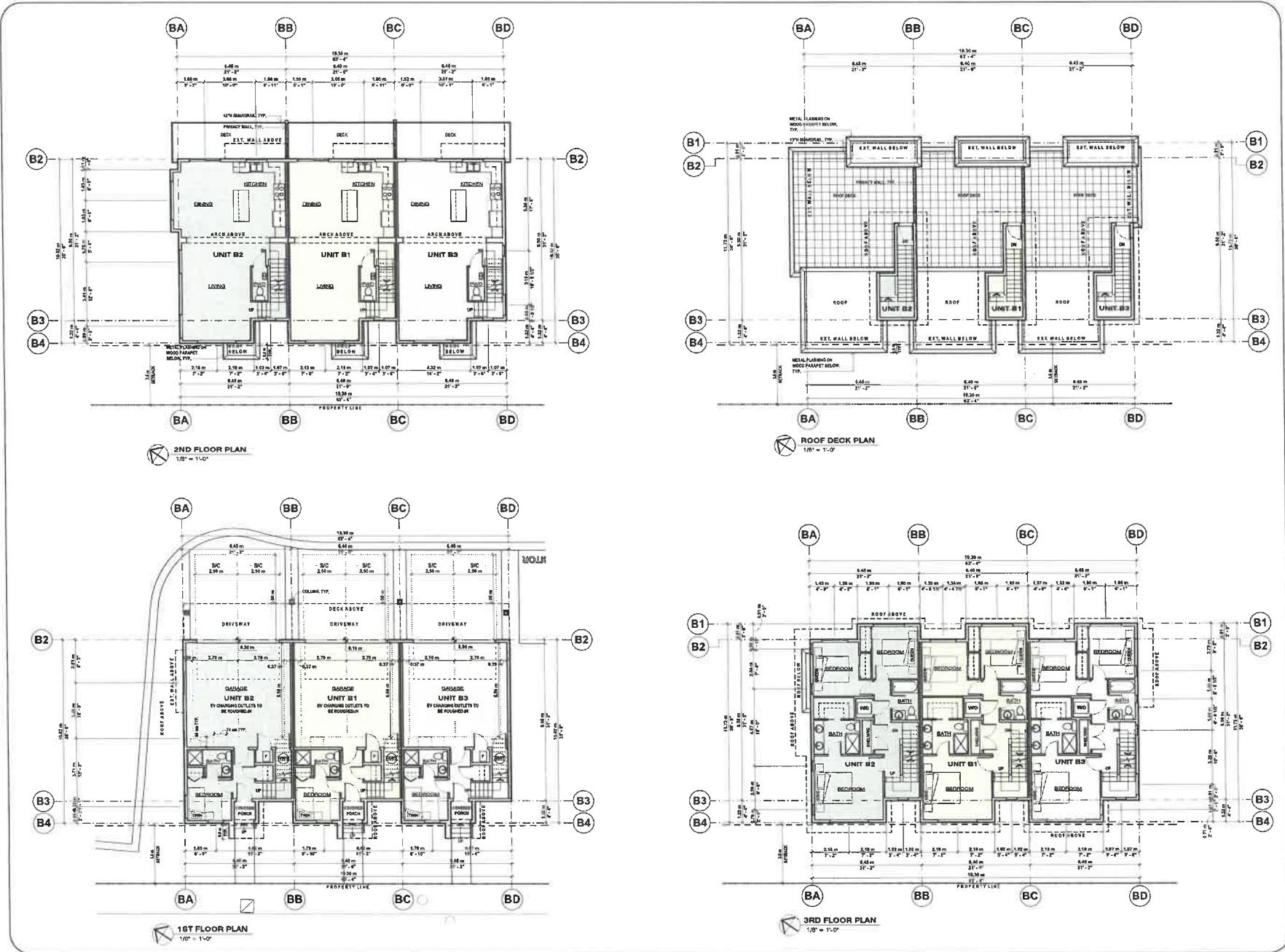


REVISIONS
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FLOOR PLANS

DP2.1-B

SCALE: 1/8" = 1'-0"





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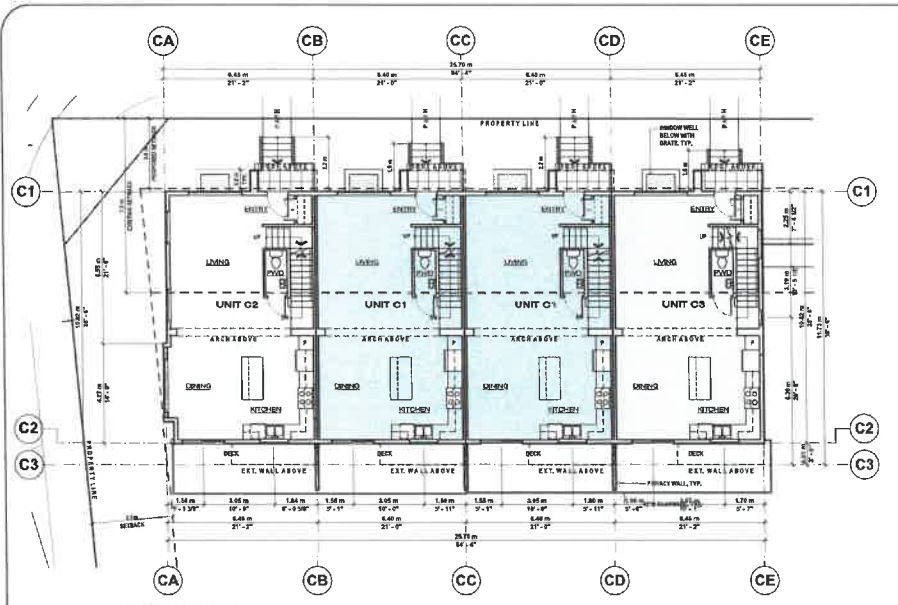
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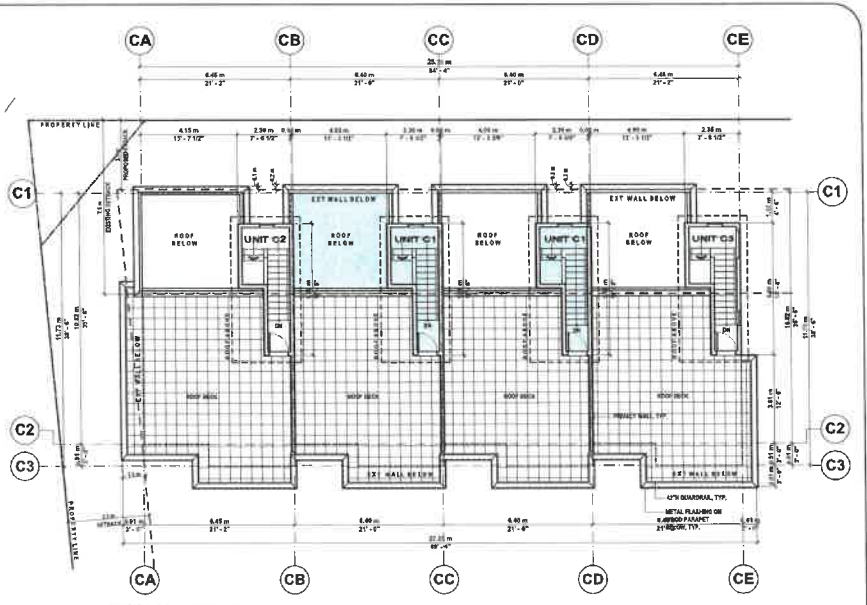
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DP.2.1-C

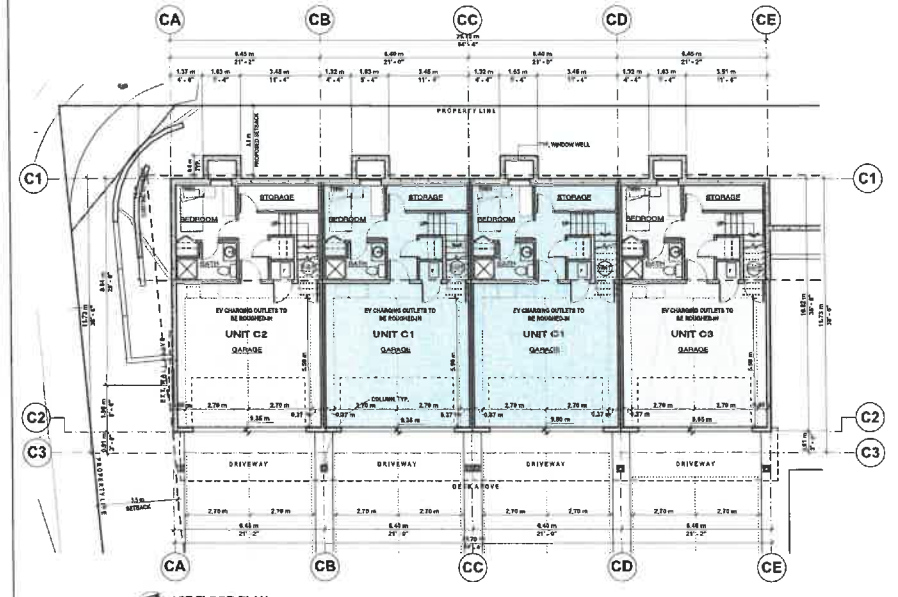
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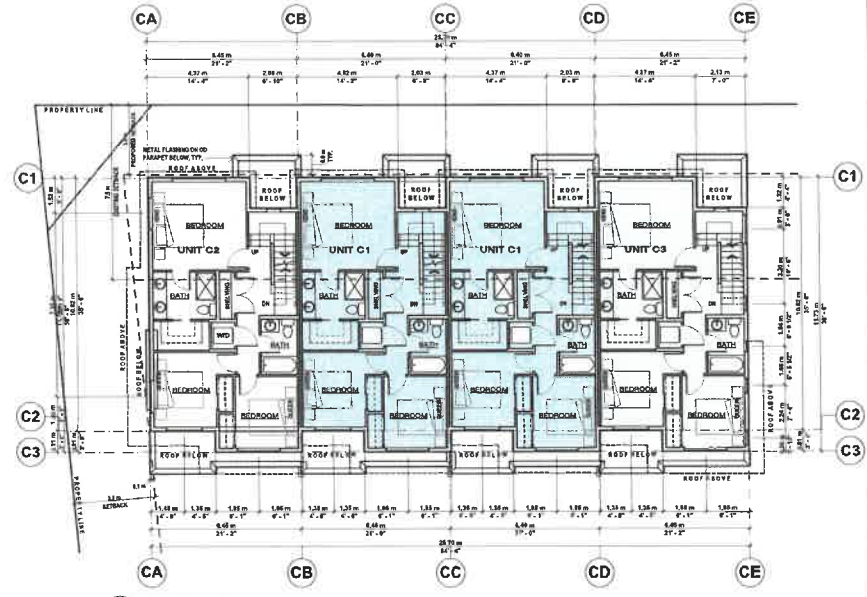
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1ST FLOOR PLAN
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3RD FLOOR PLAN
1/8" = 1'-0"



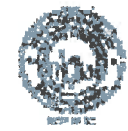
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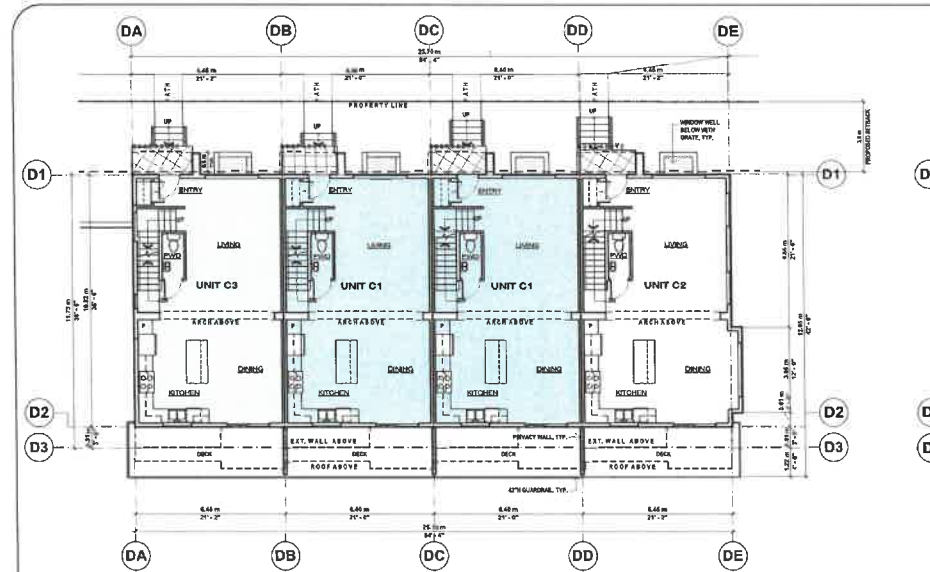
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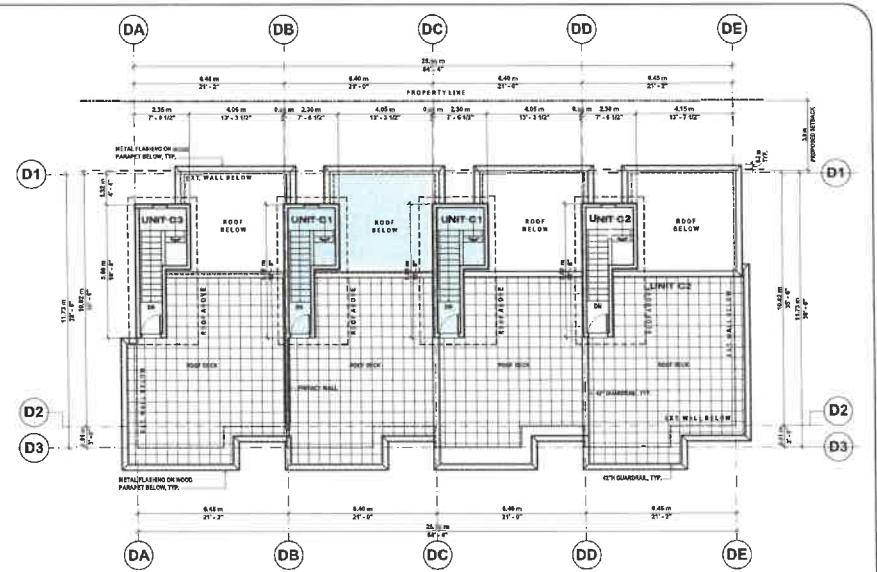
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DP.2.1-D

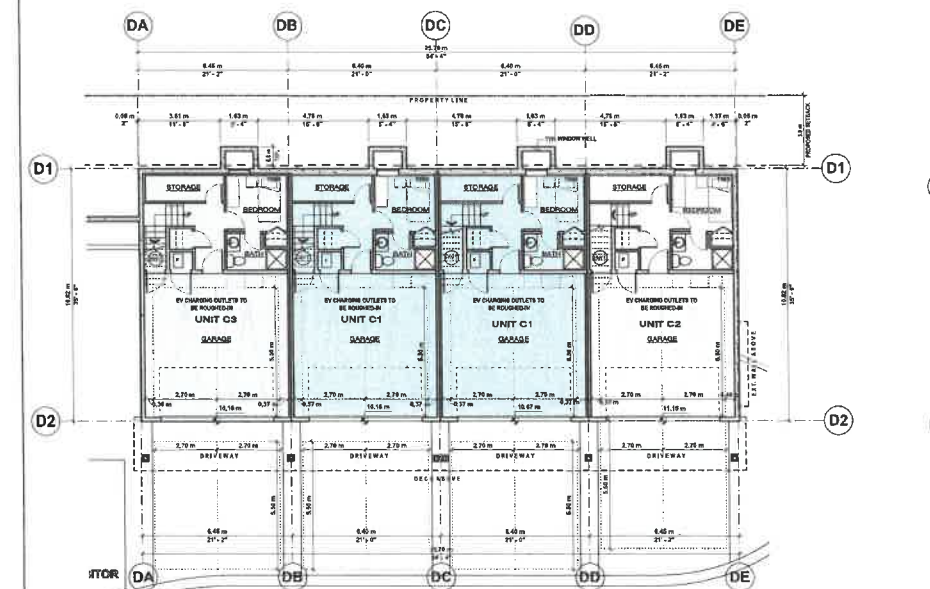
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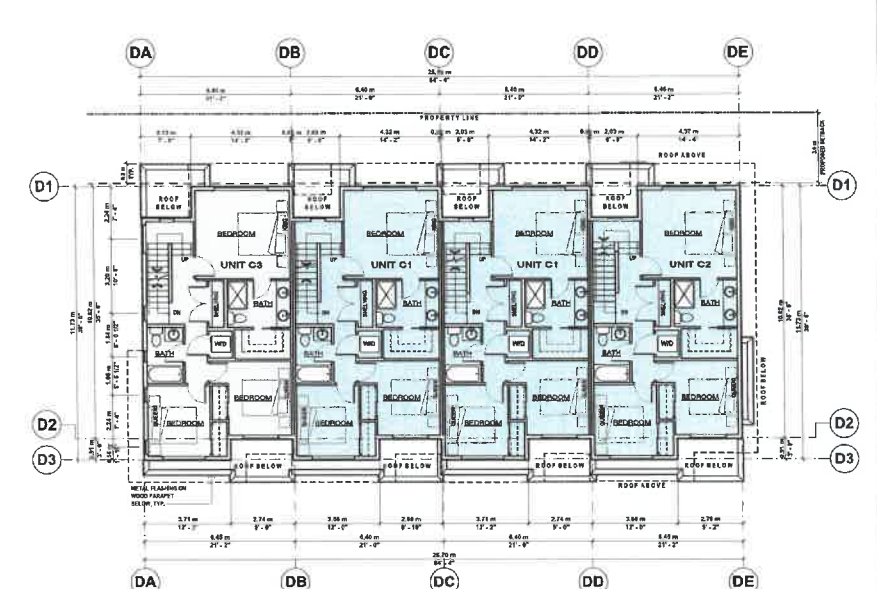
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ROOF DECK PLAN
1/8" = 1'-0"



1ST FLOOR PLAN
1/8" = 1'-0"

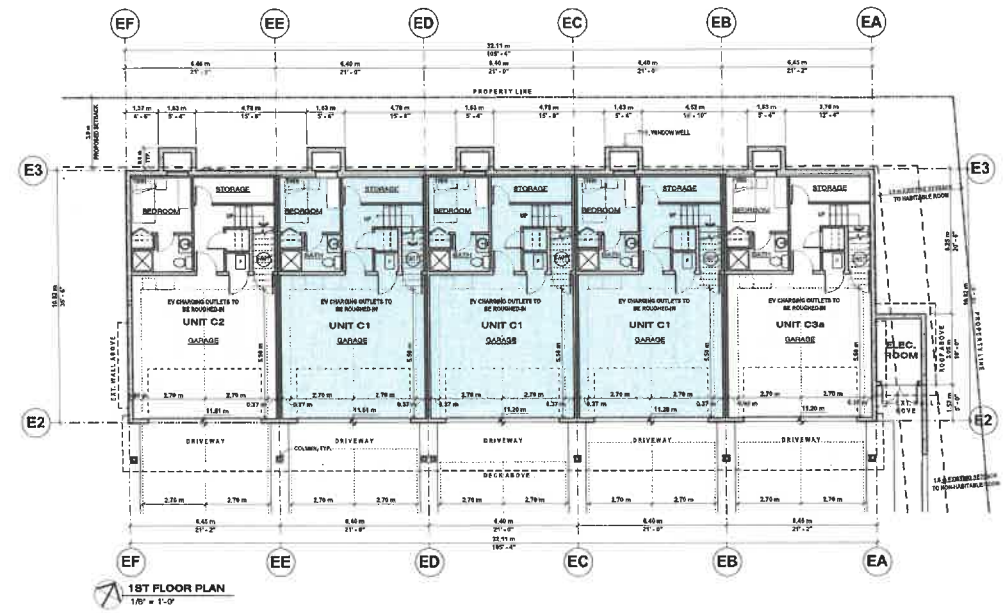
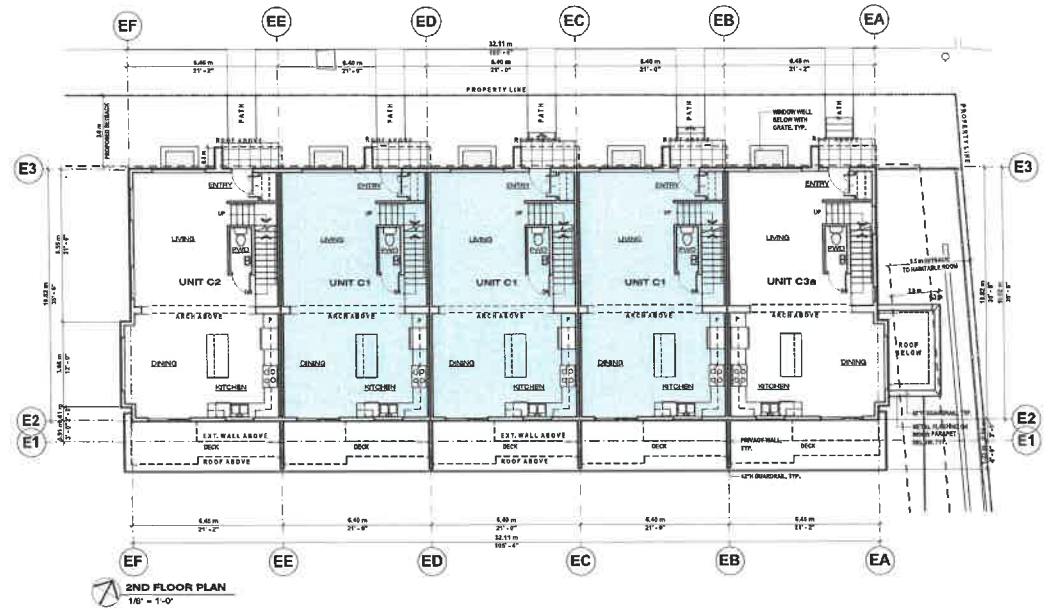


3RD FLOOR PLAN
1/8" = 1'-0"



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FLOOR PLANS

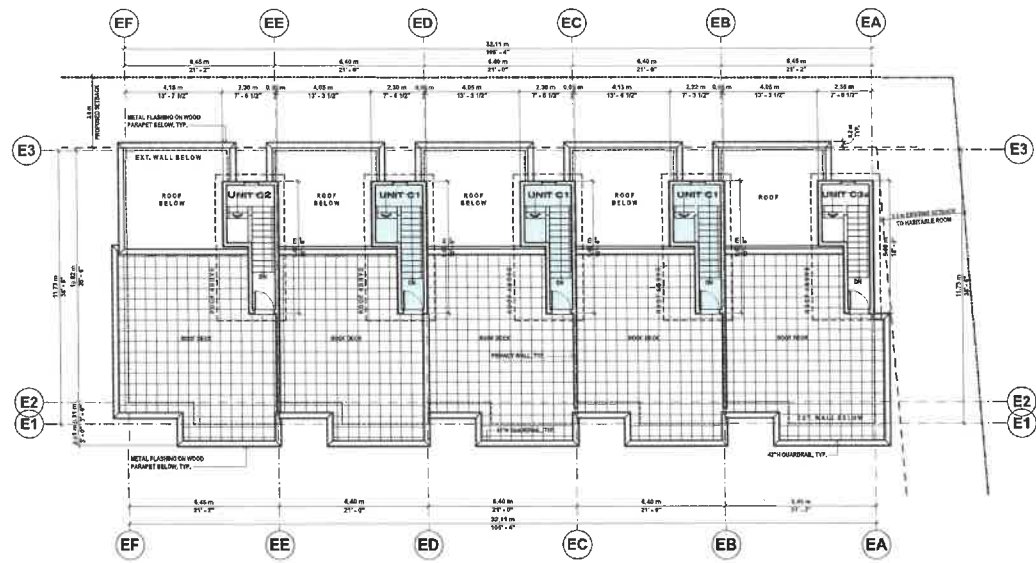
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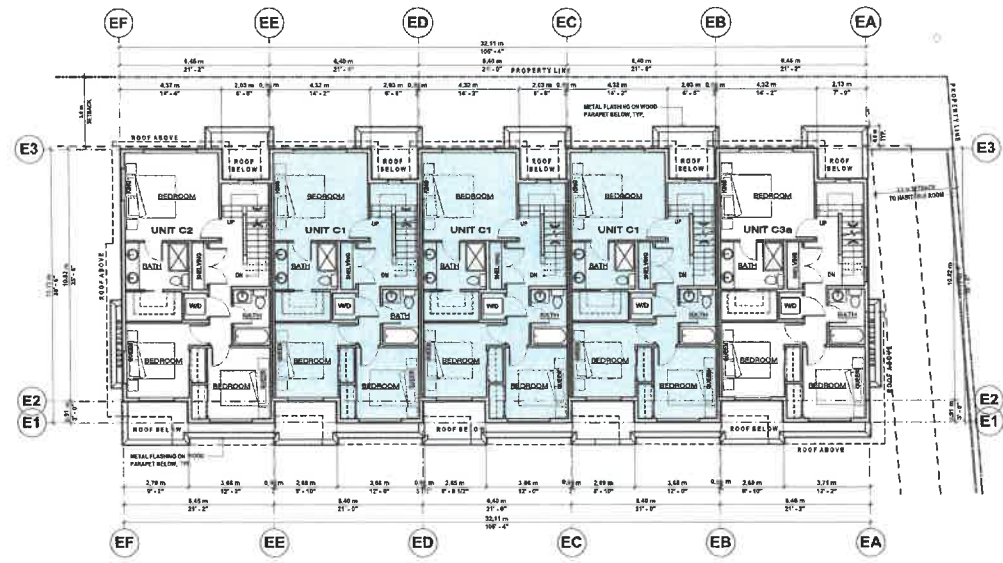


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ROOF DECK PLAN
1/8" = 1'-0"



3RD FLOOR PLAN
1/8" = 1'-0"

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MARY HILL BYPASS, PITT
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DATE

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A	2024.02.08	CP REVISION
B	2024.04.04	CP REVISION
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FLOOR PLANS

DP2.2-E

SCALE
1/8" = 1'-0"



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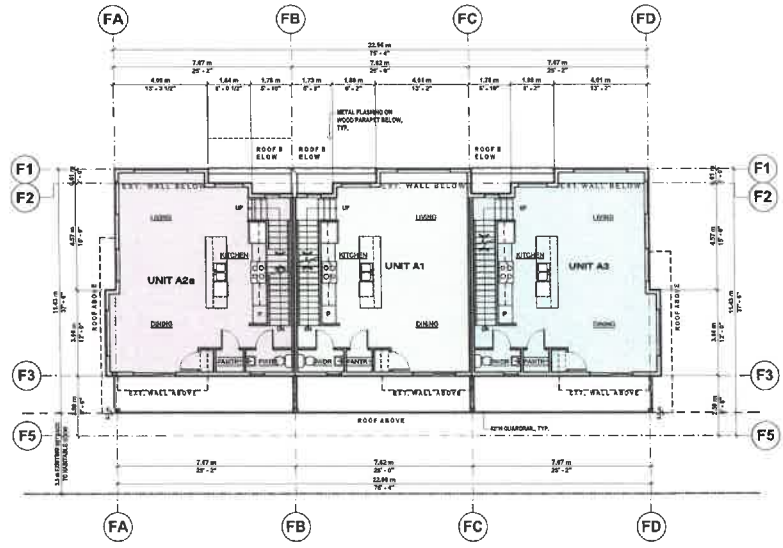


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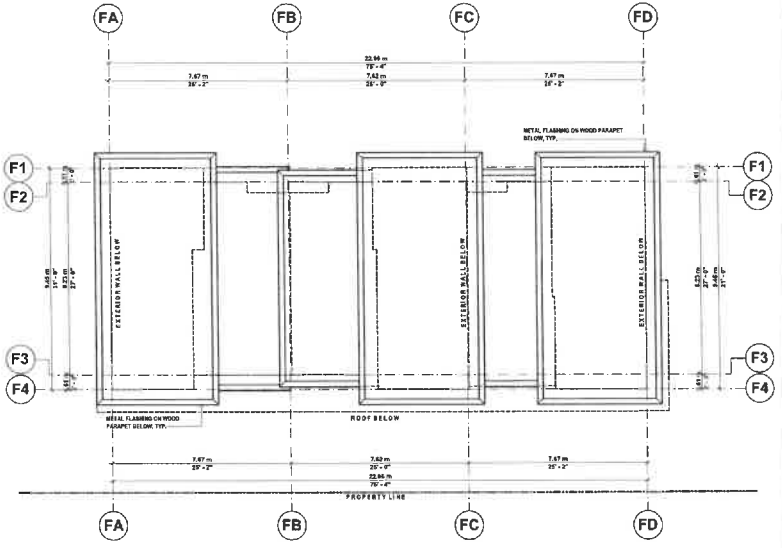
FLOOR PLANS

DP2.1-F

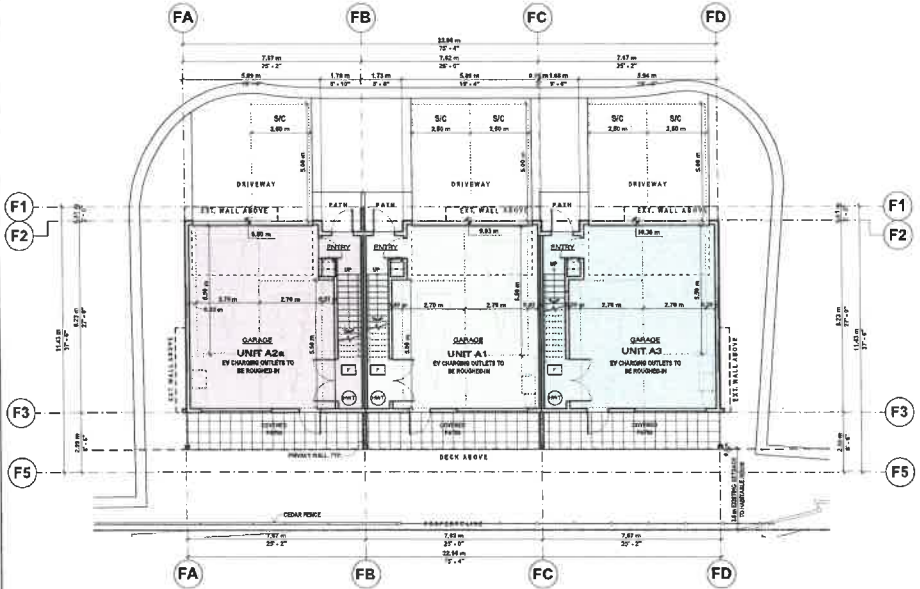
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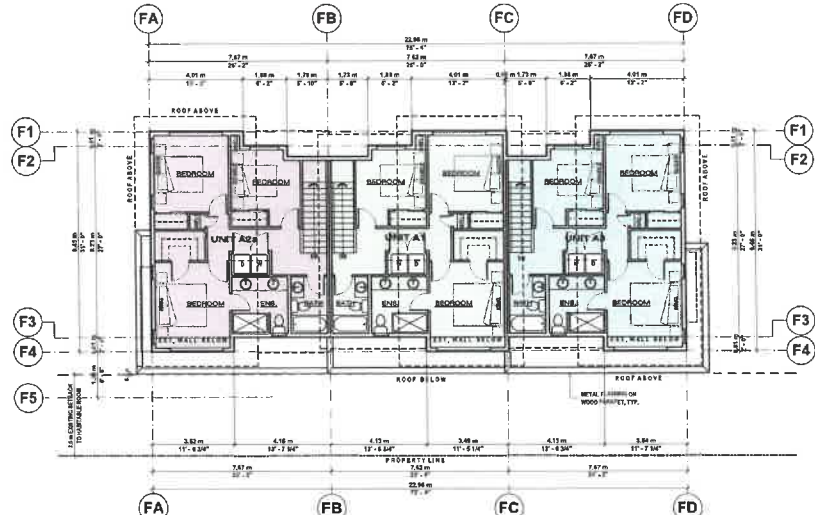
2ND FLOOR PLAN
1/8" = 1'-0"



ROOF PLAN
1/8" = 1'-0"



1ST FLOOR PLAN
1/8" = 1'-0"



3RD FLOOR PLAN
1/8" = 1'-0"



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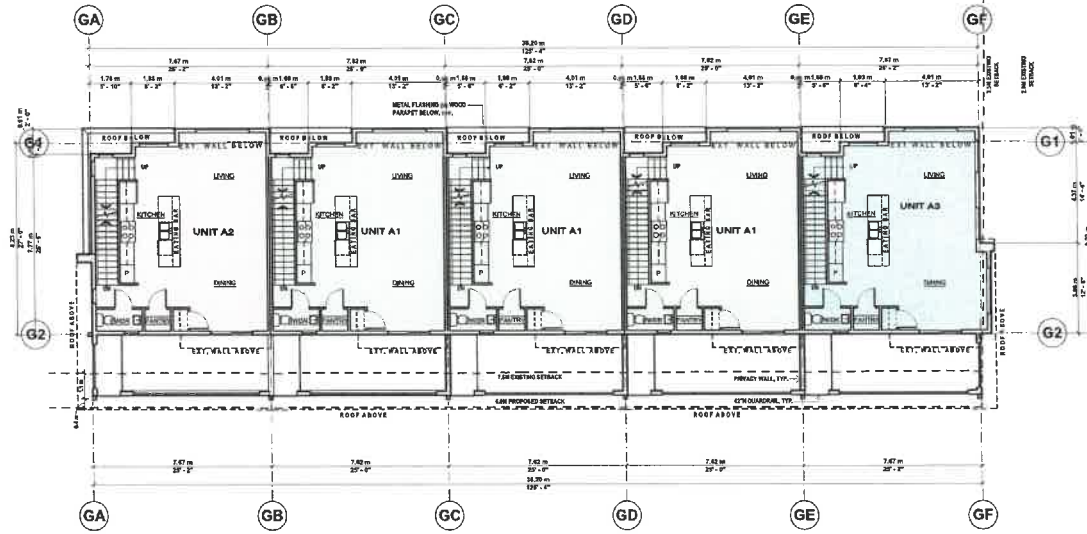


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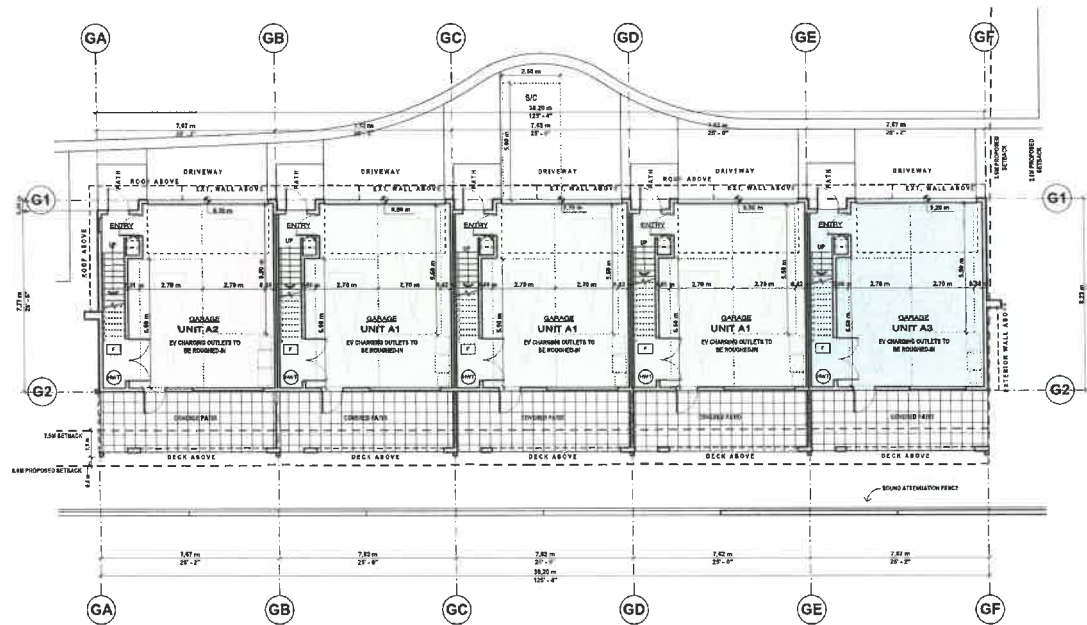
FLOOR PLANS

DP2.1-G

SCALE
1/8" = 1'-0"



2ND FLOOR PLAN
1/8" = 1'-0"



1ST FLOOR PLAN
1/8" = 1'-0"



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2024 02 05
JOB NO.
2301

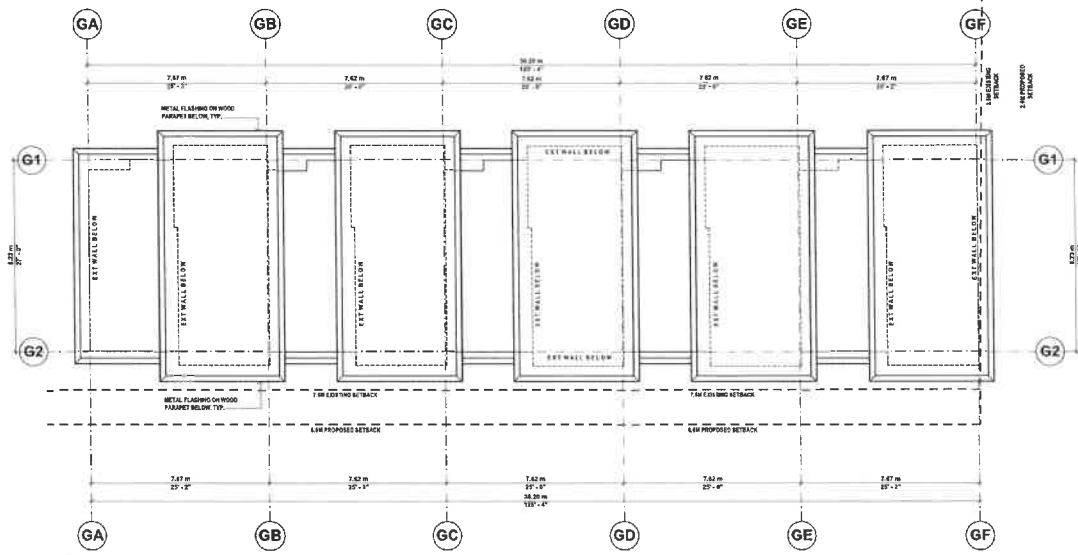


REVISION	NO.	DATE	DESCRIPTION
A	2024 02 05	DP REVISION	
B	2024 04 04	QP REVISION	
C	2024 04 26	QP REVISION	

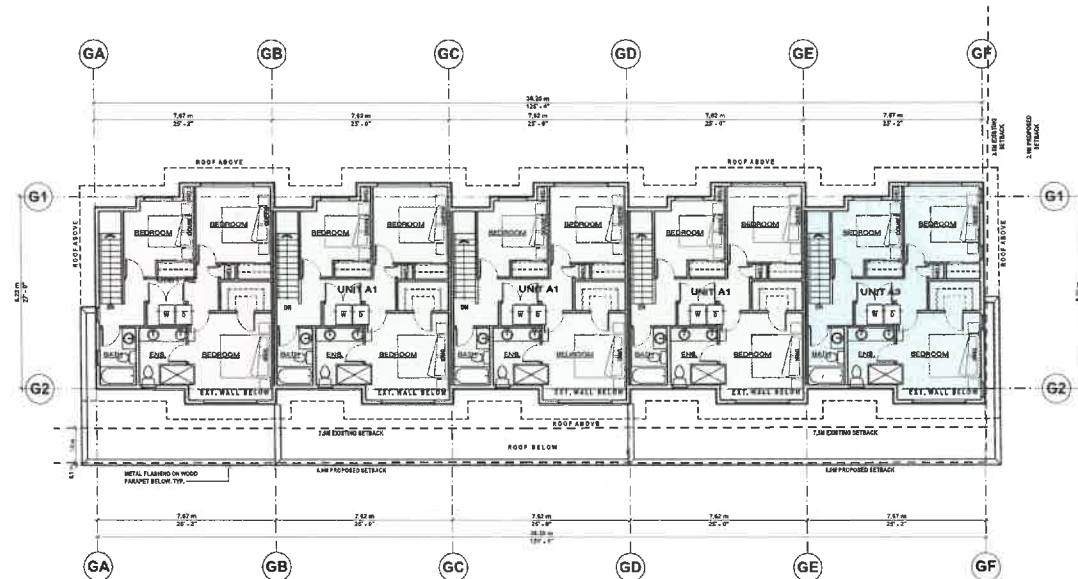
FLOOR PLANS

DP2.2-G

SCALE
1/8" = 1'-0"



ROOF PLAN
1/8" = 1'-0"

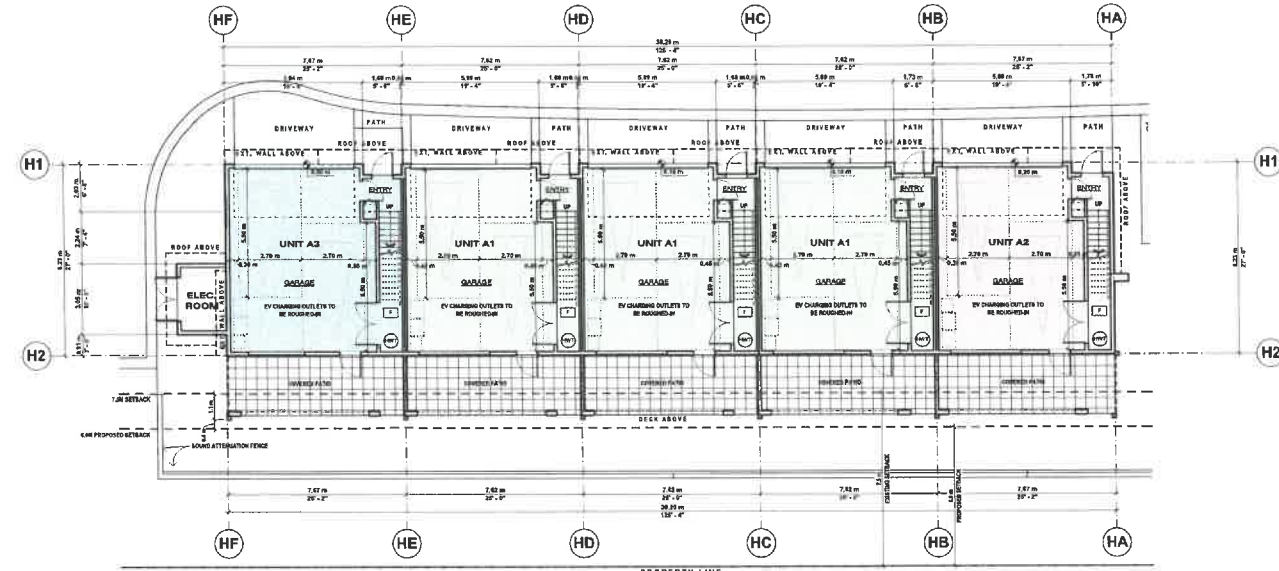
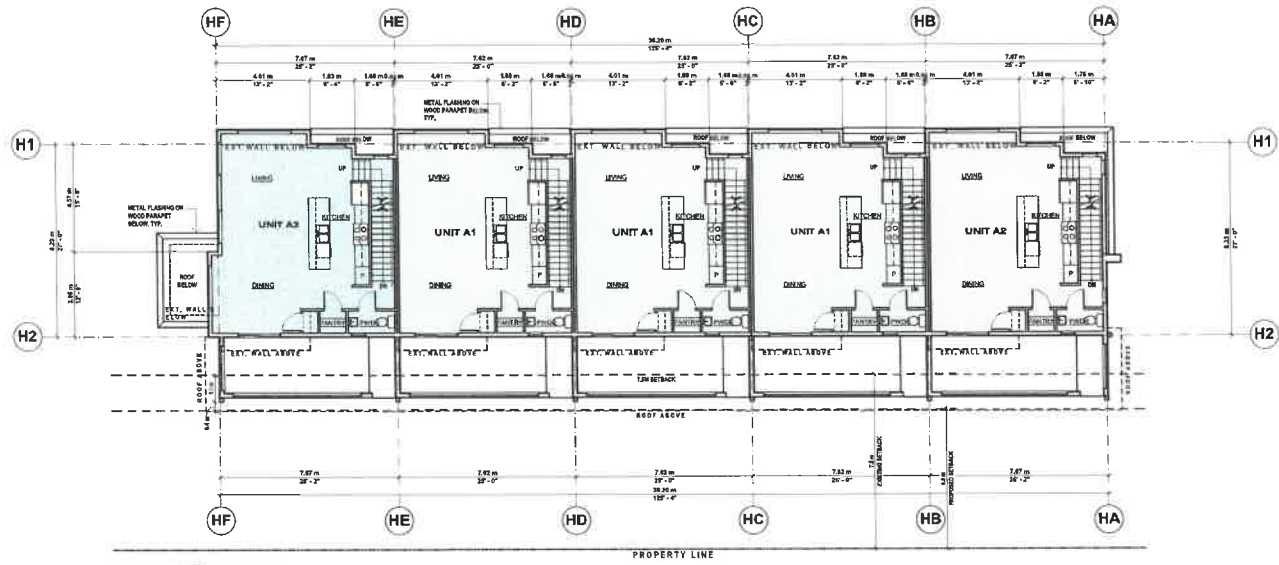


3RD FLOOR PLAN
1/8" = 1'-0"



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MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE

2024.03.08

JOB NO.

2301



NO.	DATE	DESCRIPTION
A	2024.03.08	DP REVISION
B	2024.04.04	DP REVISION
C	2024.04.29	DP REVISION

FLOOR PLANS

DP2.1-H

SCALE
1/8" = 1'-0"
141



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DATE

2024.02.08

JOB NO.

2301



REVISIONS

NO.	DATE	DESCRIPTION
A	2024-02-08	DP REVISION
B	2024-04-24	UP REVISION
C	2024-04-28	DP REVISION

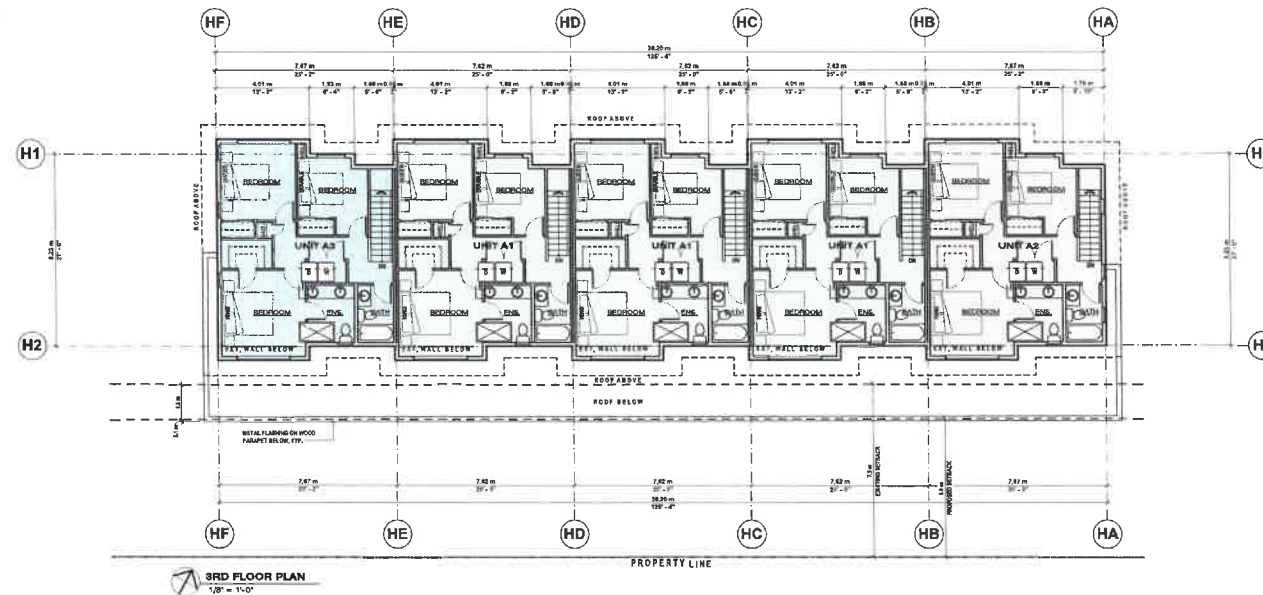
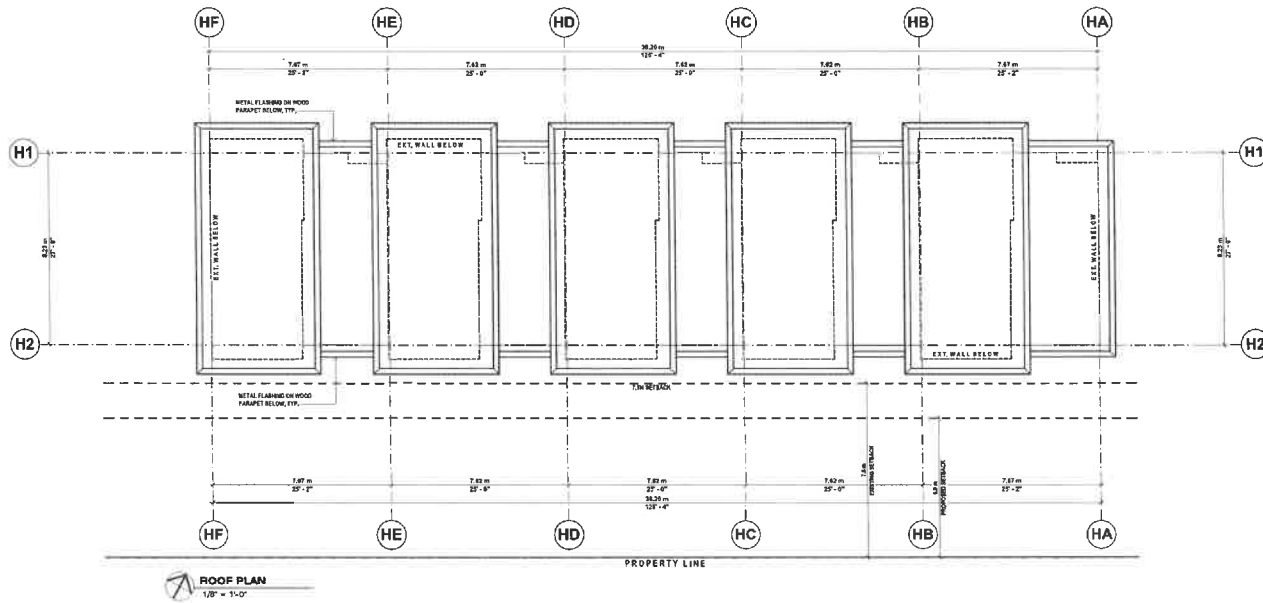
FLOOR PLANS

DP2.2-H

SCALE

1/8" = 1'-0"

142

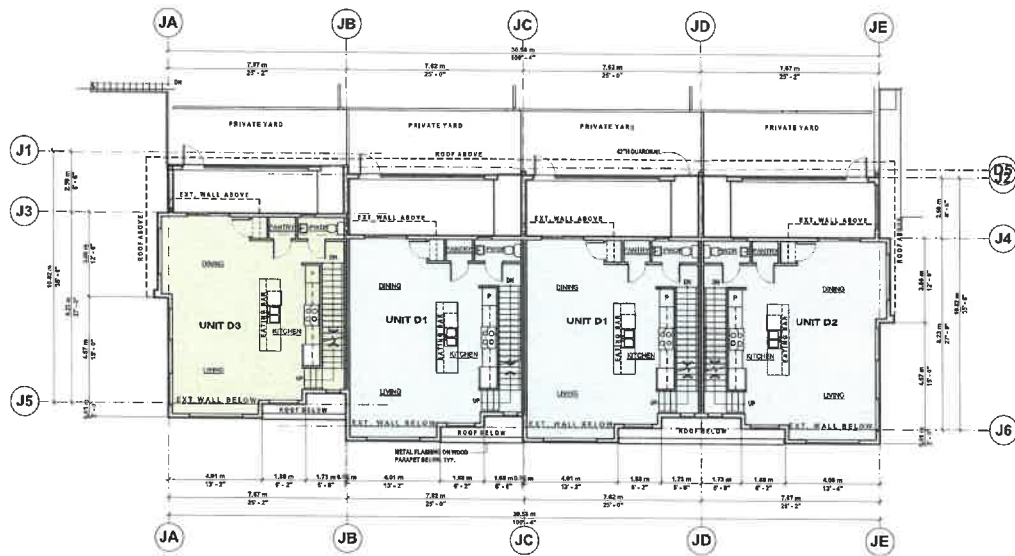




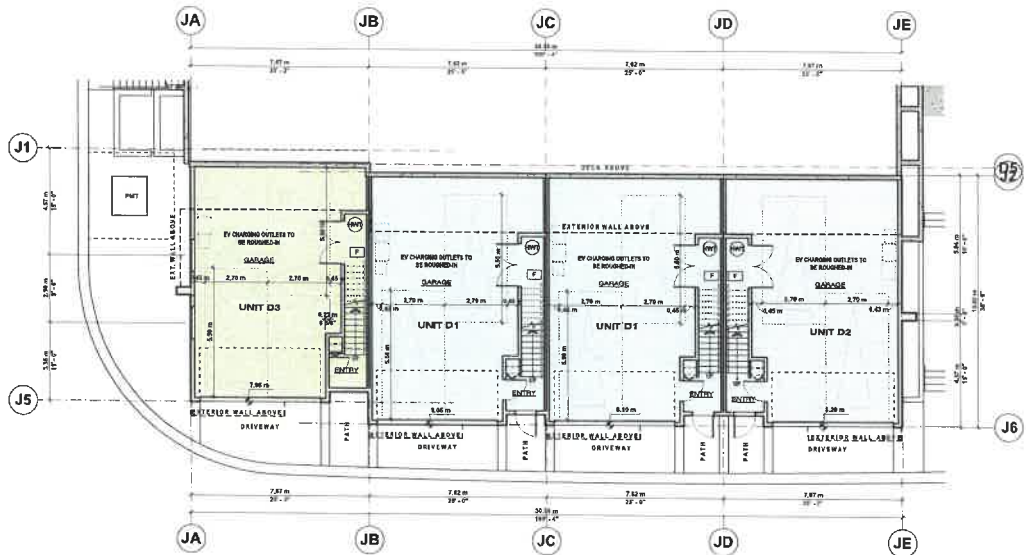
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2ND FLOOR PLAN
1/8" = 1'-0"



1ST FLOOR PLAN
1/8" = 1'-0"

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE

2024.02.08

JOB NO.

2301



NO.	DATE	DESCRIPTION
A	2024.02.08	DP REVISION
B	2024.04.04	DP REVISION
C	2024.04.09	DP REVISION

FLOOR PLANS

DP2.1-J

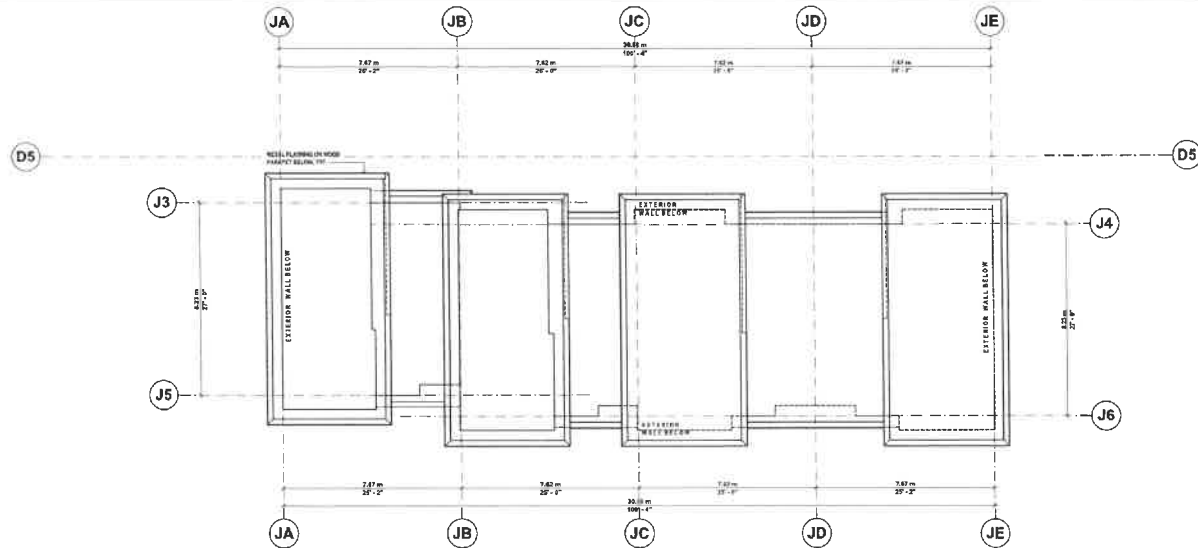
SCALE
1/8" = 1'-0"

143



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ROOF PLAN
1/8" = 1'-0"



3RD FLOOR PLAN
1/8" = 1'-0"

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE
2024-03-08
JOB NO.
2301



NO.	DATE	DESCRIPTION
A	2024-03-08	DP REVISION
B	2024-04-04	DP REVISION
C	2024-04-26	DP REVISION

FLOOR PLANS

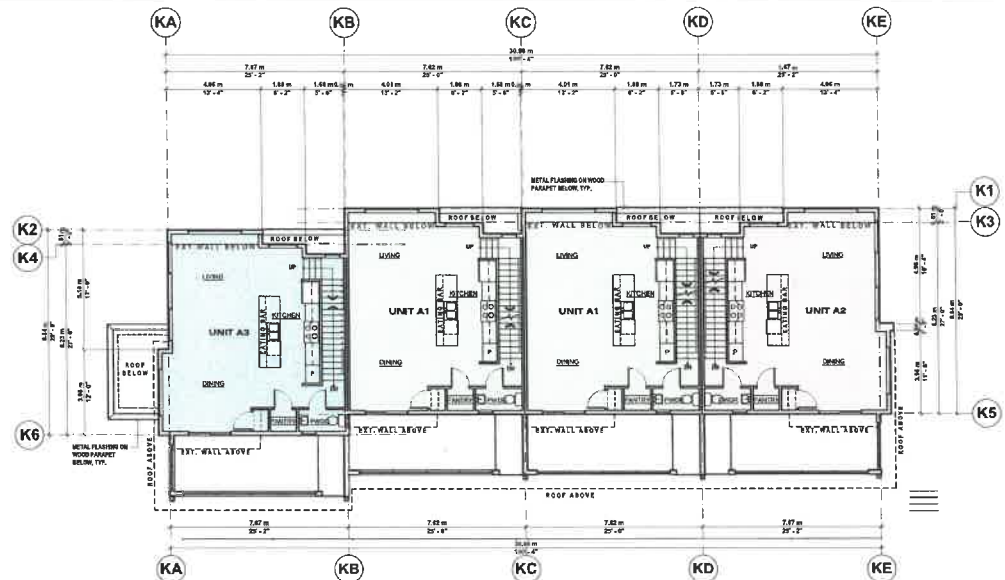
DP2.2-J

SCALE
1/8" = 1'-0"
144

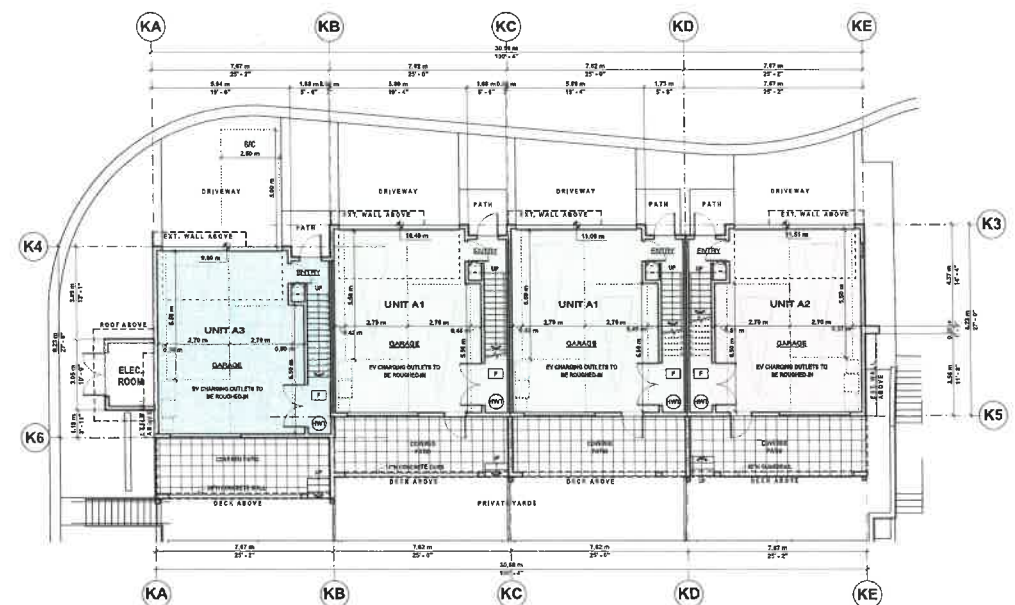


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2ND FLOOR PLAN
1/8" = 1'-0"



1ST FLOOR PLAN
1/8" = 1'-0"

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DATE: 2024.02.05
JOB NO: 2301



NO.	DATE	DESCRIPTION
A	2024.02.05	DP REVISION
B	2024.04.04	DP REVISION
C	2024.04.29	DP REVISION

FLOOR PLANS

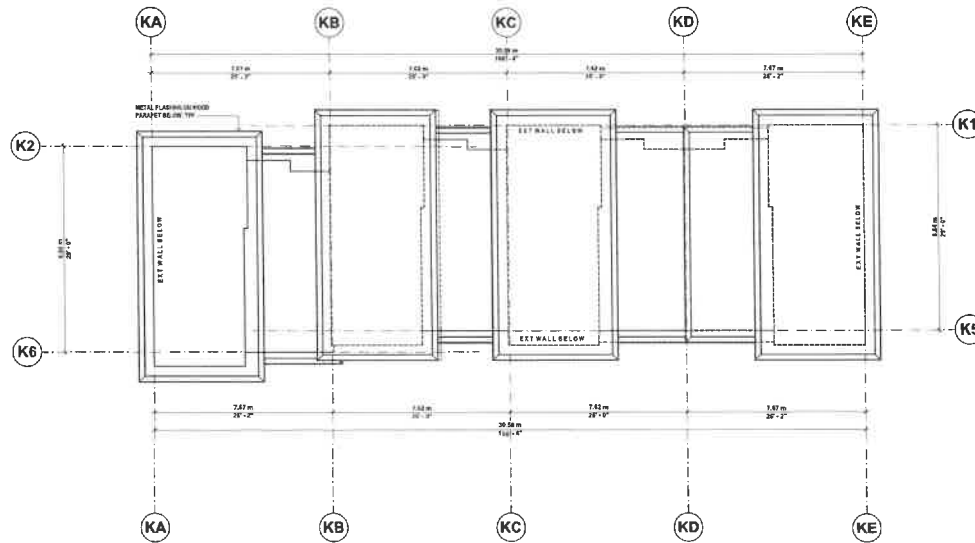
DP2.1-K

SCALE: 1/8" = 1'-0"
145

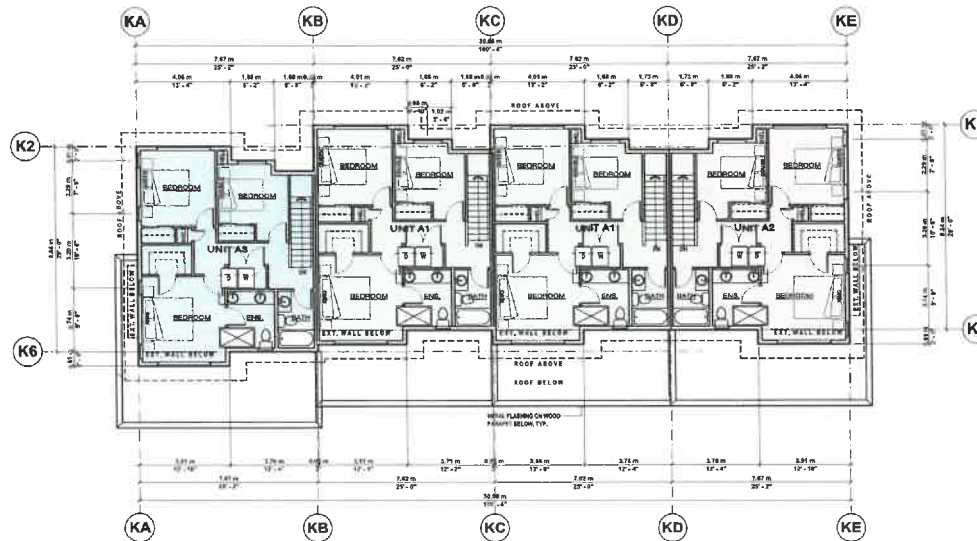


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ROOF PLAN
1/8" = 1'-0"



3RD FLOOR PLAN
1/8" = 1'-0"

GREYSTONE

MARY HILL BYPASS, PITT
RIVER RD & HARBOUR ST,
PORT COQUITLAM, BC

DATE

2024.01.08

JOB NO.

2501

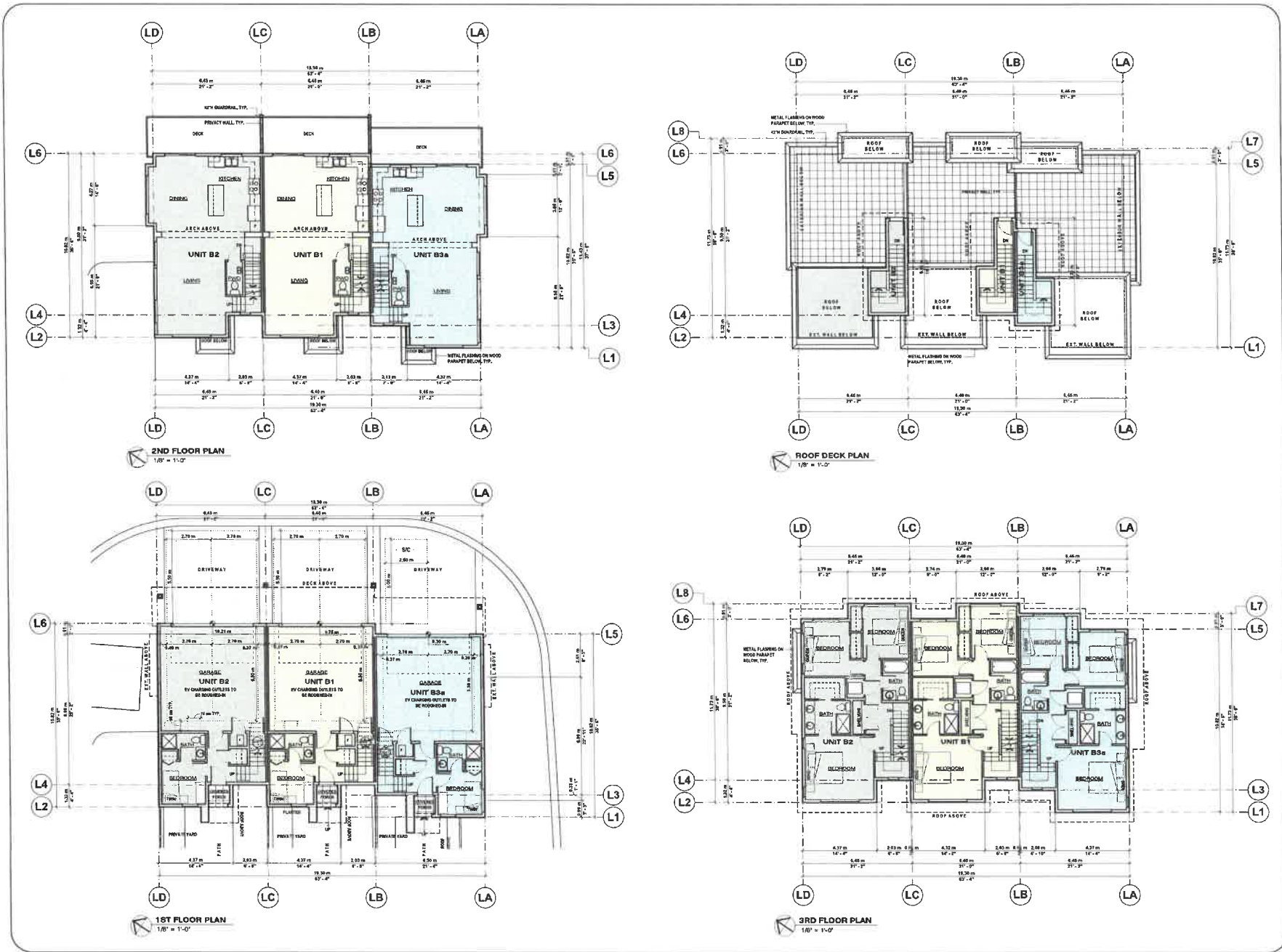


REVISIONS	NO.	DATE	DESCRIPTION
A	2024.02.08	DP REVISION	
B	2024.04.04	DP REVISION	
C	2024.04.29	DP REVISION	

FLOOR PLANS

DP2.2-K

SCALE
1/8" = 1'-0"



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MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE: 2024-02-26
JOB NO: 2301



REVISION	NO.	DATE	DESCRIPTION
A	2024-02-06	CP	REVISION
B	2024-04-04	CP	REVISION
C	2024-04-29	CP	REVISION

FLOOR PLANS

DP2.1-L

SCALE: 1/8" = 1'-0"
147



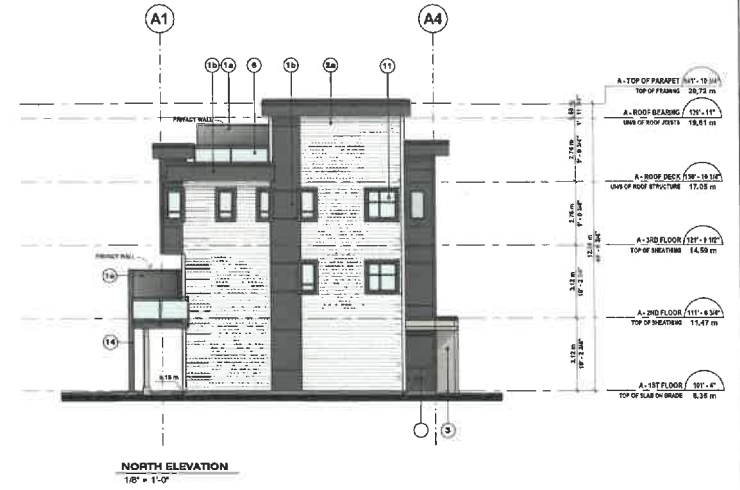
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WEST ELEVATION
1/8" = 1'-0"



NORTH ELEVATION
1/8" = 1'-0"



EAST ELEVATION
1/8" = 1'-0"



SOUTH ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES

1 - WALL, PAINTED CONCRETE/CMU PANELS	6 - GLAZING, CLASS FRAME MOUNT	12 - CAST-IN-PLACE CONCRETE
2 - BEAM/RAIN COUPLER, 2X11 SHIP-LY WHITE	7 - PREFABRICATED ALUMINUM - VESTER ALL-WINDOW BLACK	13 - METAL STUD
3 - BEAM/RAIN COUPLER, 2X14 SHIP-LY IRON	8 - GLAZING, ACROST FRAME MOUNT	14 - PAINTED TO MATCH CMU
4 - WALL, PAINTED CONCRETE/CMU PANEL	9 - PREFABRICATED ALUMINUM - VESTER ALL-WINDOW BLACK	15 - WOODWORK
5 - WALL, WOODWORK TRUSSEY/DEEP CONCRETE/CMU PANEL	10 - TRIM, PAINTED TO MATCH WINDOW COUPLER TRIM & FINISH, TO MATCH CMU	16 - WOODWORK
6 - WALL, WOODWORK TRUSSEY/DEEP CONCRETE/CMU PANEL	11 - GLAZING, WOOD TRIM	17 - WOODWORK
7 - WALL, WOODWORK TRUSSEY/DEEP CONCRETE/CMU PANEL	12 - EXTERIOR FACTORY BLACK INTERIOR FACTORY WHITE	18 - WOODWORK
8 - WALL, WOODWORK TRUSSEY/DEEP CONCRETE/CMU PANEL	13 - GLAZING, ALL WEATHER OCK GLAZING	19 - WOODWORK
9 - WALL, WOODWORK TRUSSEY/DEEP CONCRETE/CMU PANEL	14 - FACTORY BLACK FRAME	20 - WOODWORK
10 - PARAPET CAPLINING - ALUMINUM		21 - WOODWORK
11 - FACTORY BLACK		22 - WOODWORK
		23 - WOODWORK

NOTES

1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED
2. METRIC GRADE ELEVATIONS ARE ACTUAL AND GEODETIC
3. MAIN FLOOR GEODETIC GRADE ELEVATIONS MUST BE COMPARED WITH CITY DRAWINGS PRIOR TO EXCAVATION

GREYSTONE
MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE
2024.02.06

JOB NO.
2301



REVISIONS

NO.	DATE	DESCRIPTION
A	2024.03.04	CP REVISION
B	2024.04.04	CP REVISION
C	2024.04.29	CP REVISION

ELEVATIONS

DP3.1-A

SCALE
As Indicated



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GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE
2024.08.08
JOB NO.
2301



REVISION	NO.	DATE	DESCRIPTION
A	2024.08.08	DP REVISION	
B	2024.08.04	DP REVISION	
C	2024.04.20	DP REVISION	

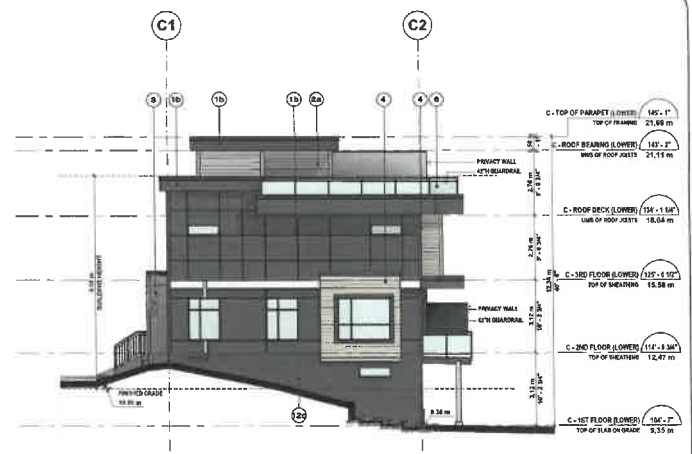
ELEVATIONS

DP3.1-C

SCALE
As Indicated



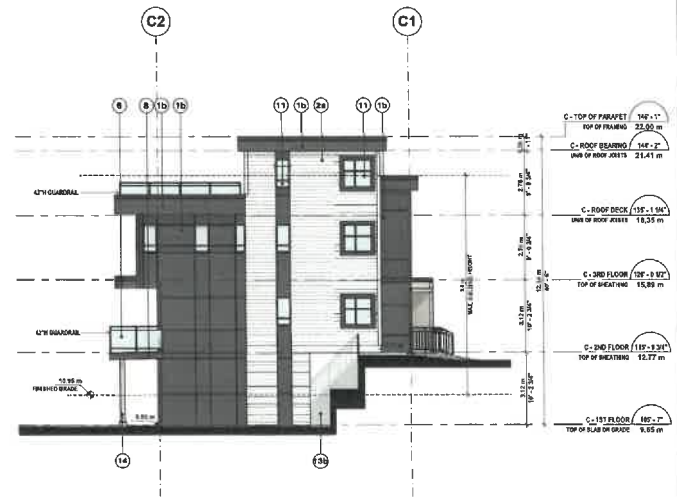
NORTH ELEVATION
1/8" = 1'-0"



WEST ELEVATION
1/8" = 1'-0"



SOUTH ELEVATION
1/8" = 1'-0"



EAST ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES

1 - WALL - PAINTED CONCRETE PANELS	6 - QUADRANT - BRASS FACE MOUNT	13 - CAST-IN-PLACE CONCRETE
2 - WALL - BRICK	7 - PRE-FINISHED ALUMINUM - WHITE ALUMINUM - BLACK	14 - NATURAL BRICK
3 - WALL - PAINTED CONCRETE LAP	8 - QUADRANT - BRASS FACE MOUNT	15 - PAINTED TO MATCH BRICK (SEE FINISH SHEET)
4 - WALL - WOODSTONE TRUSSENER'S CONCRETE/STAIN PAINT	9 - PRE-FINISHED ALUMINUM - GREY ALUMINUM - BLACK	16 - PAINTED TO MATCH BRICK (SEE FINISH SHEET)
5 - WALL - WOODSTONE TRUSSENER'S CONCRETE/STAIN PAINT	10 - TRIM - PAINTED LAMINATE, CORNER TRIM & PASSEL TO MATCH AS SHOWN	17 - TRUSS COLLING
6 - WALL - WOODSTONE TRUSSENER'S CONCRETE/STAIN PAINT	11 - GLAZING - NYLON TRIM	18 - FRAMED TO MATCH WOODSTONE LARD CASTLE
7 - WALL - WOODSTONE TRUSSENER'S CONCRETE/STAIN PAINT	12 - GLAZING - ALL WEATHER ROCK GLAZING	19 - METAL ROFFIT - FACTORY WHITE
8 - WALL - WOODSTONE TRUSSENER'S CONCRETE/STAIN PAINT	13 - FACTORY CAP LINING - ALUMINUM	
9 - WALL - WOODSTONE TRUSSENER'S CONCRETE/STAIN PAINT	14 - FACTORY BLACK	

NOTES
1. UPPER FLOOR GRADE ELEVATIONS ARE ASSUMED
2. METRIC GRADE ELEVATIONS ARE ACTUAL AND DECIDUOUS
3. MAIN FLOOR GEODETIC GRADE ELEVATIONS MUST BE CONFIRMED WITH CAME DRAWINGS PRIOR TO EXCAVATION

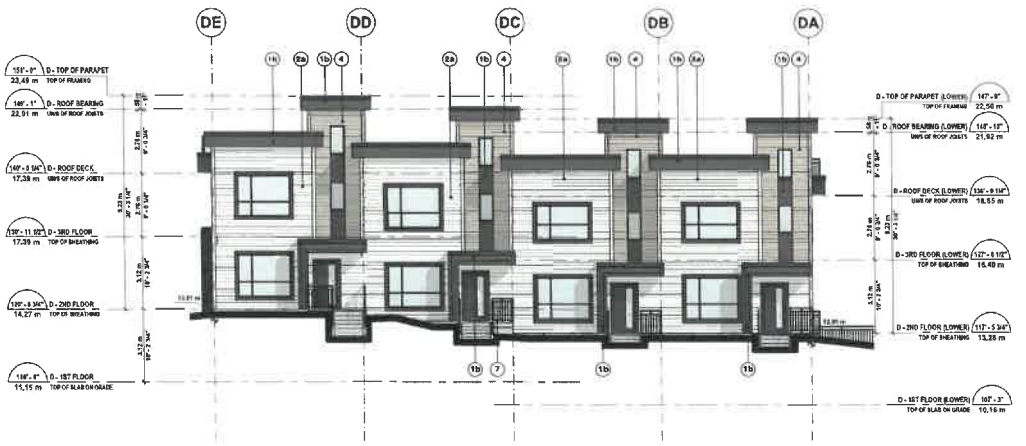


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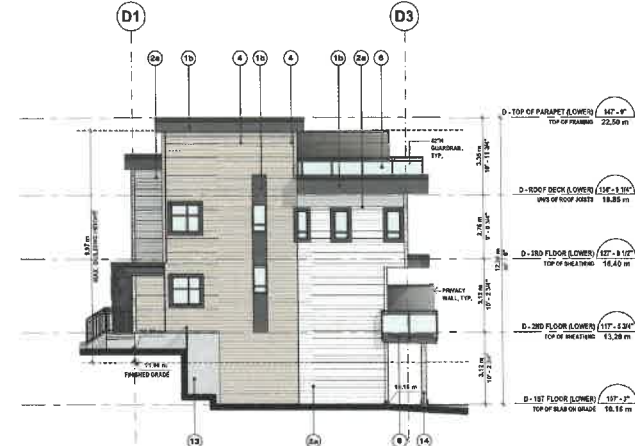
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NORTH ELEVATION

1/8" = 1'-0"



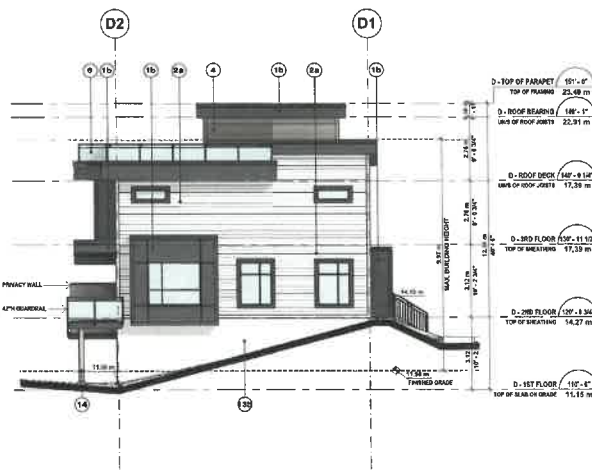
WEST ELEVATION

1/8" = 1'-0"



SOUTH ELEVATION

1/8" = 1'-0"



EAST ELEVATION

1/8" = 1'-0"

EXTERIOR FINISHES

1 WALL - FINISHED CONCRETE/CMU PANEL - 4" BENCH MARK, CC-117 SIMPLY WHITE - 2" BENCH MARK, 2" x 4" WOOD STUD BOND	4 - SHAKESIDE - GLASS FACE UP - PREPARED ALUMINUM - GENERIC ALUMINUM BLACK	11 - 4" x 8" IN PLACE CONCRETE - 4" NATURAL FINISH
2 WALL - FINISHED CONCRETE/CMU PANEL - 4" BENCH MARK, CC-117 SIMPLY WHITE - 2" BENCH MARK, 2" x 4" WOOD STUD BOND	7 - SHAKESIDE - PICKET FACE UP - PREPARED ALUMINUM - GENERIC ALUMINUM BLACK	12 - 4" x 8" IN PLACE CONCRETE - 4" NATURAL FINISH
3 WALL - WOODSIDE FINISHED CONCRETE/CMU PANEL - 4" BENCH MARK, CC-117 SIMPLY WHITE - 2" BENCH MARK, 2" x 4" WOOD STUD BOND	8 - TRIM - PAINTED BY BOHANNAN, CORNER TRIM & FASCIA - TO MATCH AS SHOWN	13 - TRIMMER COLUMN - STAINED TO MATCH WOODSIDE BENCH CASTLE
4 WALL - WOODSIDE FINISHED CONCRETE/CMU PANEL - 4" BENCH MARK, CC-117 SIMPLY WHITE - 2" BENCH MARK, 2" x 4" WOOD STUD BOND	11 - GLAZING - VINYL TRIM - EXTERIOR FACTORY BLACK & INTERIOR FACTORY WHITE	14 - METAL SLOPE - FACTORY WHITE
5 - PARAPET CAP/FLASHING - ALUMINUM - FACTORY BLACK	12 - GLAZING - ALL WEATHER DECK GLAZING - FACTORY BLACK FINISH	

NOTES
1 IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED
2 METRIC GRADE ELEVATIONS ARE ACTUAL AND GEODETIC
3 MAIN FLOOR GEODETIC GRADE ELEVATIONS MUST BE CORRELATED WITH CIVIL DRAWINGS PRIOR TO EXCAVATION

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE

2024-02-28

JOB NO.

2301



REVISIONS	NO.	DATE	DESCRIPTION
A	2024-02-28	DP	PRELIMINARY
B	2024-04-26	DP	REVISION
C	2024-04-29	DP	REVISION

ELEVATIONS

DP3.1-D

SCALE
AS NOTED

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GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE: 2024.02.08
JOB NO: 2301



NO.	DATE	DESCRIPTION
A	2024.02.08	DP REVISION
B	2024.04.04	DP REVISION
C	2024.04.29	DP REVISION

ELEVATIONS

DP3.1-E

SCALE: As Shown

NORTH ELEVATION
1/8" = 1'-0"

EAST ELEVATION
1/8" = 1'-0"

SOUTH ELEVATION
1/8" = 1'-0"

WEST ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES

1 - WALL: PAINTED CEMENTitious PANELS - 4" BRANDED MOORE, 50-115 BRILL WHITE	4 - SHADOWLINE, GLASS FACE MOIST - PREFABRICATED ALUMINUM, SICKER ALUMINUM, BLACK	13 - GEAR-PLATE CONCRETE - BRICKWORK, FROM - PAINTED TO MATCH BRU, 06-117 BRILL WHITE
2 - WALL: PAINTED CEMENTitious LAP - 4" BRANDED MOORE, 50-115 BRILL WHITE	5 - SHADOWLINE, FRICKS FACE MOIST - PREFABRICATED ALUMINUM, SICKER ALUMINUM, BLACK	14 - WOODEN COLUMN - FRAMES TO MATCH WOODWORK BANG CASTLE
3 - WALL: WOODSTONE MULTICOLOR CEMENTitious PANEL - 4" BRAND CASTLE	6 - FLOOR: PAINTED BY BECKHOFF, CONCRETE FIN & FACED - TO MATCH AS SHOWN	15 - METAL SLOPE - FACTORY WHITE
4 - WALL: WOODSTONE MULTICOLOR CEMENTitious LAP - 4" BRAND CASTLE	7 - GLAZING: VINYL TRIM - SYSTEM: FACTORY BLACK & INTERIOR FACTORY WHITE	NOTES
5 - PARAPET CAPSLING: ALUMINUM - FACTORY BLACK	8 - GLAZING: ALL WEATHER DECK GLAZING - FACTORY BLACK FRAME	1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED
		2. METRIC GRADE ELEVATIONS ARE ACTUAL, AND METRIC IS
		3. MAIN FLOOR GEODESIC GRADE ELEVATIONS MUST BE
		CONFIRMED WITH CAL. DRAWINGS PRIOR TO EXCAVATION



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WEST ELEVATION
1/8" = 1'-0"



NORTH ELEVATION
1/8" = 1'-0"



EAST ELEVATION
1/8" = 1'-0"



SOUTH ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES

1 - WALL - PAINTED CERAMITIC TILE PANELS - BELLARMI MOORE 300-117 TRIPLE WHITE - BELLARMI MOORE 310-111 WOODGATE BROWN	2 - WALL - PAINTED CERAMITIC TILE LAP - BELLARMI MOORE 300-117 TRIPLE WHITE - BELLARMI MOORE 310-111 WOODGATE BROWN	3 - WALL - WOODGATE WOODCERAMIC CERAMITIC PANEL - ALABASTRO GABLE	4 - WALL - WOODGATE WOODCERAMIC CERAMITIC LAP - ALABASTRO GABLE	5 - PARAPET CAP/PANNING - ALUMINUM - FACTORY BLACK	6 - ALUMINUM - BLADE FACE MOUNT - PRE-FABRICATED ALUMINUM - GUNITE ALUMINUM BLACK	7 - GLASS/CLAD - PIVOT FACE MOUNT - PRE-FABRICATED ALUMINUM - GUNITE ALUMINUM BLACK	8 - FRESH PAINTED CE ROUGH, QUADRA TRIM & FASDA - TO MATCH AS SHOWN	9 - GLASS - WOOD TRIM - EXTENSION FACTORY BLACK SATELITE FACTORY WHITE	10 - GLASS - ALL WEATHER DECK GLAZING - FACTORY BLACK/FRAME	11 - GAST IMP-LACE CONCRETE - 6" MINIMUM THICK - PAINTED TO MATCH BRN DR-117 IMPLY WHITE - EXPANDED TO MATCH BRN DR-117 BRN PRODUCTIVE	12 - JEWEL'S COLUMN - STAINED TO MATCH WOODSTONE SAND GABLE - TO MATCH AS SHOWN	13 - JETAL BOPPE - FACTORY WHITE
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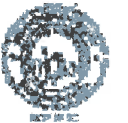
NOTES

1. IMPEDIAL FLOOR GRADE ELEVATIONS ARE ASSUMED
2. METRIC GROUP FINISHES ARE ACTUAL AND LEGGOTTO
3. MAIN FLOOR SPECIFIC GRADE ELEVATIONS MUST BE CORRELATED WITH CIVIL DRAWINGS PRIOR TO PROCAUTION

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE 2024.04.04
JOB NO. 2301



REVISIONS

NO.	DATE	DESCRIPTION
A	2024.03.29	DP REVISION
B	2024.04.04	DP REVISION
C	2024.04.26	DP REVISION

ELEVATIONS

DP3.1-F

SCALE As Indicated



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NORTH ELEVATION
1/8" = 1'-0"

EAST ELEVATION
1/8" = 1'-0"



SOUTH ELEVATION
1/8" = 1'-0"

WEST ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES

1 - WALL - PAINTED CONCRETE/CMU PANELS	6 - BRICKMASONRY (24x18) IMPER WHITE	11 - GLAZING - VINYL TRIM	16 - CLADDING - ALL WEATHER DECK CLADDING - FACTORY BLACK FINISH
2 - WALL - PAINTED CONCRETE/CMU LAP	7 - CLADDING - HORIZONTAL WOOD FACE WOOD	12 - EXTERIOR FACTORY BLACK & WHITE FACTORY WHITE	
3 - WALL - WOODSTONE "MULTIGRAPHY" CEMENTitious PANEL, 48" HIG CHAIRS	8 - BRICKMASONRY (24x18) IMPER WHITE	13 - GLAZING - ALL WEATHER DECK GLAZING - FACTORY BLACK FINISH	
4 - WALL - WOODSTONE "MULTIGRAPHY" CEMENTitious PANEL, 48" HIG CHAIRS	9 - BRICKMASONRY (24x18) WOODGRAN BROWN		
5 - BRICKMASONRY (24x18) WOODGRAN BROWN	10 - BRICKMASONRY (24x18) IMPER WHITE		
	14 - CORNER COLUMN - STAINED TO MATCH WOODSTONE BANG CHAIRS		
	15 - METAL DOOR/FIT - FACTORY WHITE		

NOTES

1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED
2. METRIC GRADE ELEVATIONS ARE ACTUAL AND GEODETIC
3. FINISH FLOOR GEODETIC GRADE ELEVATIONS MUST BE COMPARED WITH THE CIVIL DRAWINGS PRIOR TO EXCAVATION

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE
2024 02 04
JOB NO.
2301



REVISIONS

NO.	DATE	DESCRIPTION
A	2024-02-04	DP REVISION
B	2024-04-04	DP REVISION
C	2024-04-28	DP REVISION

ELEVATIONS

DP3.1-G

SCALE
AS INDICATED



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DATE: 2024.02.26
JOB NO: 2301



REVISIONS	NO.	DATE	DESCRIPTION
A	2024.02.04	DP	REVISION
B	2024.04.04	DP	REVISION
C	2024.04.29	DP	REVISION

ELEVATIONS

DP3.1-H

SCALE: As Indicated

EXTERIOR FINISHES		
1 WALL - PAINTED CONCRETE/STUCCO PANELS a) BRILLIANT MOON, 50-15 BROWN TRAILS b) BRILLIANT MOON, 210-11 WINDMILL IRON	4 GLAZING - REAR FACE MOUNT - PREFABRICATED ALUMINUM - BRACKET ALUMINUM BLACK	10 ACORN-PLATE CONCRETE a) BRILLIANT MOON b) PAINTED TO MATCH BRILLIANT MOON, 210-11 WINDMILL IRON c) PAINTED TO MATCH BRILLIANT MOON, 210-11 WINDMILL IRON
2 WALL - PAINTED CONCRETE/STUCCO LAP a) BRILLIANT MOON, 50-15 BROWN TRAILS b) BRILLIANT MOON, 210-11 WINDMILL IRON	7 GLAZING - REAR FACE MOUNT - PREFABRICATED ALUMINUM - BRACKET ALUMINUM BLACK	14 TRIMMER COLUMN STAINED TO MATCH WOODSTONE SAND CASTLE
3 WALL - WOODSTONE WOOD/CLAY/CEMENT/STUCCO PANEL a) SAND CASTLE	8 GLAZING - WHITE TRIM - EXTERIOR FACTORY BLACK & INTERIOR FACTORY WHITE	15 METAL ROOFING - FACTORY WHITE
4 WALL - WOODSTONE WOOD/CLAY/CEMENT/STUCCO LAP a) SAND CASTLE	9 GLAZING - ALL WEATHER DECK GLAZING - FACTORY BLACK PAINT	
5 - PREFABT CAP/CLADDING - ALUMINUM FACTORY BLACK		

- NOTES**
1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED
 2. METRIC GRADE ELEVATIONS ARE ACTUAL AND GEODETIC
 3. MAIN FLOOR GEODETIC GRADE ELEVATIONS MUST BE CONFIRMED WITH CIVIL DRAWINGS PRIOR TO EXCAVATION

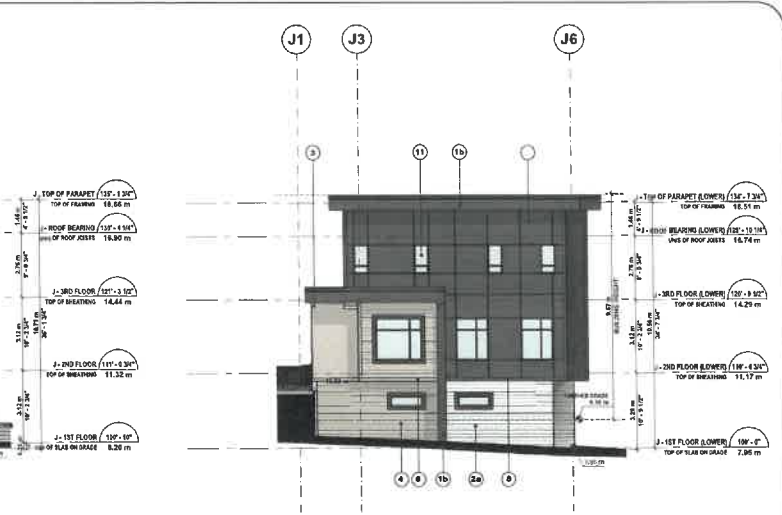


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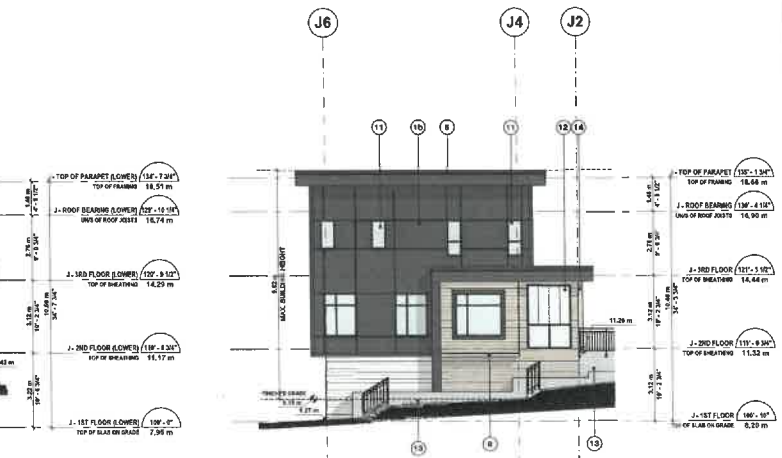
SOUTH ELEVATION
1/8" = 1'-0"



WEST ELEVATION
1/8" = 1'-0"



NORTH ELEVATION
1/8" = 1'-0"



EAST ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES			
1	WALL - PAINTED CONCRETE/STUCCO PANELS - BELMONT MOORE 204-11 BUNKY WHITE	13	CAST-IN-PLACE CONCRETE
2	WALL - PAINTED CONCRETE/STUCCO LAP - BELMONT MOORE 204-11 BUNKY WHITE	14	CONCRETE COLUMNS - STAINLESS STEEL BRACKETED TO MATCH HOISTING
3	WALL - WOODSHING MULTICOLORED CONCRETE/STUCCO PANEL - BARK CASTLE	15	METAL ROOFING - FACTORY WHITE
4	WALL - WOODSHING MULTICOLORED CONCRETE/STUCCO LAP - BARK CASTLE	6	QUADRANGLE GLASS FAÇADE MOUNT - TREATMENT ALUMINUM - GREY/TEAL ALUMINUM, BLACK
5	PANTRY COUNTERS/ISLAND - ALUMINUM - FACTORY BLACK	7	QUADRANGLE PICKET FAÇADE MOUNT - TREATMENT ALUMINUM - GREY/TEAL ALUMINUM, BLACK
8	CEILING - WOODSHING MULTICOLORED CONCRETE/STUCCO PANEL - BARK CASTLE	8	QUADRANGLE PICKET FAÇADE MOUNT - TREATMENT ALUMINUM - GREY/TEAL ALUMINUM, BLACK
9	CEILING - WOODSHING MULTICOLORED CONCRETE/STUCCO LAP - BARK CASTLE	9	TIMBER PAINTED (SEE WORKING, CORNER TRIM & PARADEA, TO MATCH AS SHOWN)
10	CEILING - WOODSHING MULTICOLORED CONCRETE/STUCCO PANEL - BARK CASTLE	10	CEILING - WOODSHING MULTICOLORED CONCRETE/STUCCO PANEL - BARK CASTLE
11	CEILING - WOODSHING MULTICOLORED CONCRETE/STUCCO LAP - BARK CASTLE	11	CEILING - WOODSHING MULTICOLORED CONCRETE/STUCCO LAP - BARK CASTLE
12	CEILING - WOODSHING MULTICOLORED CONCRETE/STUCCO PANEL - BARK CASTLE	12	CEILING - WOODSHING MULTICOLORED CONCRETE/STUCCO PANEL - BARK CASTLE

NOTES
1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED
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3. MAIN FLOOR GEODETIC GRADE ELEVATIONS MUST BE CONFIRMED WITH CIVIL DRAWINGS PRIOR TO EXCAVATION

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE
2024.03.08

JOB NO.
2301



REV	DATE	DESCRIPTION
A	2024.03.08	TOP REVISION
B	2024.04.04	DP REVISION
C	2024.04.29	DP REVISION

ELEVATIONS

DP3.1-J

SCALE
As noted



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DATE

2024-02-08

JOB NO.

2301



REVISIONS	NO.	DATE	DESCRIPTION
A	2024-02-08	DP	REVISION
B	2024-04-04	DP	REVISION
C	2024-04-29	DP	REVISION

ELEVATIONS

DP3.1-K

SCALE
As Indicated

EXTERIOR FINISHES

1 - WALL - PAINTED CONCRETE/STUCCO PANELS - BRILLIANT WHITE, 50-117 BRILLIANT WHITE - MOUNTAIN MOORE, 210-117 MOUNTAIN PINK	4 - QUADRANT - GLASS FACE WOOD - PARAFACATED ALUMINUM - WHITE/ALUMINUM OR BLACK	13 - GARB INFLATERS CONCRETE - BRILLIANT PINK
2 - WALL - PAINTED CONCRETE/STUCCO LAP - MOUNTAIN MOORE, 50-117 BRILLIANT WHITE - MOUNTAIN MOORE, 210-117 MOUNTAIN PINK	7 - GUARDRAIL - PICKET FENCE BOARD - PARAFACATED ALUMINUM - WHITE/ALUMINUM OR BLACK	14 - TRIMMED COLUMN - BEAMED TO MATCH WOODTRIM SAND CASTLE
3 - WALL - WOODTRIM "MULTICOLOUR" CONCRETE/STUCCO PANEL - SAND CASTLE	8 - TRIM - PAINTED OR BURNED, CHERRY TRIM & FASOLA - TO MATCH AS SHOWN	15 - METAL DOORS - FACTORY WHITE
9 - WALL - WOODTRIM "MULTICOLOUR" CONCRETE/STUCCO LAP - SAND CASTLE	11 - BLINDS - WHITE TRIM - EXTERIOR FACTORY BLACK EXTERIOR FACTORY WHITE	NOTES
5 - PARAPET CAP/FLASHING - ALUMINUM - 5111111 BLACK	12 - BLINDS - ALL WEATHER DECK GLASS - FACTORY BLACK FRAME	1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED
		2. METRIC GRADE ELEVATIONS ARE ACTUAL AND GEOMETRIC
		3. MAIN FLOOR DECK TO GRADE ELEVATIONS MUST BE CONFIRMED WITH CIVIL DRAWINGS PRIOR TO EXCAVATION

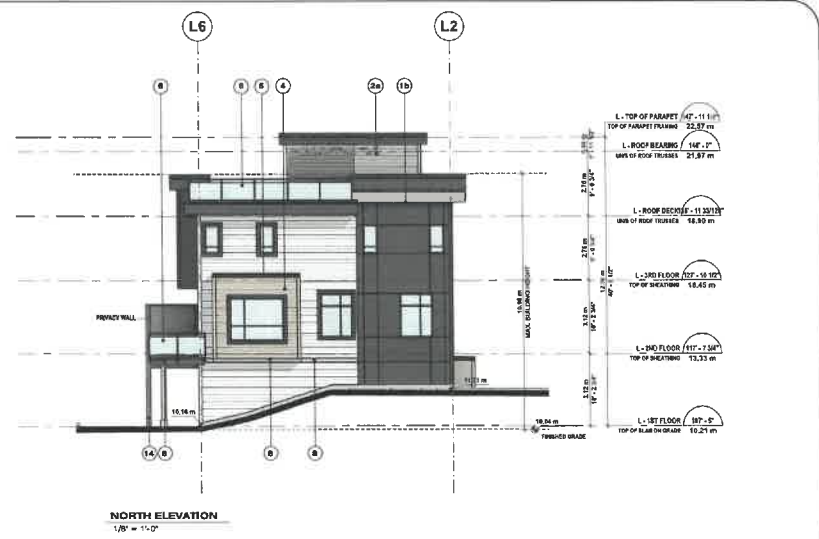


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APPROVALS.



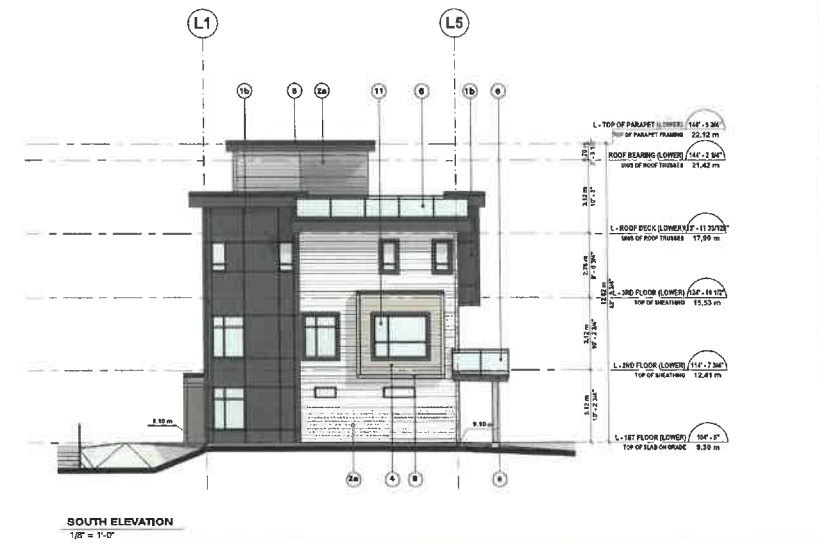
WEST ELEVATION
1/8" = 1'-0"



NORTH ELEVATION
1/8" = 1'-0"



EAST ELEVATION
1/8" = 1'-0"



SOUTH ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES		
1 WALL - PAINTED CONCRETE/STUCCO PANELS IN RELIABLE MOORE, 2024-11 BROWN 1 WHITE	4 - CLADDING - BLAZE FACE MOUNT - PREFABRICATED ALUMINUM - GAULTY ALUMINUM, BLACK	13 GABRIEL PLAZA CONCRETE - ENGRAVED FINISH - PAINTED TO MATCH BLZ 00-111 BROWN 1 WHITE - PAINTED TO MATCH BLZ 0214 BROWNING BROWN
2 WALL - PAINTED CONCRETE/STUCCO LAP IN RELIABLE MOORE, 2024-11 BROWN 1 WHITE	7 - CLADDING - PICKET FACE MOUNT - PREFABRICATED ALUMINUM - GAULTY ALUMINUM, BLACK	14 - TRIMMER COLUMN - STAINED TO MATCH WOODSTONE SAND CASTLE
3 WALL - WOODSTONE MULTICOLOUR CONCRETE/STUCCO PANEL IN SAND CASTLE	8 - TRIM - PAINTED 2X MORGAN, CORNER TRIM & FASCIA: TO MATCH 04 BROWN	15 - METAL SHOFFY - FACTORY WHITE
4 WALL - WOODSTONE MULTICOLOUR CONCRETE/STUCCO LAP IN SAND CASTLE	9 - CLADDING - WOOD TRIM - EXTERIOR FACTORY BLACK / INTERIOR FACTORY WHITE	
5 - PARAPET OVERLAPPING - JUMBO/OF FACTORY BLACK	11 - CLADDING - ALL WEATHER DECK GLAZING - FACTORY BLACK FRAME	

NOTES
1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED.
2. METRIC GRADE ELEVATIONS ARE ACTUAL AND DEODIC.
3. MAIN FLOOR DEODIC GRADE ELEVATIONS MUST BE
CORRIFIED WITH CIVIL DRAWINGS PRIOR TO EXCAVATION.

GREYSTONE

MARY HILL BYPASS, PITT
RIVER RD & HARBOUR ST,
PORT COQUITLAM, BC

DATE
2024.02.06
JOB NO.
2301



REVISIONS	NO	DATE	DESCRIPTION
A	2024.02.06	CP PREVISION	
B	2024.04.04	CP PREVISION	
C	2024.04.28	CP PREVISION	

ELEVATIONS

DP3.1-L

SCALE
AS SHOWN

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LANDSCAPE ARCHITECTS
Suite C160 - 4185 Sisk Creek Drive
Burnaby, British Columbia, V5C 9C9
p: 604 294-0011 | f: 604 294-0022

SEAL:

NO.	DATE	REVISION DESCRIPTION	DR.
1	24 MAR 16	CITY COMMUNITY DEVELOPMENT PLAN	BT
2	24 FEB 16	YARD UPDATED	BT
3	24 MAR 16	REVISED PLAN	BT
4	24 JUN 16	SKETCH TO GP	BT

CLIENT:

PROJECT:
43 UNIT TOWNHOUSE DEVELOPMENT
PRINCE STREET AND PITT RIVER ROAD
PORT COQUITLAM, BC

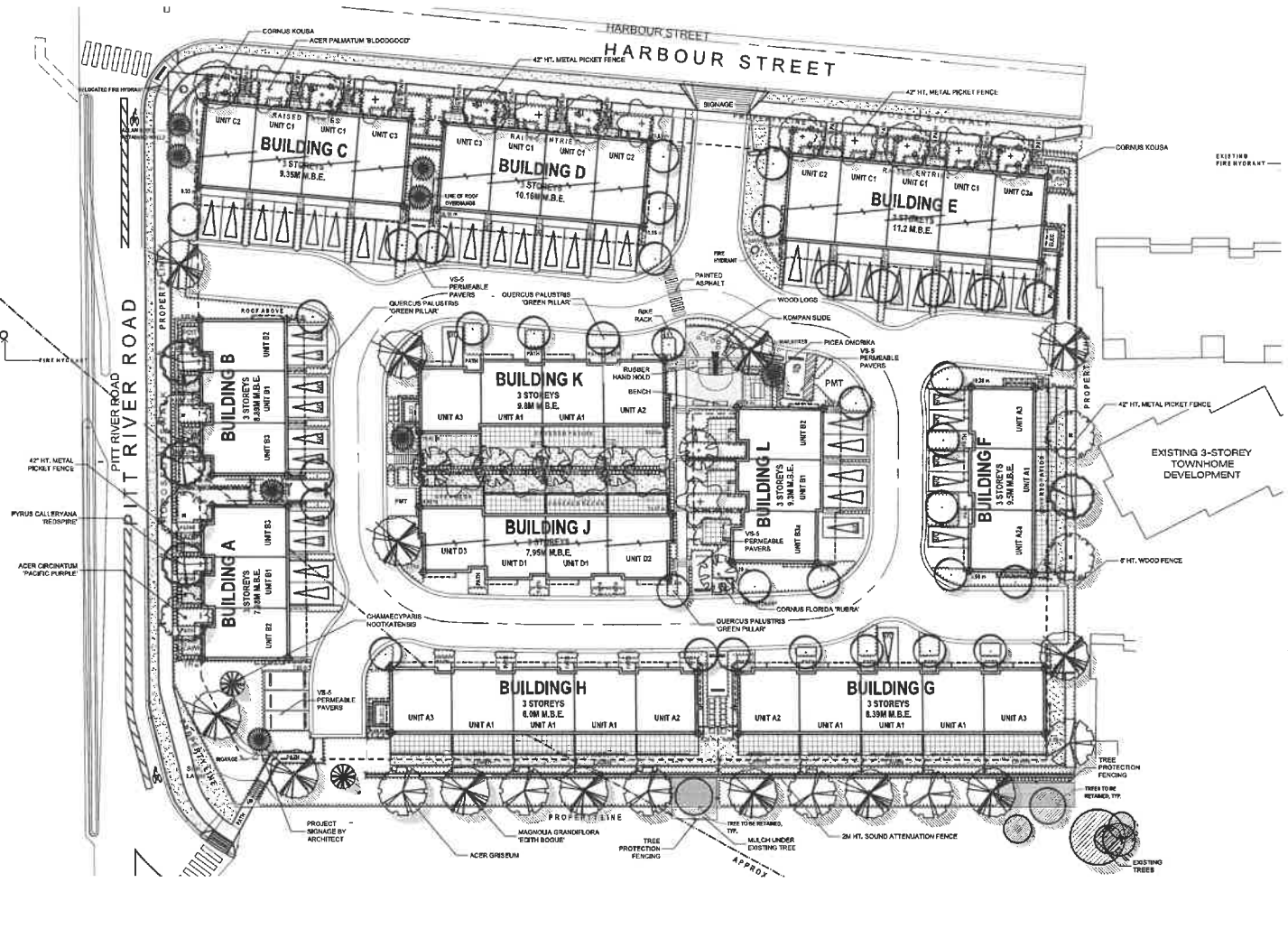
DRAWING TITLE:
LANDSCAPE PLAN

DATE: 24 MAR 16 DRAWING NUMBER:
SCALE: 1:250
DRAWN: RJ
DESIGN: RJ
CHK'D: YR

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OF 7

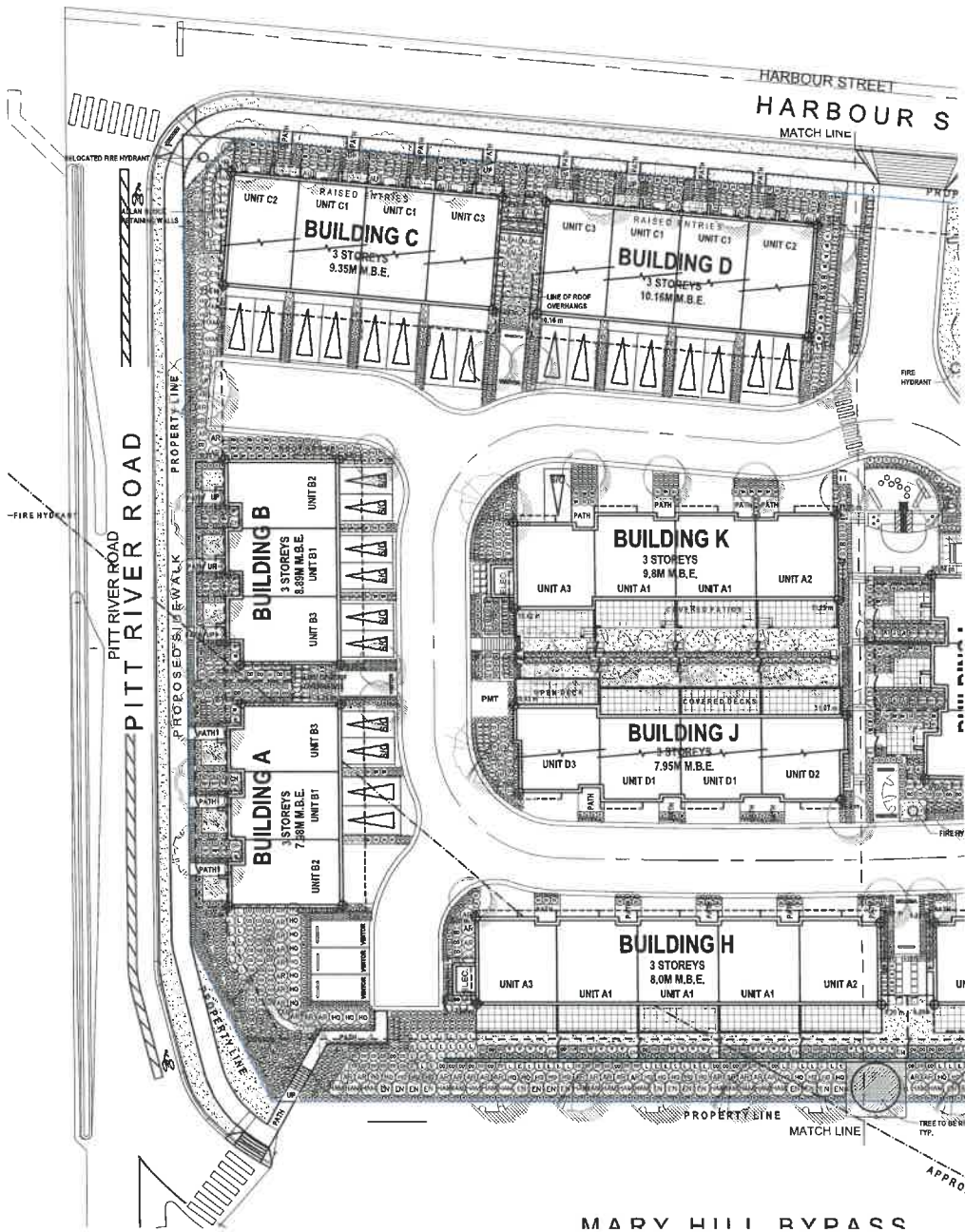
33313-2 2/P PMG PROJECT NUMBER 23-232 159



KEY	QTY	BOTANICAL NAME	COMMON NAME	PLANTED SIZE / REMARKS
5		ACER ORINATUM 'PACIFIC PURPLE'	Bronze Vine Maple	2.5M HT, BAB, 3 STEM CLUMP
6		ACER GRISBAUM	Paperbark Maple	90CM CAL, 1.0M STD, BAB
11		ACER PALMATUM 'BLOODGOOD'	Red Japanese Maple	2.5M HT, BAB, UPRIGHT FORM
2		CHAMAECYPARIS NODIKATENSIS	Noddy Cypress	2M HT, BAB
8		CORNUS FLORIDA 'RUBRA'	Pink Flowering Dogwood	60CM CAL, 1.5M STD, BAB
7		CORNUS KOUSA	Kousa Dogwood	2M HT, BUSH FORM, BAB
13		MAGNOLIA GRANDIFLORA 'EDITH BOGUE'	Edith Bogue Southern Magnolia	2M HT, BAB
10		PICOLA CHAENOKIA	Siberian Spruce	2M HT, BAB
4		PIRUS CALLERYANA 'REDSPRICE'	Ornamental Pear	90CM CAL, 1.5M STD, BAB
36		QUERCUS PALUSTRIS 'GREEN PILLAR'	Green Pillar Pin Oak	8CM CAL, 1.5M STD, BAB

NOTES: * PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION. CONTAINER SIZES SPECIFIED AS PER ORCA STANDARD. BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES. * REF TO DISCUSSIONS FOR DEFINED CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS. * SEARCH AND REVIEW: MAKE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY. AREA OF SEARCH TO INCLUDE LOWER MAINLAND AND FRASER VALLEY. * SUBSTITUTIONS: * OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO MAKING ANY SUBSTITUTIONS. TO THE SPECIFIED MATERIAL. UNAPPROVED SUBSTITUTIONS WILL BE REJECTED. ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REVIEW TO SUBSTITUTE. SUBSTITUTIONS ARE SUBJECT TO BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION. * ALL AVAILABILITY. * ALL LANDSCAPE MATERIAL AND WORKMANSHIP MUST MEET OR EXCEED BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD AUTHORIZED BY LANDSCAPE ARCHITECT.

NOTES: #1 REPLACEMENT TREES REQUIRED, #1 REPLACEMENT TREES PROVIDED.
ALL SOFT LANDSCAPE AREAS TO BE IRRIGATED WITH A HIGH EFFICIENCY AUTOMATIC IRRIGATION SYSTEM, INSTALLATION TO ULLA, B.C. STANDARDS, LATEST EDITION.



PLANT SCHEDULE

KEY	QTY	BOTANICAL NAME	COMMON NAME	PLANTED SIZE / REMARKS
56		ARBUTUS UNEDO 'COMPACTA'	COMPACT STRAWBERRY BUSH	43 POT, 80CM
44		AKUBIA JAPONICA 'TERRATIFOLIUM'	RAMBOOTHED JAPANESE AKUBIA	43 POT, 80CM
26		AZALEA JAPONICA 'DORADO'S GEMSON'	AZALEA, PINK/BISS-BRED	43 POT, 80CM
141		BUXUS MICROPHYLLA 'WINTER GEM'	LITTLE LEAF BOX	43 POT, 80CM
14		CHOREYA TERENATA 'SUNSHINE'	MORNING BLOSSOM ORANGE	43 POT, 80CM
252		CORNUS BENICOLA	DOGWOOD	43 POT, 80CM
22		ENKANTHUS CAMPANULATUS	ENKANTHUS	43 POT, 10CM
27		FANFARISEA MOLLIS	CHINESE WITCH HAZEL	43 POT, 80CM
80		HYDRANGEA QUERCIFOLIA	OAKLEAF HYDRANGEA	43 POT, 80CM
26		KALAMA LATIFOLIA 'ELF'	DWARF MOUNTAIN LAUREL	43 POT, 80CM
4		RHOODOENDRON 'ROMA BELL'	RHOODOENDRON	43 POT, 80CM
178		RHOODOENDRON 'P. J. M.'	RHOODOENDRON; LIGHT PURPLE, E. MAY	43 POT, 80CM
20		ROSA MIDLAND 'EDWICK'	BUNGALOW ROSE	42 POT, 80CM
138		ROSA MIDLAND 'TREV'	MIDLAND ROSE; RED	42 POT, 80CM
265		SHIMADA JAPONICA (DOR. MALE)	JAPANESE SHIMADA	42 POT, 80CM
223		SPYRACA JAPONICA 'LITTLE PRINCESS'	LITTLE PRINCESS SPYRACA; PINK	42 POT, 80CM
840		TAXUS X MEDIA 'HELIX'	HILLS YEW	1.5M 840
64		WEIGELA FLORIDA 'PURPUREA'	PURPLE WEIGELA	43 POT, 80CM
881		CAREX FLAEGELLIFERA 'NOVI'	RIVER WEEPING SEDGE	41 POT
333		CAREX OSHIMENIS 'EVERGOLD'	EVERGOLD JAPANESE SEDGE	41 POT
105		PENSTEMON ALPINO 'CROCUS'S LITTLE BUNNY'	FOUNTAIN GRASS	41 POT
78		HELLEBORUS x HYBRIDUS	LUTEA ROSE	1.5CM POT
113		HOSTA 'PATRIOT'	HOSTA, GREEN AND WHITE VARIEGATED	41 POT, 1 EYE
119		LAVENDULA ANGSTROFOLIA 'MUNSTEAD'	ENGLISH LAVENDER, COMPACT, VIOLET-BLUE	41 POT
397		LIRODIUM 'RUSCARI'	BLUE LILY-TURTLE	41 POT
86		LOMONDIA PILEATA	PRIVET HONEYBUCKLE	42 POT, 25CM
161		POLYSTICHUM MANIFLUM	WESTERN SWORD FERN	41 POT, 25CM

NOTES: PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION. CONTAINER SIZES SPECIFIED AS PER CMAA STANDARD. BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES. REFER TO SPECIFICATIONS FOR DETAIL OF CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS. SEARCH AND REVIEW MAKE UP MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY. AREA OF SEARCH TO INCLUDE LOWER MAINLAND AND FRASER VALLEY. SUBSTITUTIONS OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO WORKING. ANY SUBSTITUTIONS TO THE SPECIFIED MATERIAL, UNAPPROVED SUBSTITUTIONS WILL BE REJECTED. ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REQUEST TO SUBSTITUTE. SUBSTITUTIONS ARE SUBJECT TO BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD. DEFINITION OF CONDITIONS OF AVAILABILITY. ALL LANDSCAPE MATERIAL AND WORKMANSHIP MUST MEET OR EXCEED BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD LATEST EDITION. ALL PLANT MATERIAL MUST BE PROVIDED FROM CERTIFIED DISEASE FREE NURSERY. BONGOLDS NOT PERMITTED IN GROWING MEDIUM UNLESS AUTHORIZED BY LANDSCAPE ARCHITECT.

NOTE: ALL SOFT LANDSCAPE AREAS TO BE IRRIGATED WITH A HIGH EFFICIENCY AUTOMATIC IRRIGATION SYSTEM, INSTALLATION TO I.L.A.R.C. STANDARDS, LATEST EDITION.

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p: 604 294-0011 f: 604 294-0622

SEAL:

NO.	DATE	REVISION DESCRIPTION	DR.
1	24 MAR 16	CITY COMMENTS/NEW SITE PLAN	AJ
2	24 FEB 06	HAND DRAFTED	AJ
3	16 MAR 05	REVISION PLAN	RE
4	24 MAR 14	SCALE OF 1/8" = 1'-0"	AJ

CLIENT:

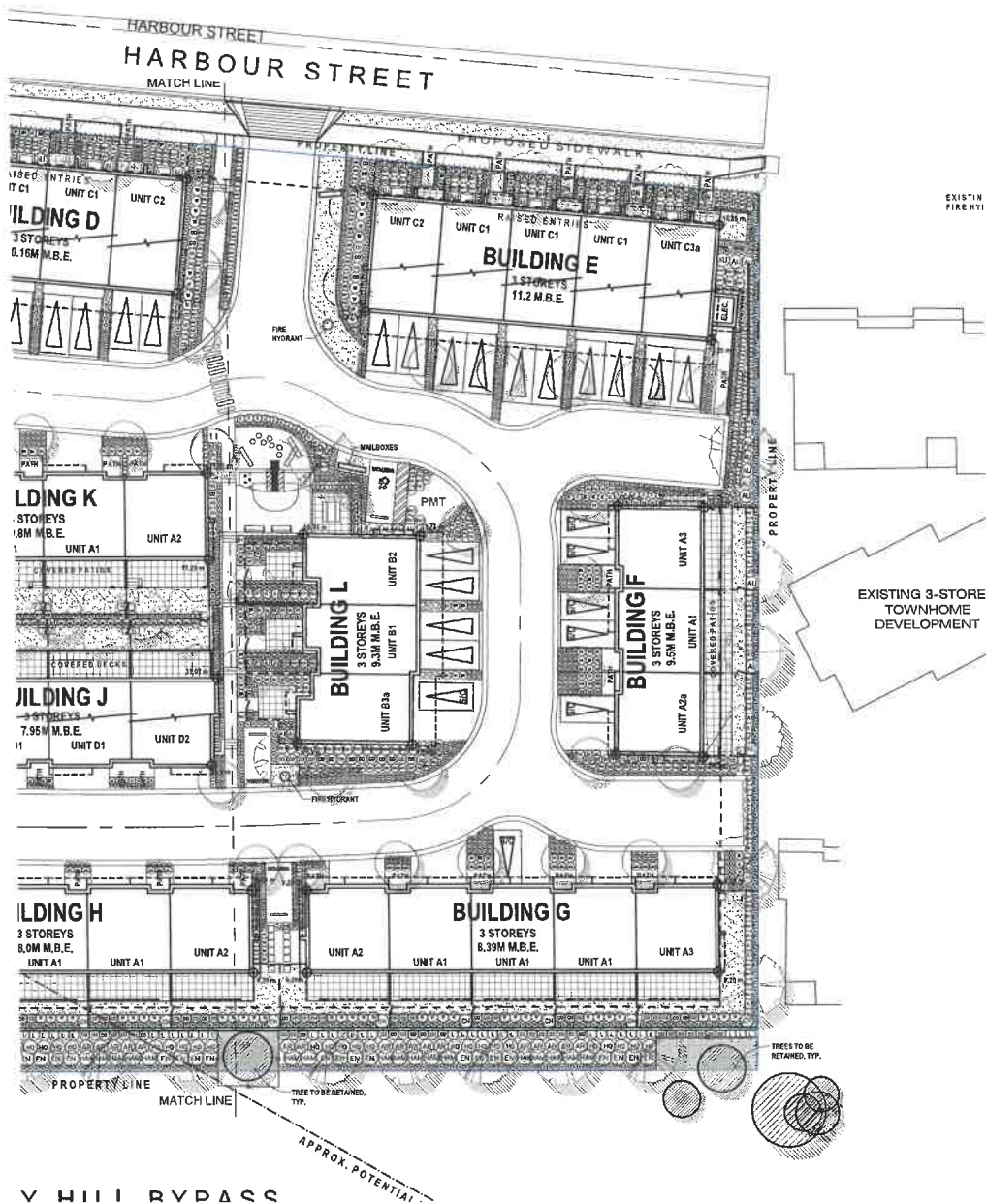
PROJECT:
43 UNIT TOWNHOUSE DEVELOPMENT
PRINCE STREET AND PITT RIVER ROAD
PORT COQUITLAM, BC

DRAWING TITLE:
SHRUB PLAN

DATE: 24 JAN 16 DRAWING NUMBER:
SCALE: 1:300 **L2**
DRAWN: RJ
DESIGN: RJ
CHECK: VR QF 7

PMG PROJECT NUMBER: 23-232 190





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SEAL

NO.	DATE	REVISION DESCRIPTION	DR.
1	24 MAR 20	ISSUED FOR PERMITS	RI
2	24 FEB 20	ISSUED FOR PERMITS	RI
3	24 JAN 24	ISSUED FOR PERMITS	RI

NO.	DATE	REVISION DESCRIPTION	DR.
1	24 MAR 20	ISSUED FOR PERMITS	RI
2	24 FEB 20	ISSUED FOR PERMITS	RI
3	24 JAN 24	ISSUED FOR PERMITS	RI

PROJECT:
43 UNIT TOWNHOUSE DEVELOPMENT
PRINCE STREET AND PITT RIVER ROAD
PORT COQUITLAM, BC

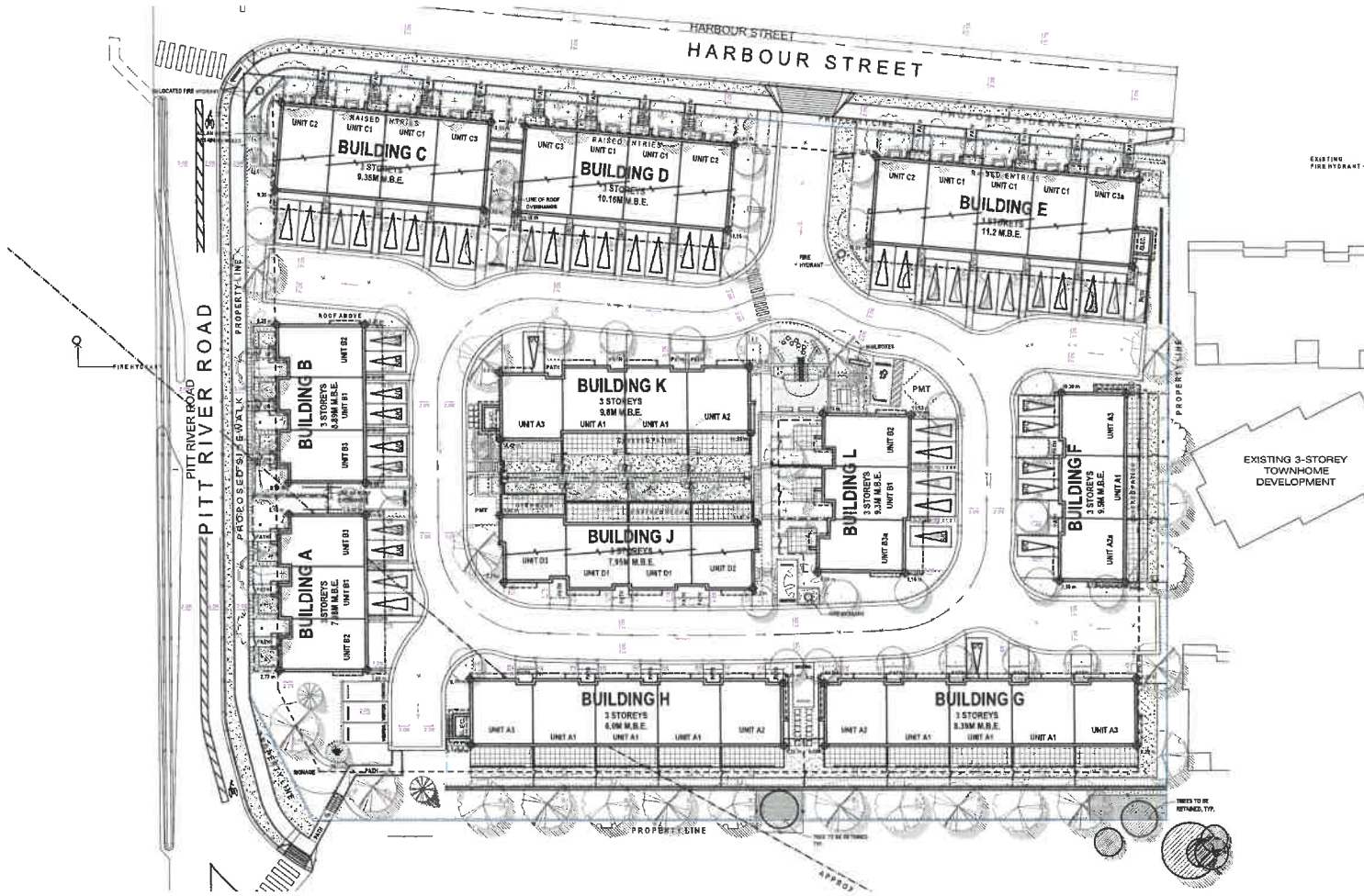
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DATE: 26 JAN 16
SCALE: 1:200
DRAWN: RI
DESIGN: RI
CHKD: YR

L3

OF 7

PMG PROJECT NUMBER: 23-232
161



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 p: 604 294-0011 | f: 604 294-0022

SEAL:

NO.	DATE	REVISION DESCRIPTION	DR.
1	24 JAN 24	ISSUE FOR DP	AR
2	24 JAN 24	REVISED PLAN	PM
3	24 FEB 09	WARD UPGRADE	AR
4	24 MAR 24	CITY COMMENTS/PLAN WITH PLAN	AR

NO.	DATE	REVISION DESCRIPTION	DR.
1	24 JAN 24	ISSUE FOR DP	AR
2	24 JAN 24	REVISED PLAN	PM
3	24 FEB 09	WARD UPGRADE	AR
4	24 MAR 24	CITY COMMENTS/PLAN WITH PLAN	AR

CLIENT:

PROJECT:
43 UNIT TOWNHOUSE DEVELOPMENT

PRINCE STREET AND PITT RIVER ROAD
 PORT COQUITLAM, BC

DRAWING TITLE:
LANDSCAPE GRADING

DATE: 24 JAN 15 DRAWING NUMBER:
 SCALE: 1:250
 DRAWN: RJ
 DESIGN: RJ
 CHCKD: YR

L4

OF 7

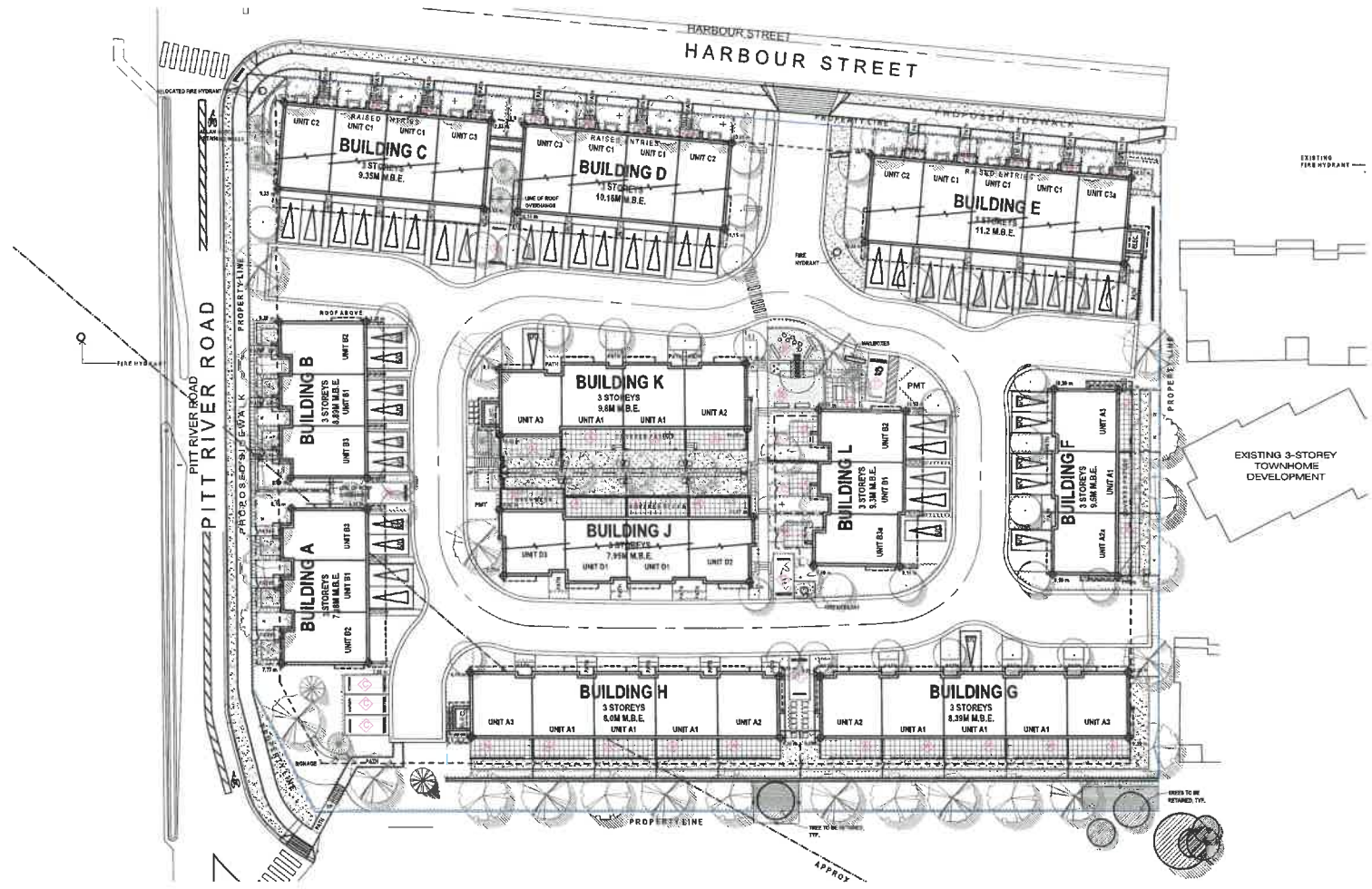
PMG PROJECT NUMBER: 23-232 162



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Burnaby, British Columbia, V5C 6G8
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SEAL



MATERIAL KEYS:

	PAVER: BELGARD CORTAZ SLAB 24" X 24" COLOUR: GRANITE
	FBAR PLAY SURFACE
	CONCRETE

FURNITURE KEYS:

	KOUMAR PGM110121 EMBA HANOUT BENCH COLOUR: MEDIUM GREEN
	MAGLIN M.B.M76 BENCH SERIES M.B.B.TO P.C(2)
	WOOD LOGS AND STEPPERS
	MAGLIN 500 SERIES BIKE RACKS
	WELBIONE BAYVIEW PICNIC TABLE
	MARATHON SURFACES RUBBER HAND HOLDS

NOTE: ANY SUBSTITUTE OF PLAY EQUIPMENT OR FURNITURE NEEDS TO BE REVIEWED/APPROVED BY LANDSCAPE ARCHITECT.



NO.	DATE	REVISION DESCRIPTION	DR.
4	18 MAR 24	CITY COMMENTS/REVISION PLAN	RJ
3	24 FEB 24	WAD UPDATED	RJ
2	24 MAR 24	REVISION PLAN	RM
1	24 MAR 24	ISSUED FOR CP	RJ

PROJECT:
43 UNIT TOWNHOUSE DEVELOPMENT

PRINCE STREET AND PITT RIVER ROAD
PORT COQUITLAM, BC

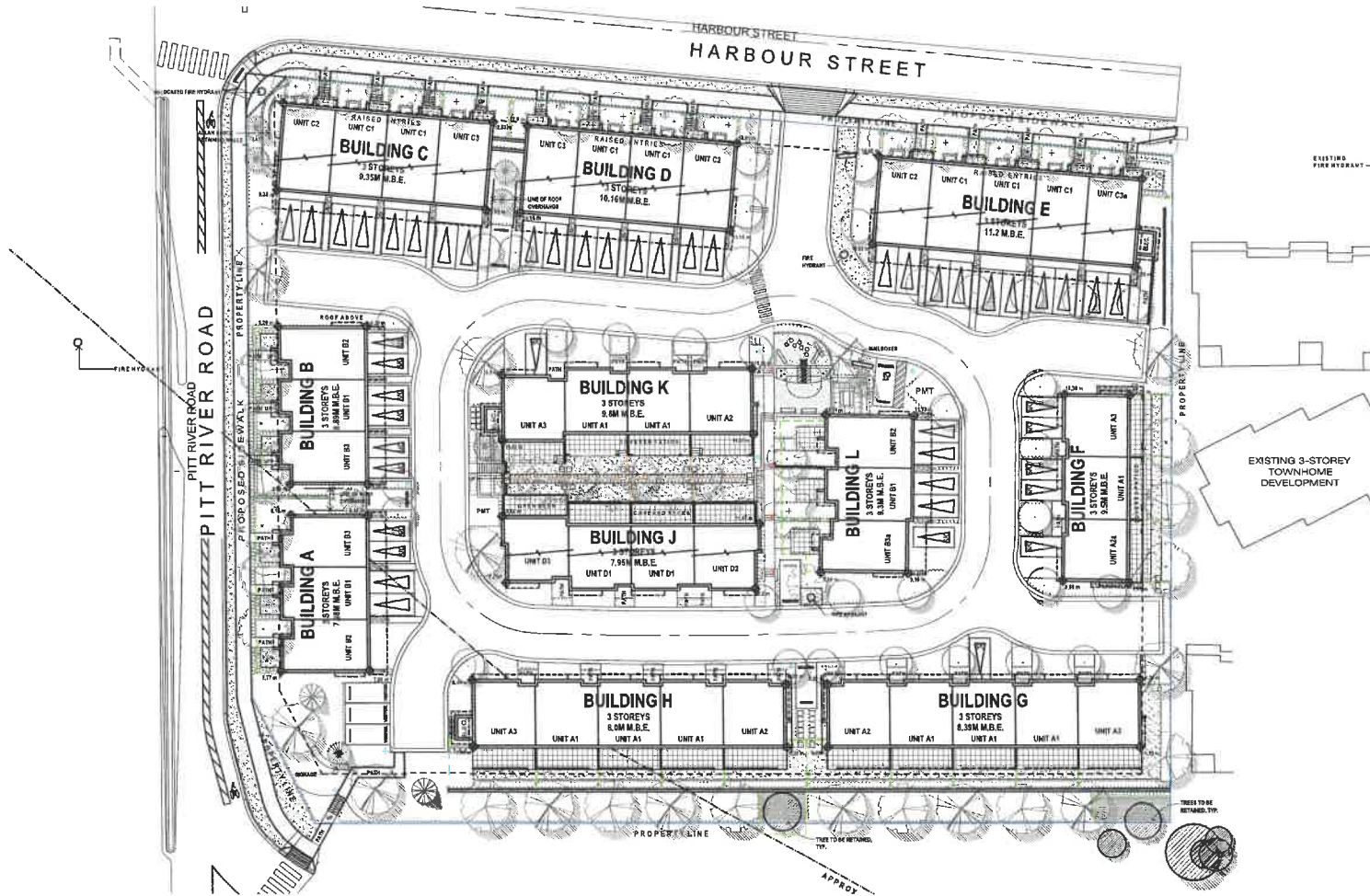
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MATERIAL PLAN

DATE: 24 JAN 24 DRAWING NUMBER:
SCALE: 1:250
DRAWN: RJ
DESIGN: RJ
CHD: YR

L5

OF 7

23292-2 2/P PMG PROJECT NUMBER: 23-232 103



LIGHT LEGEND

	BOLLARDS WITH LIGHTS HAND RAILS WITH LAMP UNIT BELOW
	STEP LIGHT & WALL LIGHT LEAVING ROAD & LIGHT BELOW

NOTE: LIGHTING BRACKET/LOCATION REFER TO ELECTRICAL DWG.

FENCE LEGEND

	40' HT. METAL PICKET FENCE
	30' HT. ALUMINUM FENCE AS PER MANUFACTURER REPORT
	6' HT. WOOD FENCE
	2' HT. SECURITY FENCE MODEL # P95038



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ARCHITECTS
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Burnaby, British Columbia, V5C 0G9
p: 604 294-0011 : f: 604 294-0022

SEAL:

NO.	DATE	REVISION DESCRIPTION	DR.

NO.	DATE	REVISION DESCRIPTION	DR.
4	24 MAR 25	CITY COMMENTS/NEW SET PLAN	AJ
3	24 FEB 05	ISSUED FOR PERMIT	AJ
2	16 JAN 25	ISSUED FOR PERMIT	TR
1	24 JAN 24	ISSUED FOR PERMIT	AJ

CLIENT:

PROJECT:
43 UNIT TOWNHOUSE DEVELOPMENT
PRINCE STREET AND PITT RIVER ROAD
PORT COQUITLAM, BC

DRAWING TITLE:
FENCING & LIGHTING PLAN

DATE: 24 JAN 26 DRAWING NUMBER:
SCALE: 1:250
DRAWN: RJ **L6**
DESIGN: RJ
CHKD: YR OF 7

23232-22P PMG PROJECT NUMBER 23-232 164

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P: 604 294-0011 F: 604 294-0022

SEAL

NO.	DATE	REVISION DESCRIPTION	DR.
1	24 MAR 16	CITY COMMENT RESPONSE SET PLAN	RM
2	30 FEB 16	FINAL LAYOUT	AJ
3	24 MAR 16	REV LAYOUT	RM
4	10 FEB 16	REVISED FOR DP	AJ

NO.	DATE	REVISION DESCRIPTION	DR.
1	24 MAR 16	CITY COMMENT RESPONSE SET PLAN	RM
2	30 FEB 16	FINAL LAYOUT	AJ
3	24 MAR 16	REV LAYOUT	RM
4	10 FEB 16	REVISED FOR DP	AJ

NO.	DATE	REVISION DESCRIPTION	DR.
1	24 MAR 16	CITY COMMENT RESPONSE SET PLAN	RM
2	30 FEB 16	FINAL LAYOUT	AJ
3	24 MAR 16	REV LAYOUT	RM
4	10 FEB 16	REVISED FOR DP	AJ

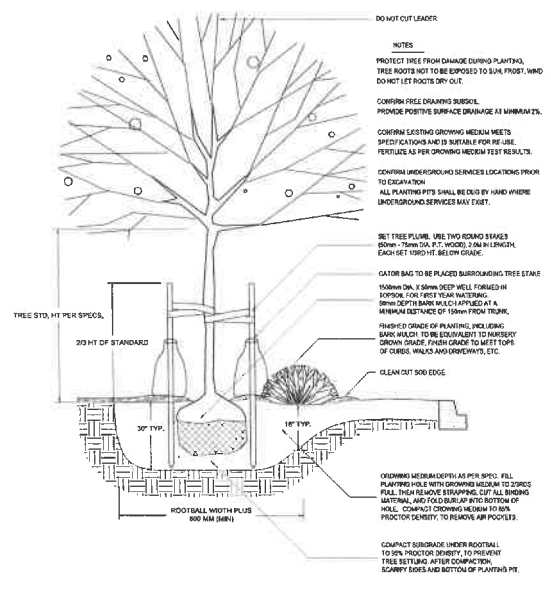
PROJECT:
43 UNIT TOWNHOUSE DEVELOPMENT
PRINCE STREET AND PITT RIVER ROAD
PORT COQUITLAM, BC

DRAWING TITLE:
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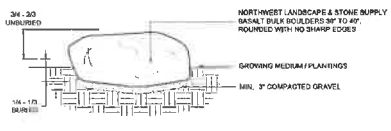
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CHK'D: YR

PMG PROJECT NUMBER: 23-232

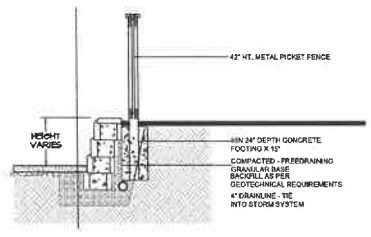
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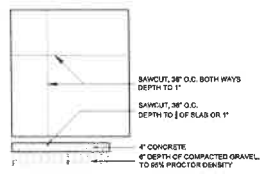
1 TREE AND SHRUB PLANTING AT GRADE
1:25



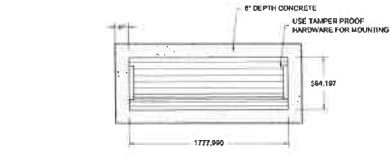
3 FLAT BOULDER
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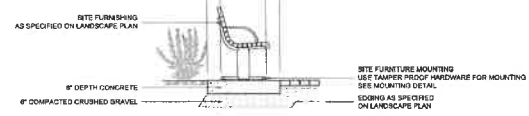
4 ALLAN BLOCK WALL
1:25



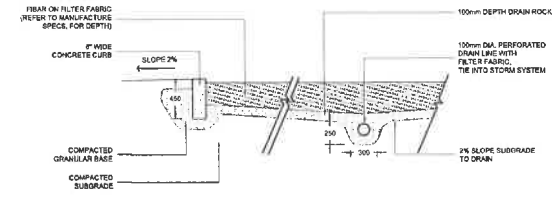
5 CONCRETE SAW CUTS
1:25



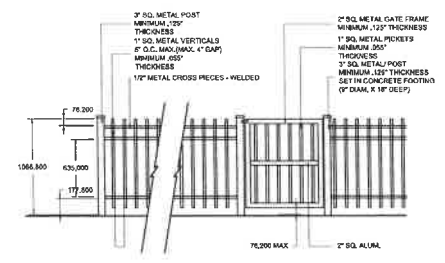
6 BENCH ON CONCRETE PAD
1:25



7 SITE FURNITURE MOUNTING
1:25

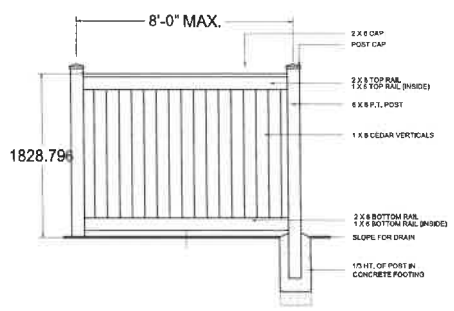


8 42" HT. METAL PICKET FENCE
1:25



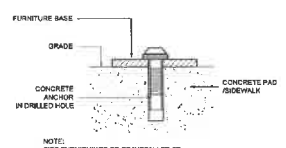
9 42" HT. METAL PICKET FENCE
1:25

NOTE:
INDUSTRIAL-GRADE METAL FENCING
HEAVY DUTY HANDLES & HINGES
BANK AND CORNER JANT POWDER COATING
COLOUR OF FENCE TO BE APPROVED BY LANDSCAPE ARCHITECT



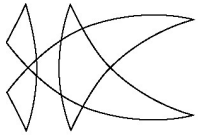
2 6' HT. WOOD FENCE
1:25

NOTE:
ALL POST TO BE PRESURE TREATED TO CSA STANDARD. ALL OTHER MEMBERS
1. TO BE MINIMUM #3 CONSTRUCTION GRADE UNLESS OTHERWISE SPECIFIED.
2. APPLY TWO COATS PRESERVATIVE TO ALL END CUTS.
3. ALL HARDWARE TO BE NOT STAINLESS STEEL.
4. APPLY TWO COATS OF STAIN ACCORDING TO MANUFACTURER'S SPECIFICATIONS, AND IN A COLOUR TO MATCH BUILDING AS APPROVED BY THE PROJECT ARCHITECT.



7 SITE FURNITURE MOUNTING
1:25

NOTE:
SITE FURNISHINGS TO BE INSTALLED TO MANUFACTURER'S SPECIFICATIONS



Brown Strachan Associates
Consulting Engineers in Acoustics

Project: A04.182

March 6, 2024

Caliber Projects Ltd.
205 - 6360 202 Street
Langley, BC V2Y 1N2

Attention: Mr. Riley Schmidt, Development Manager

Dear Mr. Schmidt:

Re: Greystone - 1884-1930 Harbour Street, 1887-1911 Prince Street
and 1155 Pitt River Road, Port Coquitlam (City File #: RZ000261 & DP000551)

Appended is our updated report entitled "Greystone - Acoustical Evaluation", which considers the current proposed townhouse development.

Please call if you have any questions.

Yours very truly,

BROWN STRACHAN ASSOCIATES

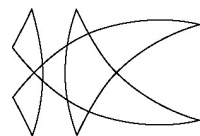
Aaron Peterson, P.Eng.

AP/sb/24Mar/Caliber-Greystone.ttl.wpd

GREYSTONE
ACOUSTICAL EVALUATION

Prepared for:
CALIBER PROJECTS LTD.

Aaron Peterson, P.Eng.
Andrew R. Fawcett, P.L.Eng., AScT.
March 6, 2024



Brown Strachan Associates
Consulting Engineers in Acoustics

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- 1.0 INTRODUCTION

- 2.0 DESIGN CRITERIA

- 3.0 RECOMMENDATIONS
 - 3.1 Recommended Exterior Design Levels for Traffic Noise
 - 3.2 Façade Upgrades
 - 3.3 Ventilation & Equipment

- 4.0 DISCUSSION
 - 4.1 Method of Evaluation
 - 4.2 Traffic Noise
 - 4.3 Sound Attenuation Fence
 - 4.4 Interior Noise

- 5.0 CONCLUSION

APPENDIX



GREYSTONE - ACOUSTICAL EVALUATION

1.0 INTRODUCTION

Brown Strachan Associates (BSA) have been retained by Caliber Projects Ltd. to conduct an acoustical evaluation of the proposed Greystone townhouse development at 1884-1930 Harbour Street, 1887-1911 Prince Street and 1155 Pitt River Road, Port Coquitlam (City File: RZ000261 & DP000551), as designed by The Nerdy Architect on drawings issued for DP Revisions 2024.02.06 (appended).

In response to comment 10 of the City of Port Coquitlam Application Review letter dated 1 November 2023 (appended), the terms of reference of this report are to assess future traffic noise at the proposed development and to recommend acoustical façade upgrades to satisfy indoor noise design criteria recommended in the Canada Mortgage and Housing Corporation (CMHC) publication Road and Rail Noise: Effects on Housing (print appended). This report considers noise from future traffic on the Mary Hill Bypass (Provincial Highway 7B), Pitt River Road and Harbour Street.

2.0 DESIGN CRITERIA

The proposed townhouse development has been evaluated with respect to the following CMHC indoor noise level design criteria:

<u>Room</u>	<u>Noise Levels (Decibels)</u>
Bedrooms	35
Living, dining and recreation rooms	40
Kitchen, bathrooms and hallways	45

The noise level design criteria above are A-weighted 24-hour equivalent levels, Leq(24) in decibels (dB).

3.0 RECOMMENDATIONS

The following are acoustical recommendations to satisfy the CMHC design criteria, based on the drawings indicated above, and should be referenced in the tender documents. These recommendations may be revised based on the final building design, façade details, window and exterior door shop drawings, etc. The IFT drawings and preliminary window / exterior door shop drawings should be reviewed with reference to the recommendations in this report.



Notations should be included on the construction drawings indicating that the acoustical recommendations in this report will be incorporated into the final design and construction, as concurred with or amended by the City of Port Coquitlam.

Coordination of Code requirements, acoustical recommendations, field reviews, letters of assurance, construction or occupancy certification requirements, etc., should be provided by the Registered Professional of Record (RPR).

Noise sources or acoustical design considerations for which insufficient information exists at this stage have not been evaluated, e.g. equipment, etc. Considerations such as structural, thermal, building envelope or performance requirements, fire ratings, etc., should be designed by others.

3.1 Disclosure

In addition to any legal agreements required by the City of Port Coquitlam (ref. comment 11.e. of the Application Review letter, appended), full disclosure should be made to prospective residents that the development site is along an arterial traffic/transit route, with vehicular traffic, trucks and buses operating day and night. The disclosure should indicate that traffic/transit sources cause noise and possible vibration, which may be annoying to some individuals. The City of Port Coquitlam, Translink and the Ministry of Transportation & Infrastructure (MoTI) may have specific disclosure wording satisfying their requirements.

3.2 Recommended Exterior Design Levels For Traffic Noise

At the most exposed locations along the Mary Hill Bypass, the recommended exterior design level for traffic/transit noise is $Leq(24) = 73$ dB (appended), which considers an unobstructed view from the upper floors of the townhouses over the proposed sound attenuation fence to traffic on the Mary Hill Bypass (see 4.2 Traffic Noise and 4.3 Sound Attenuation Fence).

3.3 Façade Upgrades

Sound transmission through the façade has been evaluated based on the window and exterior door areas indicated on the drawings, and conventional façade construction including exterior finishes comparable to cementitious cladding (elevations appended). To satisfy the CMHC design criteria, recommended window, exterior door and wall upgrades are indicated on the appended Greystone - Façade Upgrade Schedule (Schedule).

Where OITC acoustical ratings are specified on the appended Schedule, the window and exterior door supplier(s) should submit fenestration test reports per ASTM E90 representative of their proposed assemblies, i.e. complete window frame and exterior door assemblies with



proposed glazing (not generic glazing-only data). At substantial completion, and as supporting documentation for the RPR, the supplier(s) should confirm in writing that their rated assemblies, as installed on site, are equivalent to their tested assemblies and conform fully with this report and the appended Schedule.

Window and exterior door assemblies should satisfy Code requirements, including airtightness, etc. Considerations such as wind loading, safety, structural, thermal requirements, visual specifications, etc., should be checked for all windows and exterior doors (by others), and may dictate thicker glazed units than the references indicated on the Schedule (subject to BSA review of specified ASTM E90 acoustical test reports). Glazing may require strengthened glass to satisfy Code requirements or considerations such as structural, visual, manufacturer's weight or size restrictions, etc., e.g. mullions may be required.

3.4 Ventilation & Equipment

Sound transmission through the façade has been evaluated based on windows and doors in the closed position. Ventilation details, thermal requirements, etc., should be designed by a mechanical consultant. Equipment should be selected to satisfy Code acoustical requirements, e.g. 6.2.1.1 & 9.32.3.5, and the City of Port Coquitlam Noise Control Bylaw, 1994, No. 2891. For equipment considered critical, near townhouses, decks or roof decks, amenity areas, adjacent buildings or properties, BSA should review the proposed installation details.

If make-up air ducts penetrating the façade are required to satisfy ventilation requirements, the ducts should be designed to provide a noise reduction of about 50 dB for exterior noise, e.g. nom. 6ft. of 4" diameter acoustically lined ductwork or flexible connector. Where specified, ERV/HRV systems should be designed with equivalent treatment to reduce sound transmission into the townhouses. Exhaust ducts to the exterior from suite bathrooms, kitchens, etc., do not require acoustical upgrades such as lining.

4.0 DISCUSSION

4.1 Method of Evaluation

The method of evaluation used in this report gives detailed consideration of sound insulation referencing NRC's IBANA-Calc analysis software and related validation studies (see 4.4 Interior Noise, below). To determine possible façade upgrades necessary to satisfy the indoor design criteria, evaluation of the proposed façade construction is based on Leq(24) traffic sound transmission, windows and doors in the closed position, rooms with the greatest exposure to noise and the largest exterior wall, window and door areas with respect to floor area.



4.2 Traffic Noise

Future traffic noise exposure has been evaluated based on the forecasted 2030 a.m. & p.m. peak hour traffic data in the CTS Traffic Impact Assessment (TIA) of 5 April 2018, prepared for the mixed-use development previously proposed on this site (appended). CTS confirm that an updated TIA is not warranted for the currently proposed townhouse development (CTS letter of 3 January 2024, appended). The a.m. + p.m. peak hour data are considered equivalent to 13% of the daily total traffic (MoTI ref. data appended). All local roads in this area have been evaluated based on the posted speed limits, i.e. 70 km/h on Mary Hill Bypass, 50 km/h on Pitt River Road and Harbour Street. The City's truck route and Translink Transit System maps have been considered (prints appended).

Based on the CTS traffic data, site observations and previous acoustical studies in the area, the following design volumes have been used to evaluate future traffic noise at the development site:

	Vehicles per day (vpd)	% Heavy vehicles
Mary Hill Bypass (E. / W.Bnd):	41,900 / 36,100	5
Pitt River Road (N. + S.Bnd):	10,800	1
Harbour Street (E. + W.Bnd):	1,500	1

Design traffic noise levels have been derived from statistical tables in CMHC's Road and Rail Noise: Effects on Housing, developed by NRC. These tables have been used on numerous housing site assessments throughout Metro Vancouver, including recent studies in this area, with good correlation between measured and calculated levels (typically +/-1 dB for normal traffic conditions). For the design volumes, the CMHC calculated future traffic noise level is $Leq(24) = 73$ dB at the most exposed locations along the Mary Hill Bypass (printouts appended), which considers an unobstructed view to traffic from the upper floors of the townhouses over the sound attenuation fence recommended by the MoTI.

To check that the CMHC traffic noise calculations correlate with traffic in this area, sample daytime measurements were conducted at a Test Location approximately 1.5m north of the south property line along Mary Hill and 28m west of the east property line. The average measured $Leq = 74$ dBA (Table 2 & Graph: SUMM, appended). For the observed traffic, the calculated CMHC equivalent traffic noise level is $Leq(24) = 75$ dB (printout: predict, appended). The difference is attributed to westbound traffic on Mary Hill moving slower than the 70 km/h posted speed limit, likely as a result of congestion at the Pitt River Road intersection (printout: predict2). No corrections have been made to the recommended exterior design levels for this local site condition.



4.3 Sound Attenuation Fence

The analysis in this report considers an unobstructed view to traffic on the Mary Hill Bypass. For the sound attenuation fence recommended by the MoTI (ref. comment 10 of the Application Review letter), a conventional 2-3m high solid barrier/fence weighing nominally 2psf will reduce traffic noise where the line-of-sight to vehicles is interrupted. Where the line-of-sight is over the barrier, e.g. upper floor living/dining rooms and bedrooms in Buildings G & H, etc., no reduction to the design traffic noise levels has been considered.

4.4 Interior Noise

Noise in the townhouses has been evaluated referencing NRC's IBANA-Calc analysis software, related validation studies, statistical third octave band traffic source data normalized to future design conditions and façade transmission loss data. Detailed calculations of traffic sound transmission through the façade are summarized in Table 1 and include the absorption typical of furnished rooms (printouts appended). Table 1 shows the sound levels transmitted by each sound path, such as windows and exterior walls, and compares the total sound to the Leq(24) design criterion.

The analysis in this report indicates the interior sound levels satisfy the design criteria. Sound levels can vary relative to calculated levels due to normal variation in transportation activity, including traffic speed and volume, on-site performance of façade components, flanking sound transmission, room absorption, possible contribution of other sources, etc.

This report, or review of related documentation such as disclosure statements, legal agreements or restrictive covenants, window and door shop drawings, manufacturer's fenestration acoustical data, etc., is not a certification of on-site noise levels, or any aspect of the construction details. See appended Acoustical Evaluation Reports - Background Information.

5.0 CONCLUSION

Provided the recommendations in this report are implemented, our evaluation indicates the design of the proposed Greystone townhouse development satisfies the CMHC indoor noise level design criteria. The IFT drawings and preliminary window / exterior door shop drawings should be reviewed with reference to the recommendations in this report and the appended facade upgrade Schedule.



APPENDIX



GREYSTONE - FAÇADE UPGRADE SCHEDULE

This two page schedule forms part of the Brown Strachan Associates (BSA) acoustical report dated 6 March 2024 and should be read with the full report. It is the supplier's responsibility to ensure that the rated windows and exterior doors, as installed on site, fully conform to this schedule and report (confirm in writing, when requested). Meet all Code requirements. The IFT drawings and preliminary window / exterior door shop drawings should be reviewed with reference to the following upgrades.

Unless otherwise indicated in table below, provide all townhouses with conventional exterior construction, including window and exterior door assemblies with standard thermal glazing. Specified façade upgrades are applicable to all exterior walls, doors and windows in a given room, including rooms extending over multiple façades.

T/H Bldg.	Units	Rooms	Window & Door Upg.	Exterior Wall Upg.
A	B1 & B3	West Bedrooms	OITC 32	2x GWB
		Liv./Din./Kit. & East Bedrooms	OITC 29	
	B2	Living/Dining/Kitchen	OITC 32	2x GWB
		Corner Bedrooms (two full ext. walls)	OITC 32	2x GWB on Res.
		East Bedroom	OITC 29	
B	B1, B2 & B3	Bedrooms (all) & Living Room	OITC 29	
C	C1 & C3	South Bedrooms	OITC 29	
	C2	Liv./Din./Kit. & Bedrooms (all)	OITC 29	
F	A2a	Liv./Din./Kit. & Bedrooms (all)	OITC 29	
G & H	A1	Dining Room*	OITC 32	
		South Bedroom	OITC 35	2x GWB on Res.
		North Bedrooms	OITC 29	
	A2	Dining Room*	OITC 32	
		South Bedroom	OITC 35	2x GWB on Res.
		North Bedrooms	OITC 29	
	A3	Living/Dining*/Kitchen	OITC 32	2x GWB
		South Bedroom	OITC 35	2x GWB on Res.
		North Corner Bedroom (two ext. walls)	OITC 29	2x GWB
		North Bedroom (one ext. wall)	OITC 29	
J	D1	South Bedrooms	OITC 29	
	D2 & D3	Living/Dining/Kitchen	OITC 29	
		Bedrooms (all)	OITC 29	
L	B3a	Living/Dining/Kitchen	OITC 29	
		Bedrooms (all)	OITC 29	

* Where fully glazed decks are specified, e.g. Lumon, etc., facade upgrades are not required.



GREYSTONE - FAÇADE UPGRADE SCHEDULE (cont'd)

Legend

- OITC 35:** Provide OITC 35 rated window and exterior door assemblies (Note: stringent design requirement possibly requiring triple glazing and/or thick laminated glazing.)
- OITC 32: Provide OITC 32 rated window and exterior door assemblies (typ. with laminated glazing, e.g. 6-13-6Lam glazing).
- OITC 29: Provide OITC 29 rated window and exterior door assemblies (typ. with 6-13-4 or 6-13-6 thermal glazing).
- 2x GWB: Provide two layers of 5/8" Type X drywall (2x GWB) directly to suite side of exterior wall framing (exclude closets, ensuites and exterior walls with cabinetry).
- 2x GWB Provide 1/2" 25ga. single web resilient metal channels attached directly to suite side of exterior walls at 24" o.c., with on Res.: 2x GWB (exclude closets and ensuites). The following note should be included on construction drawings: "Where resilient furring is specified, install according to manufacturer's instructions. Ensure drywall screws do not contact framing. Do not install furring between layers of drywall or between sheathing & drywall."

Provide window and exterior door assemblies satisfying Code airtightness requirements. Where OITC acoustical ratings are specified, provide fenestration test reports per ASTM E90, as tested on representative window and exterior door assemblies, i.e. complete window frame and door assemblies with proposed glazing (not generic glazing only data). For all glazing in windows and exterior doors, check considerations such as wind loading, safety, structural requirements, visual specifications, etc. If necessary, provide thicker glazed units than the references indicated above (subject to BSA review of specified ASTM E90 test reports). Glazing may require strengthened glass to satisfy Code requirements and may have a size limitation to satisfy structural requirements, visual specifications, manufacturer's weight or size restrictions, etc., e.g. mullions may be required. See Acoustical Evaluation Reports - Background Information (appended to report).

Schedule based on Townhouse drawings issued for DP Revision dated 2024.02.06.



GREYSTONE



1,016 0 508.0 1,016 Meters



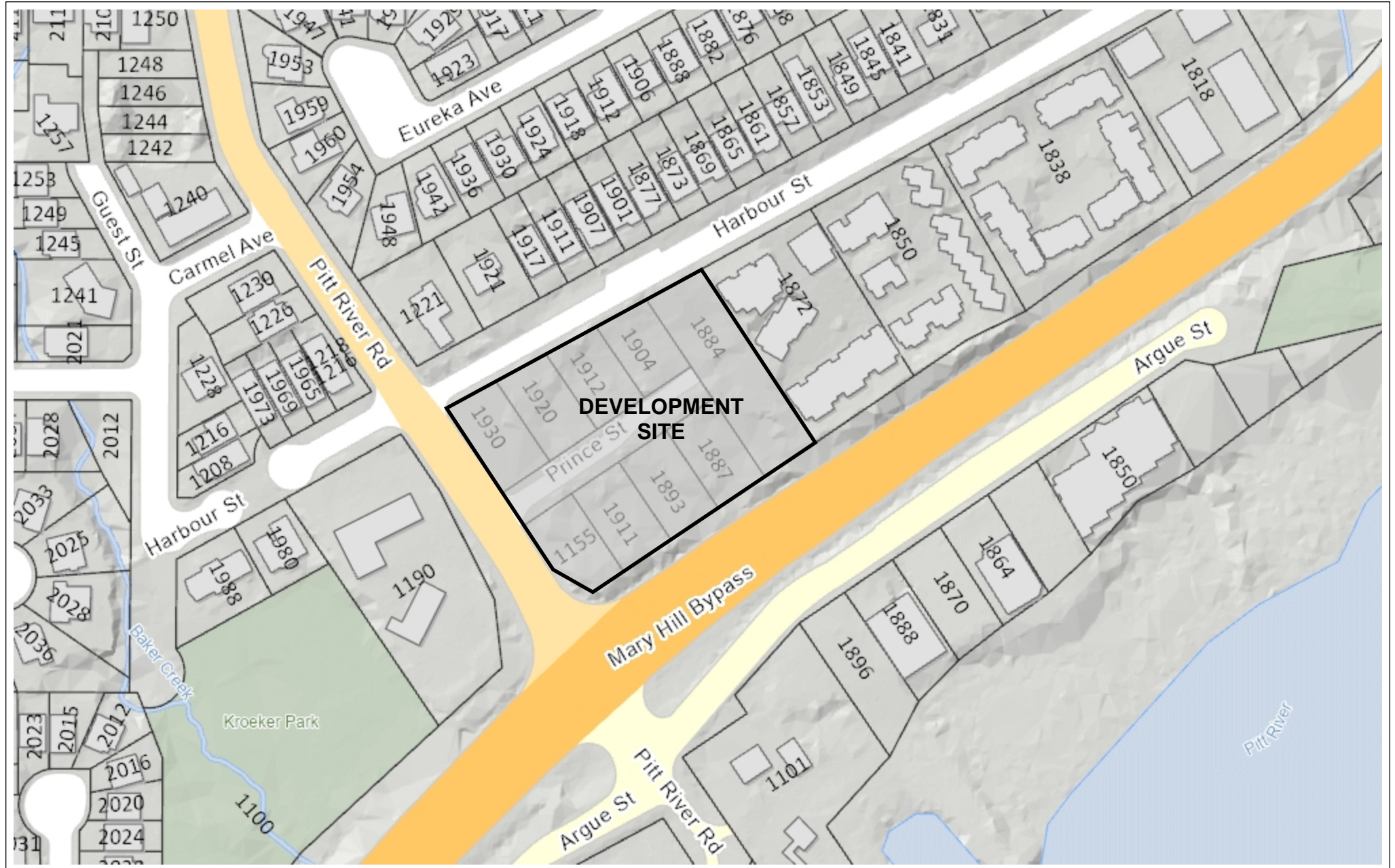
Scale 1 : 40,000

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Mar 4, 2024

GREYSTONE



51 0 25.4 51 Meters

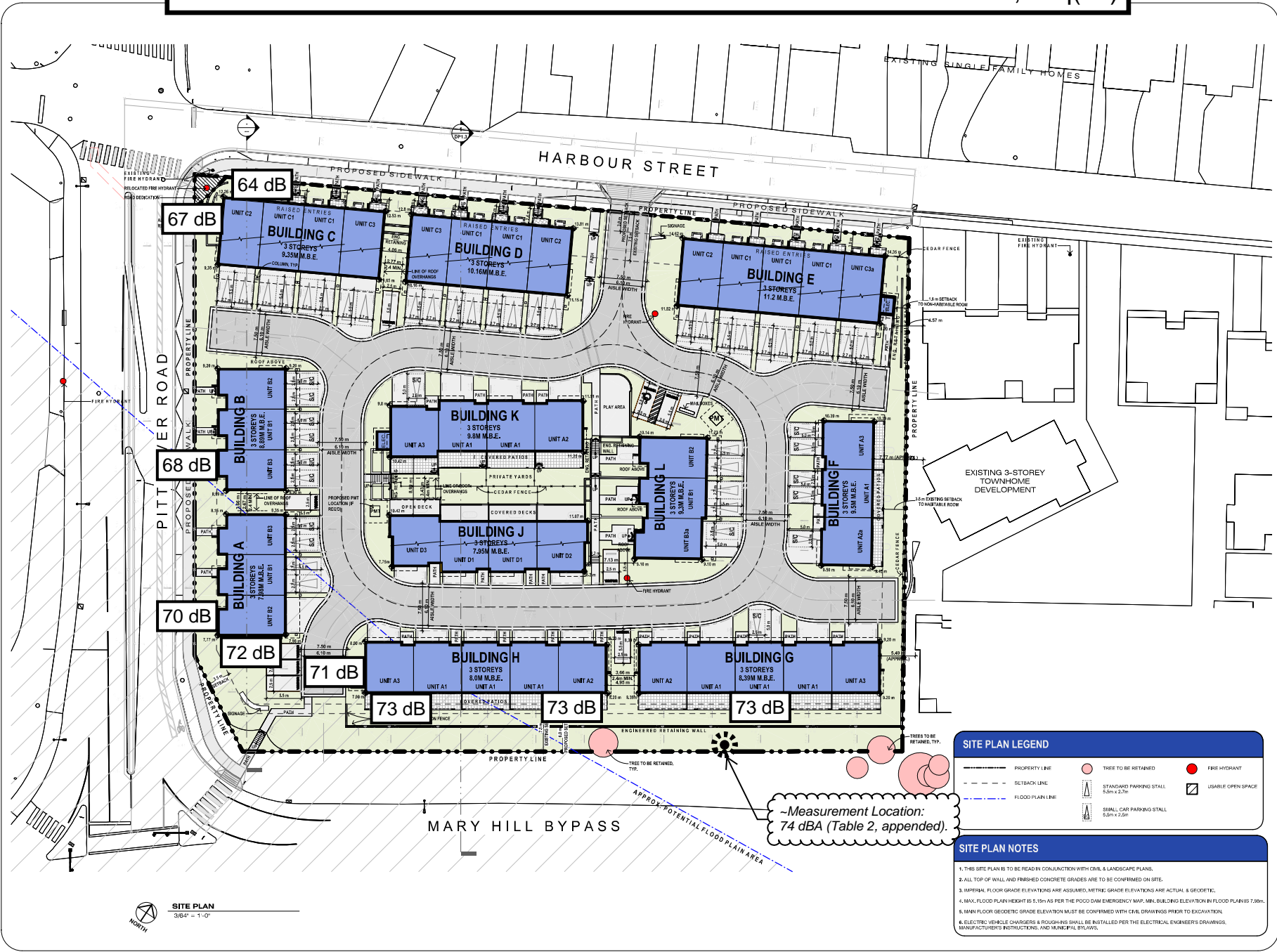


Scale 1 : 2,000

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RECOMMENDED EXTERIOR DESIGN LEVELS FOR TRAFFIC NOISE, Leq(24)



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DATE: 2024-02-06
JOB NO.: 2301



2024-02-06
REVISIONS NO. DATE DESCRIPTION
A 2024.02.06 DP REVISION

SITE PLAN
DP1.1

SCALE As Indicated



GREYSTONE

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GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

VIEW FROM MARY HILL BYPASS

COVER SHEET

DP0.0

SCALE



604 821 9088
nerdyarchitect.ca

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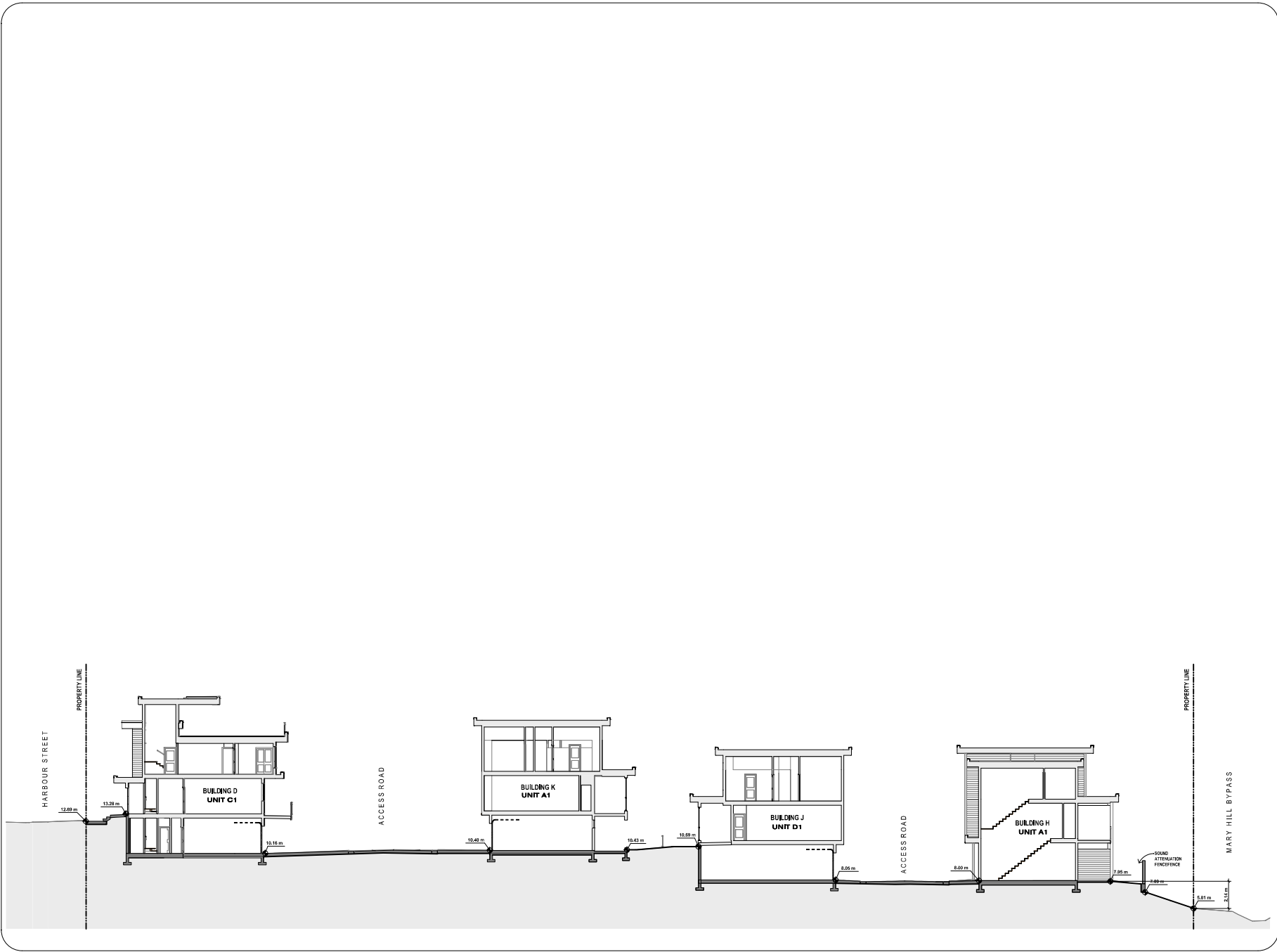
2024-02-06

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SITE SECTIONS

DP1.3

SCALE
3/32" = 1'-0"



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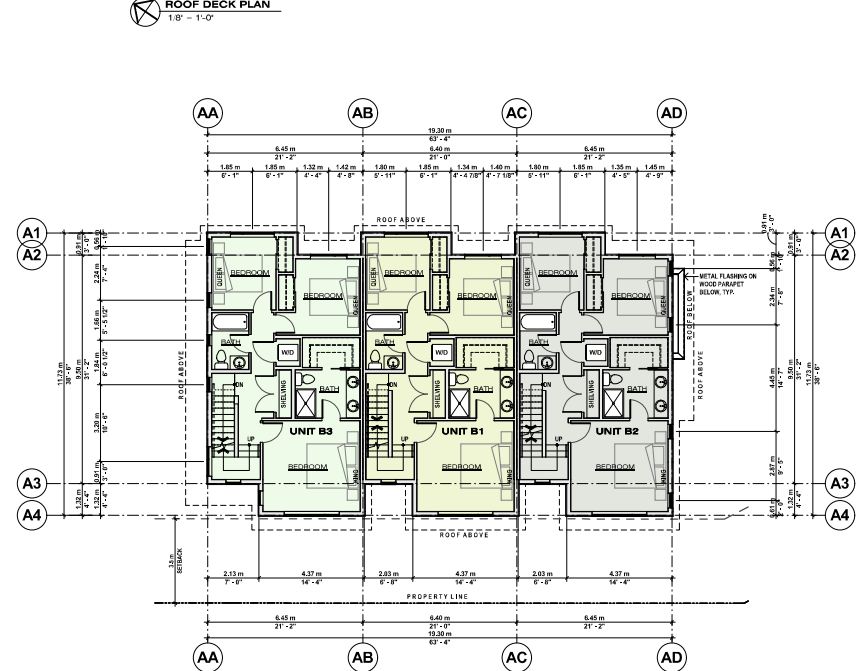
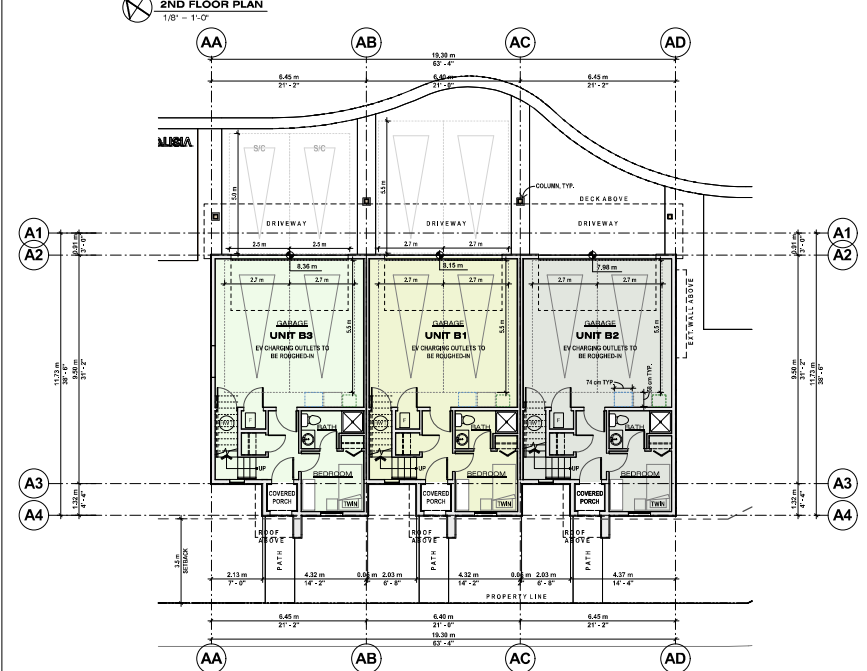
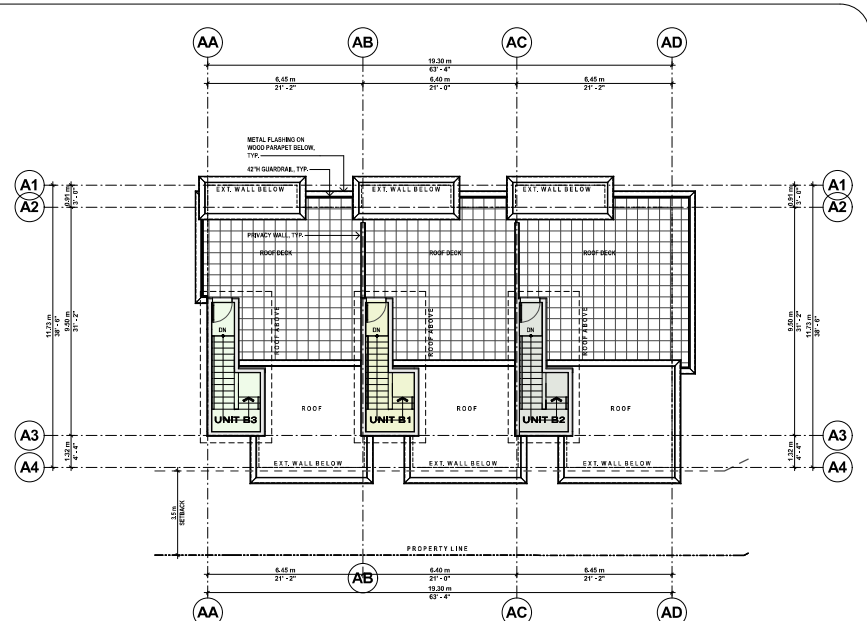
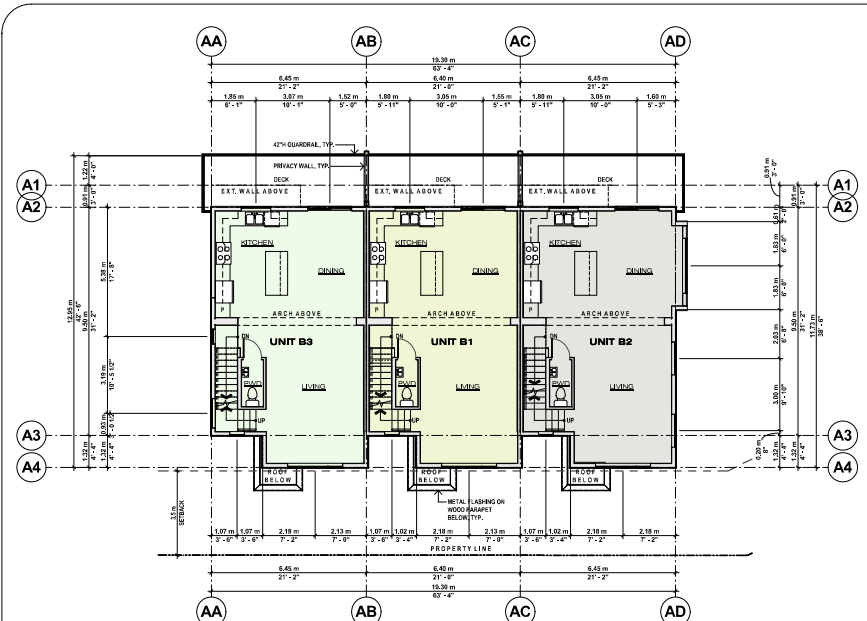


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FLOOR PLANS

DP2.1-A

SCALE
1/8" = 1'-0"



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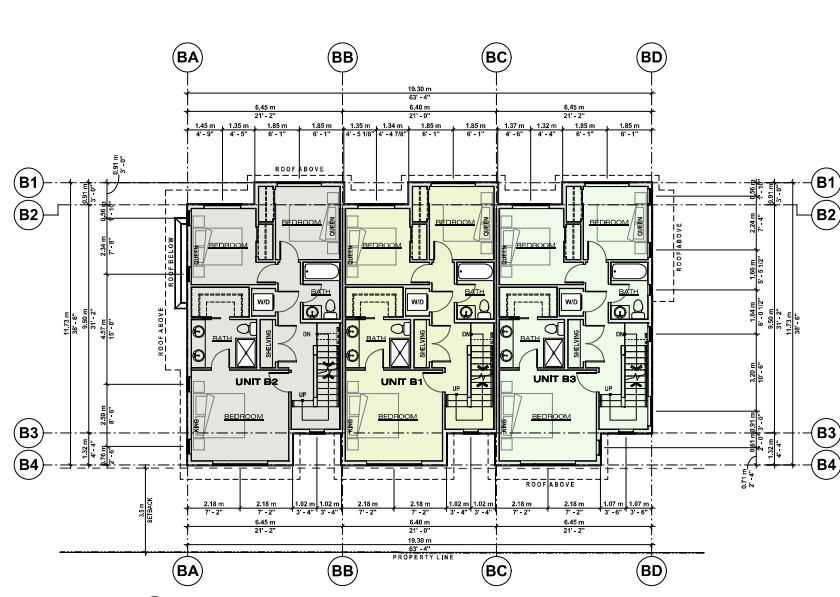
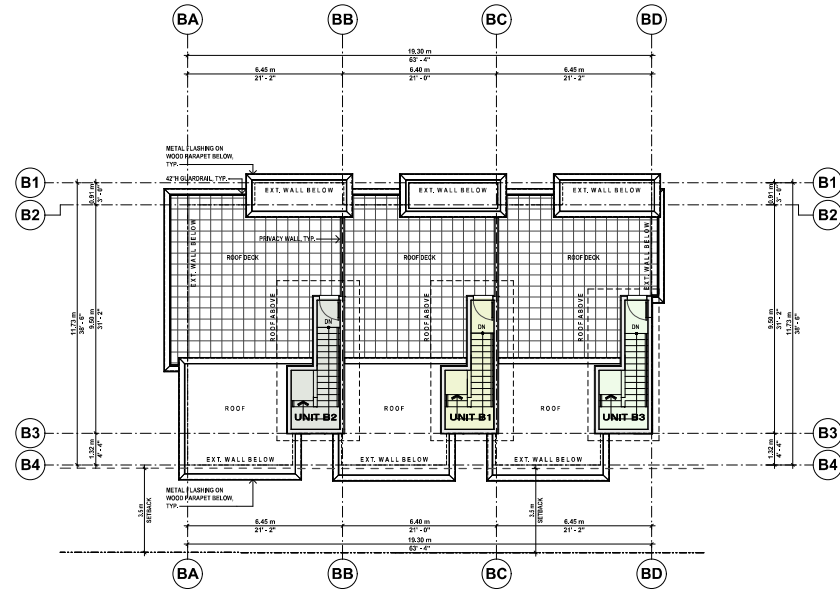
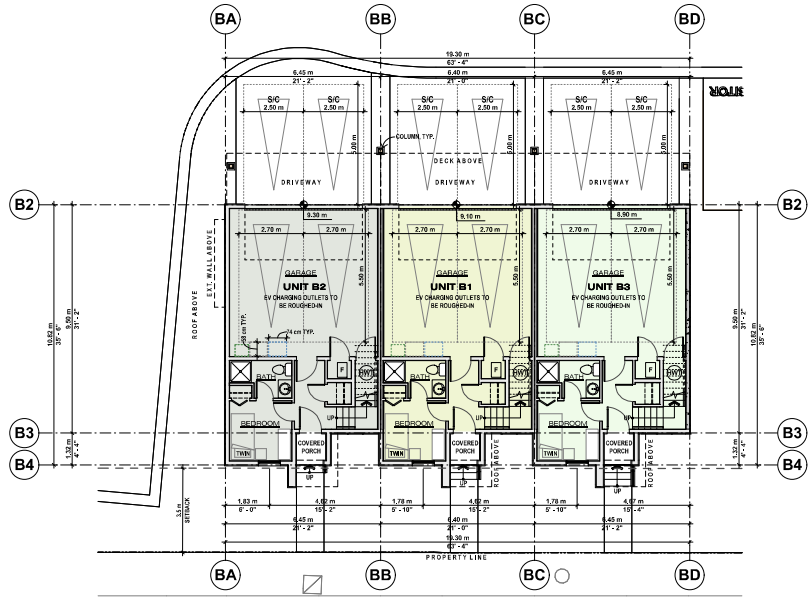
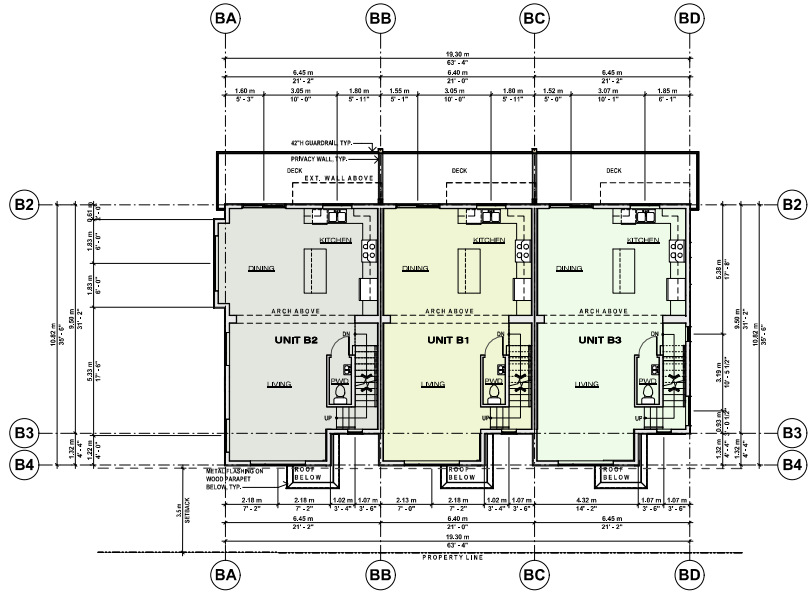
2024-02-06

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FLOOR PLANS

DP2.1-B

SCALE
1/8" = 1'-0"



GREYSTONE

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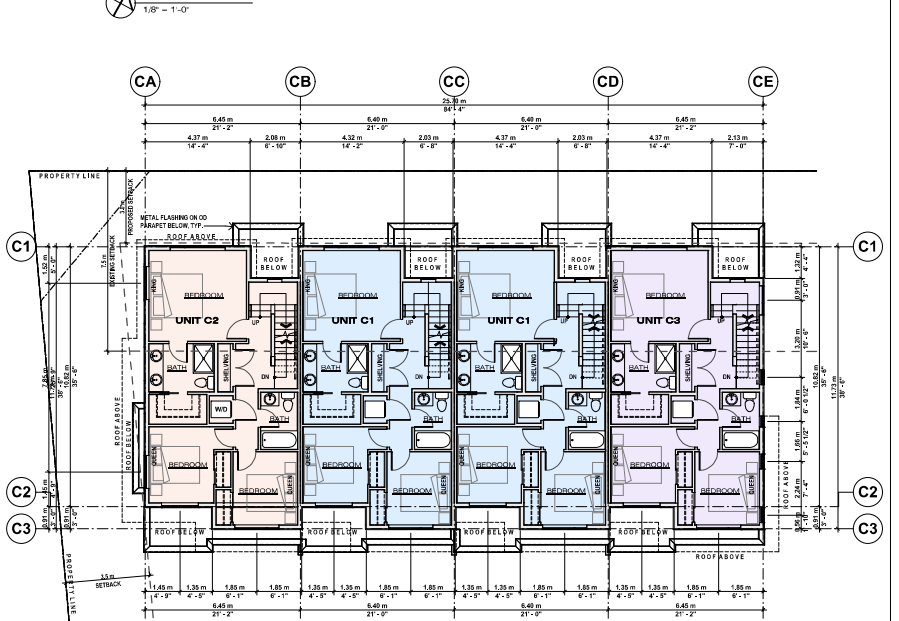
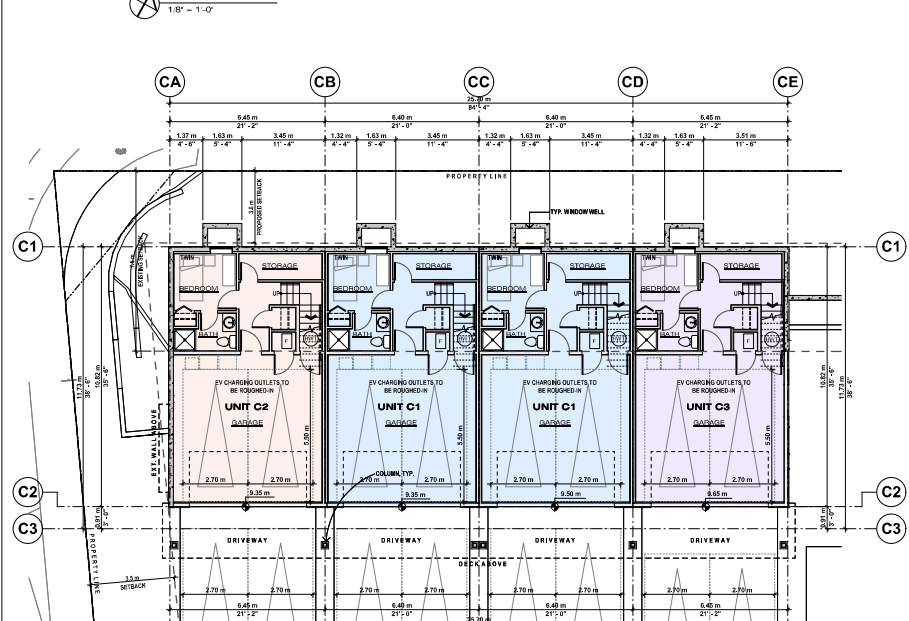
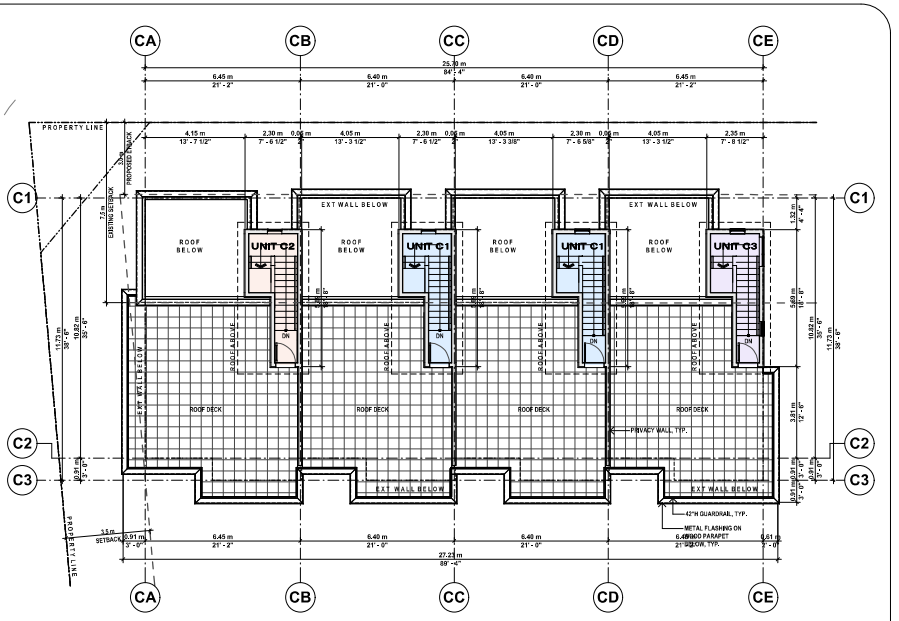
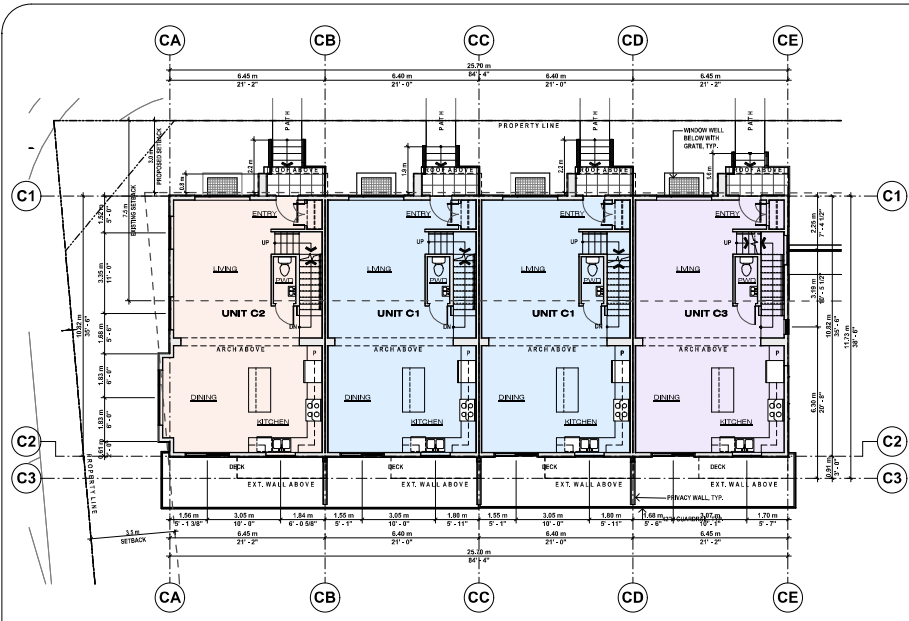
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FLOOR PLANS

DP2.1-C

SCALE
1/8" = 1'-0"



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JOB NO.: 2301

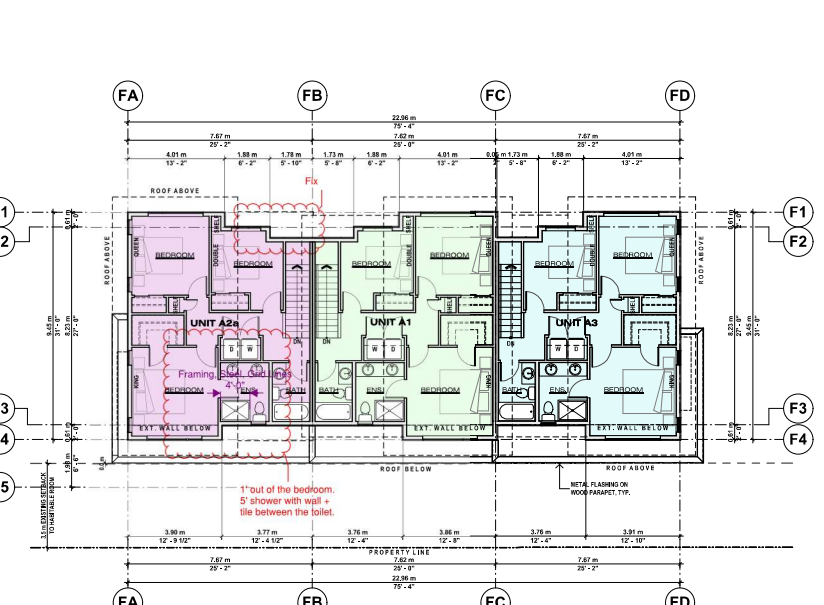
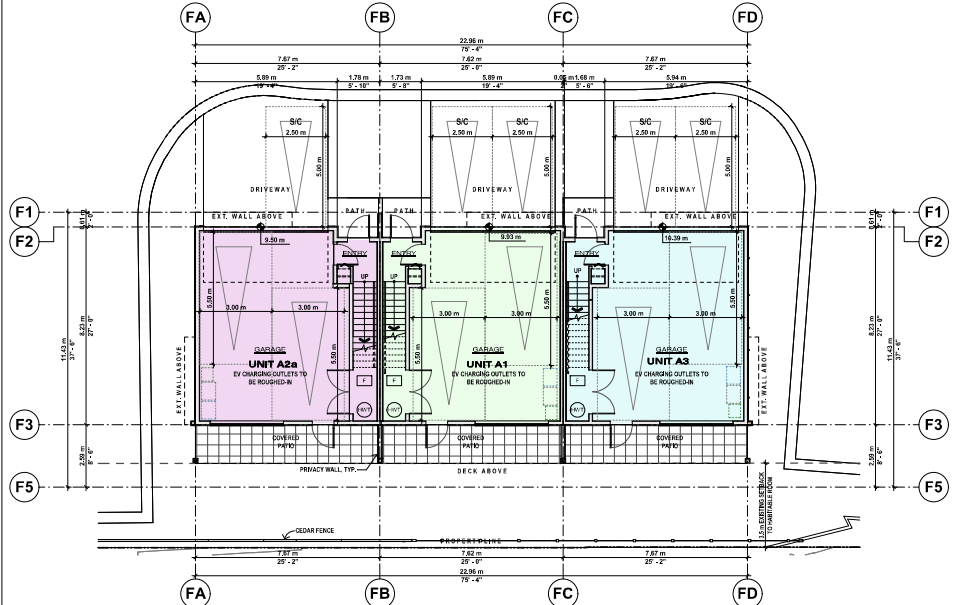
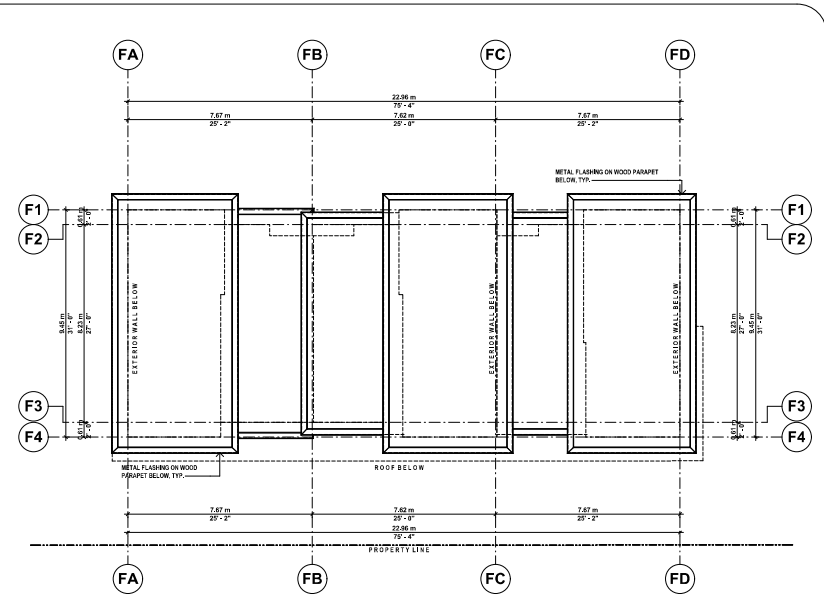
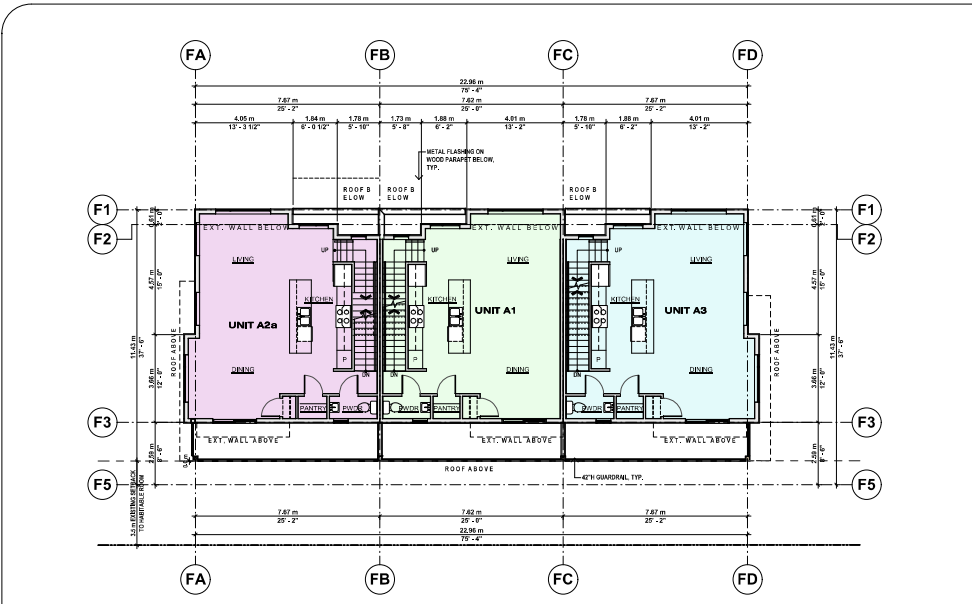
REGISTERED ARCHITECT
ALEXANDER MAY ROSE
BRITISH COLUMBIA
2024-02-06

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FLOOR PLANS

DP2.1-F

SCALE: 1/8" = 1'-0"



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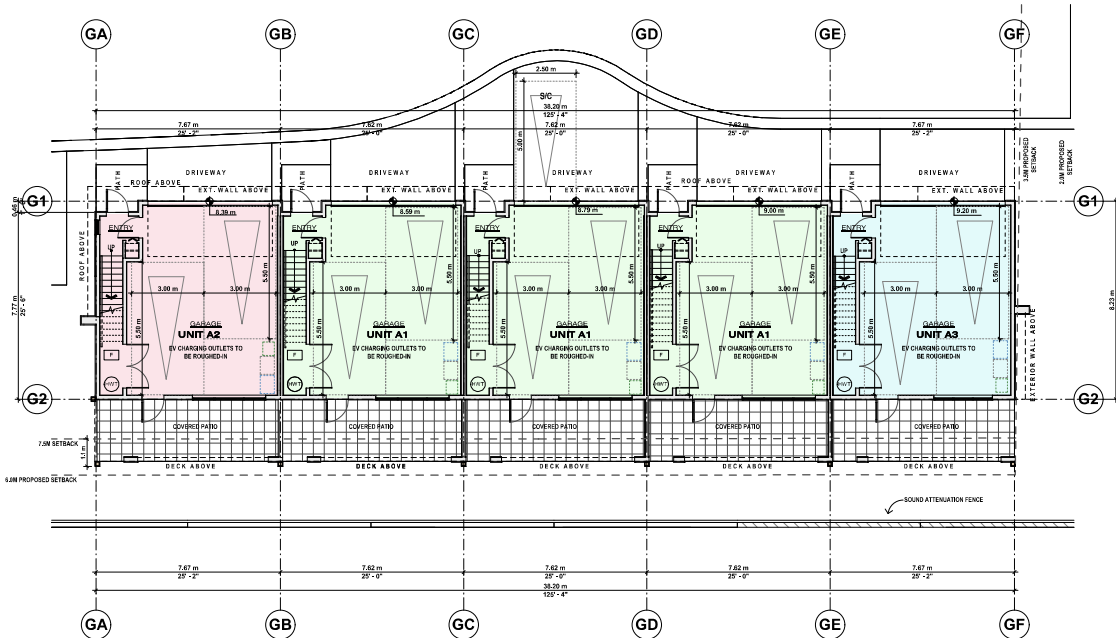
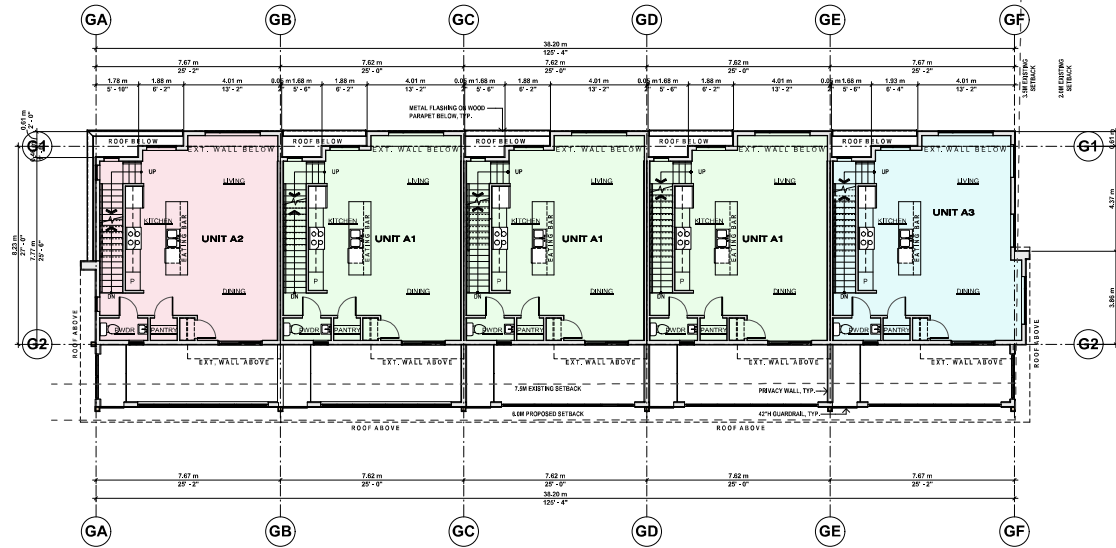
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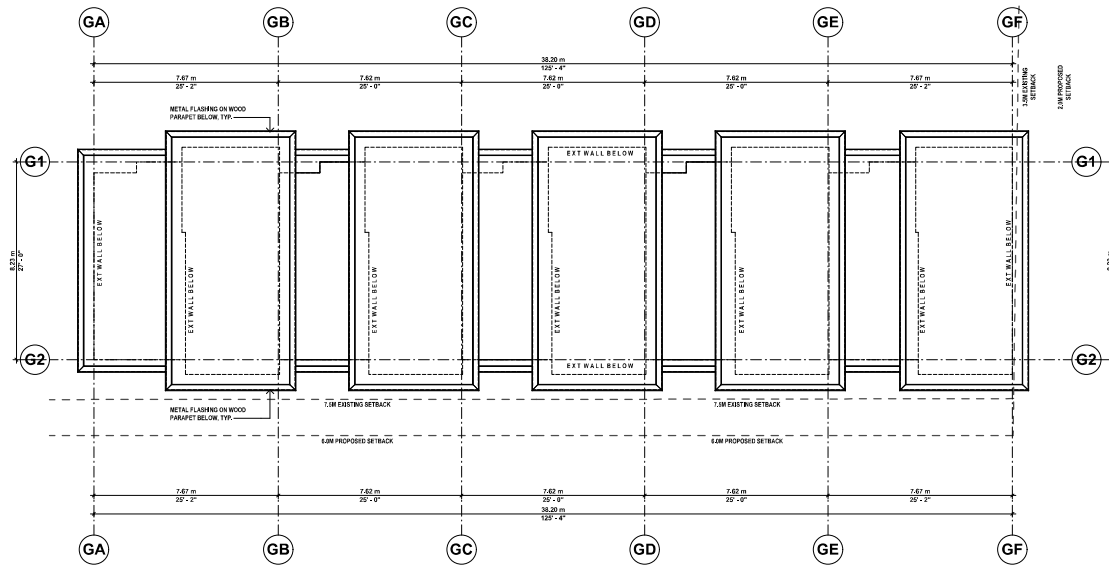
NO.	DATE	DESCRIPTION
A	2024.02.06	DP REVISION

FLOOR PLANS

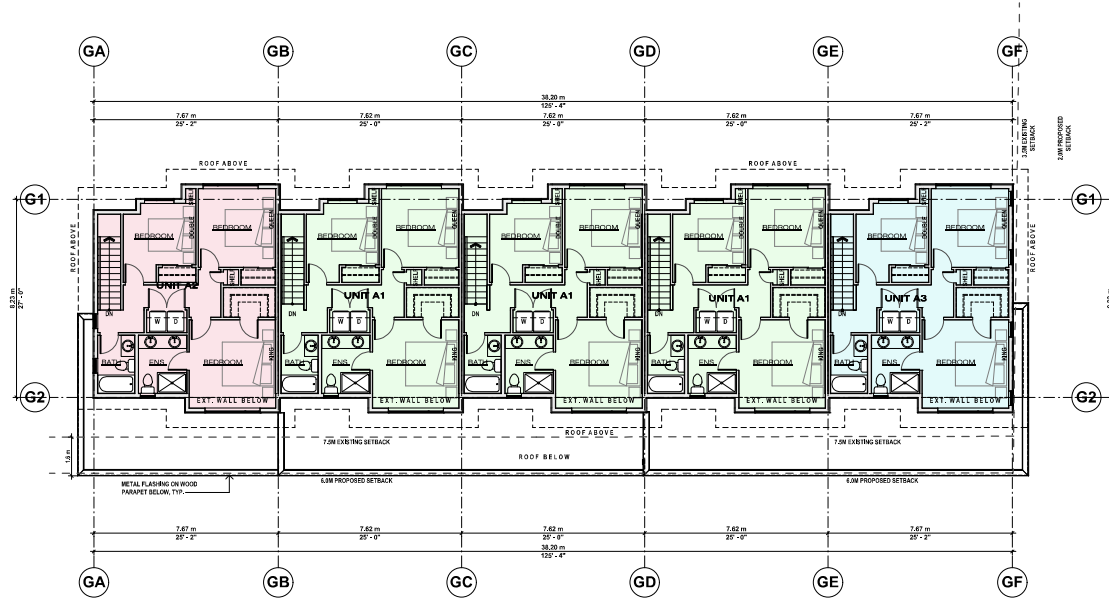
DP2.1-G

SCALE
1/8" = 1'-0"





ROOF PLAN
1/8" = 1'-0"



3RD FLOOR PLAN
1/8" = 1'-0"



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FLOOR PLANS

DP2.2-G

SCALE: 1/8" = 1'-0"

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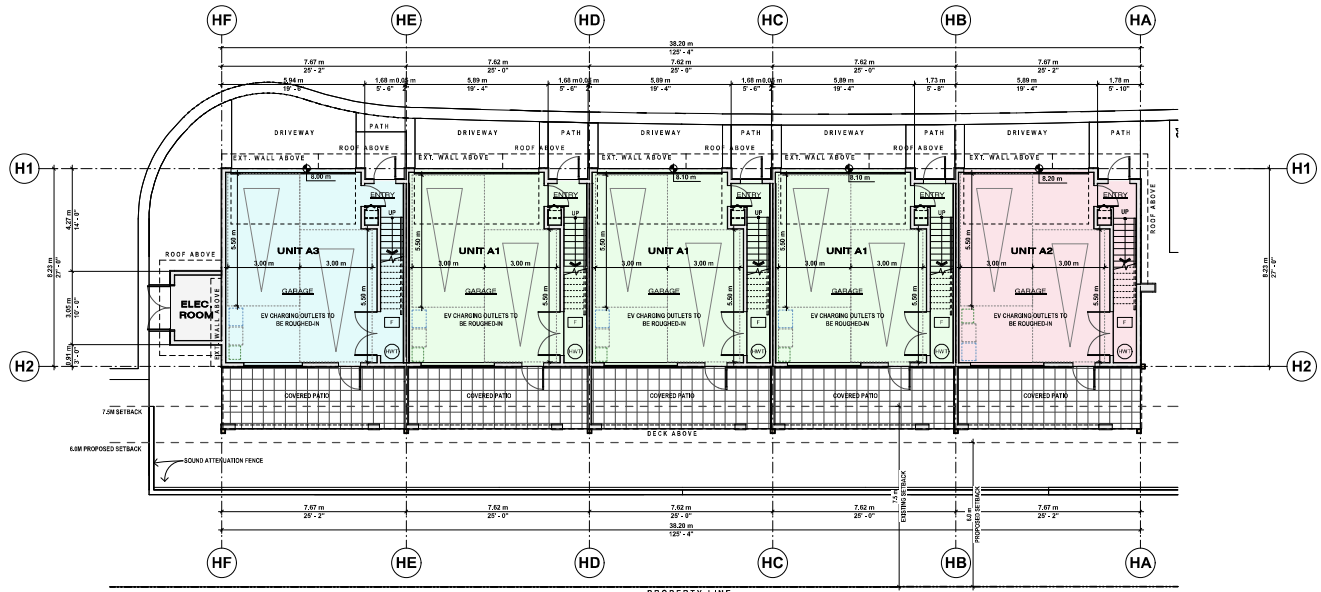
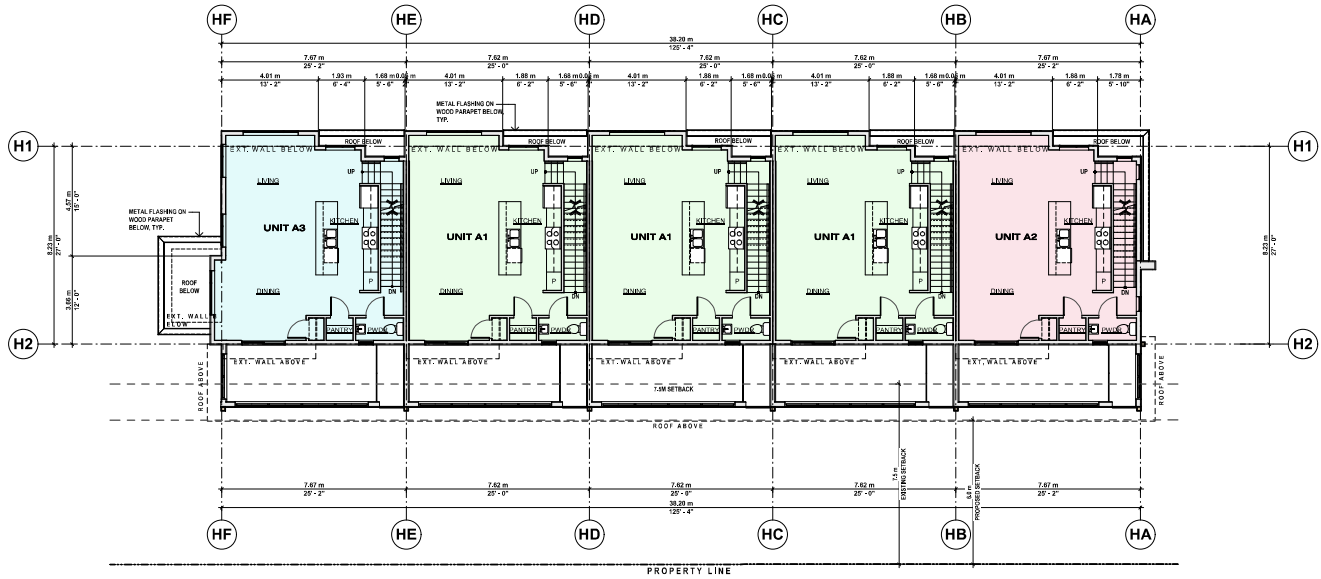
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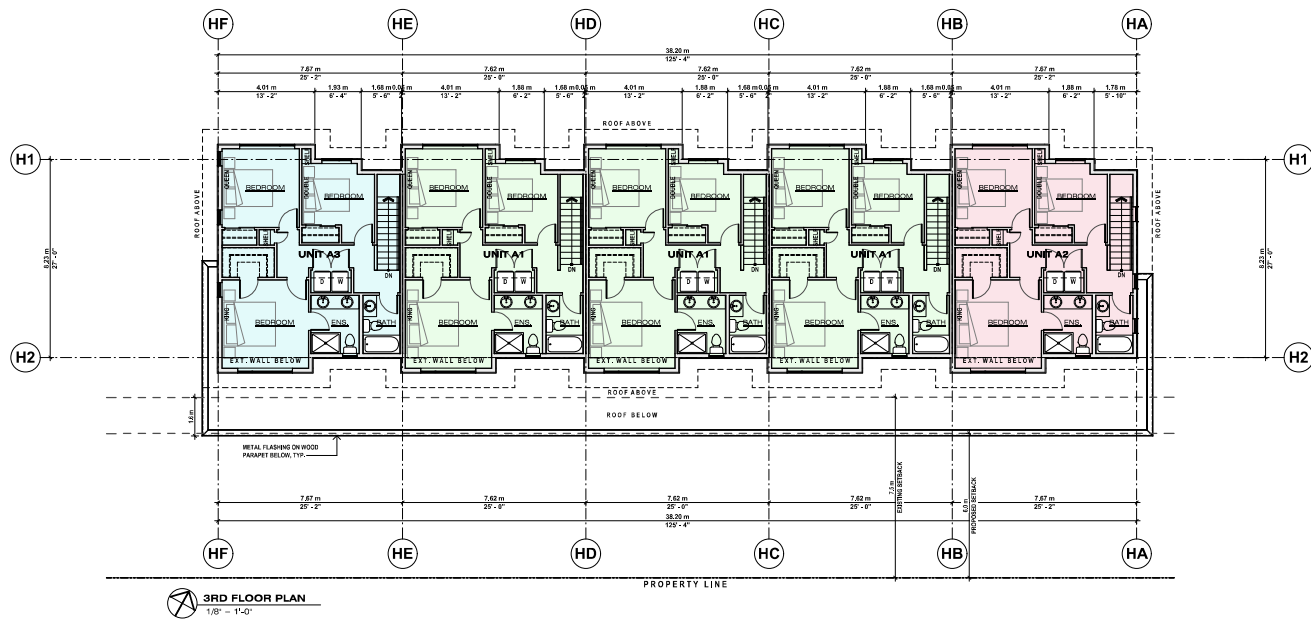
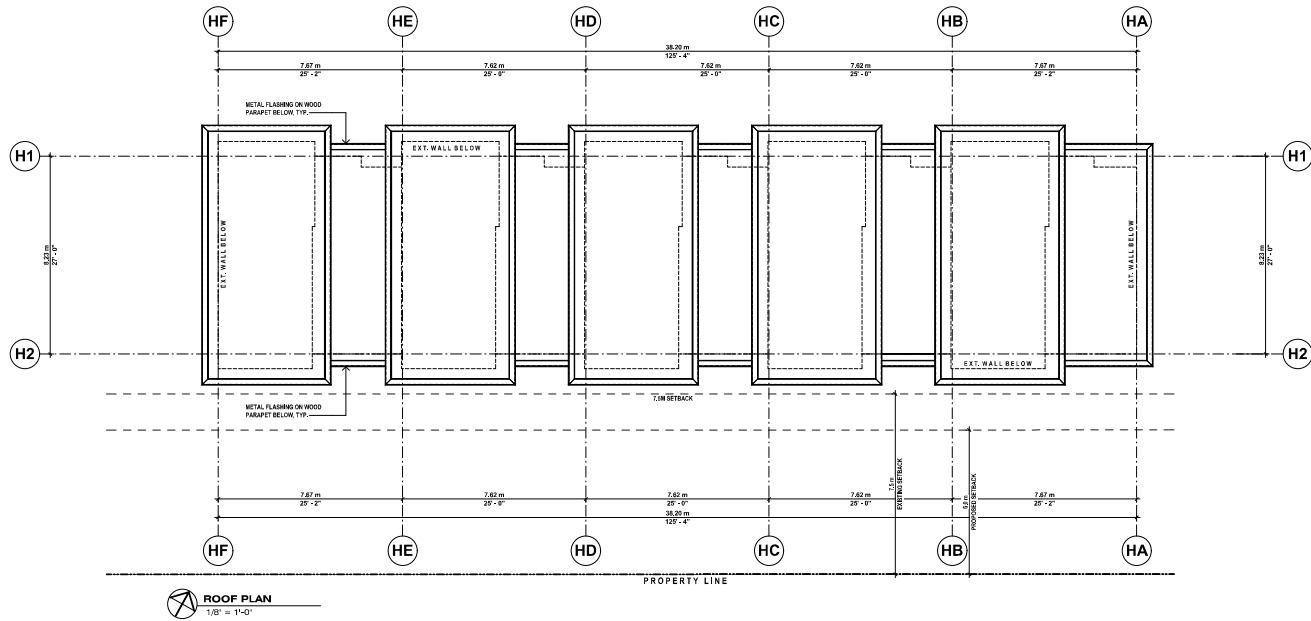
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A	2024.02.06	DP	REVISION

FLOOR PLANS

DP2.1-H

SCALE
1/8" = 1'-0"





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FLOOR PLANS

DP2.2-H

SCALE
1/8" = 1'-0"

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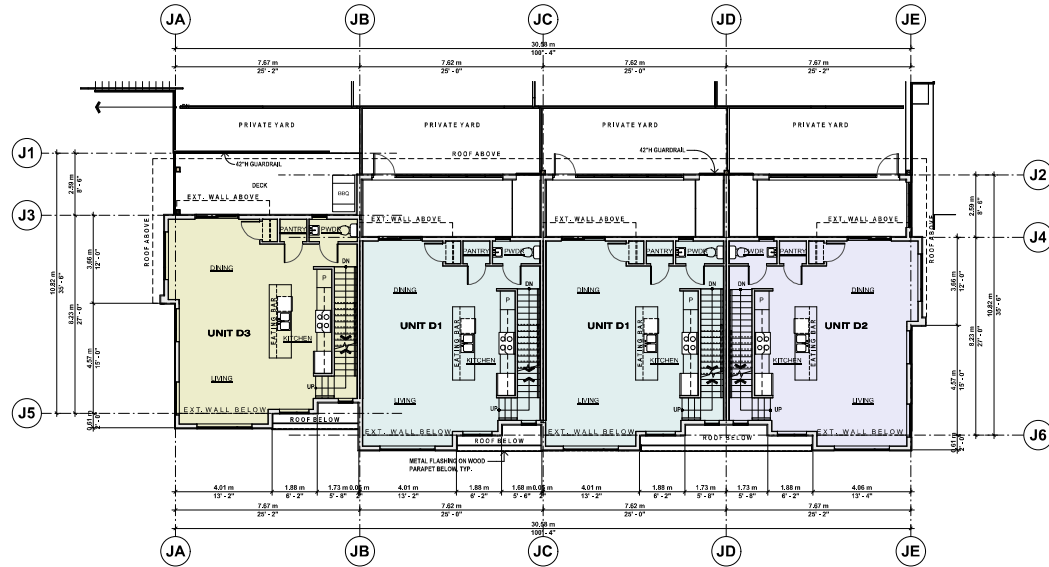
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A	2024-02-06	DP	REVISION

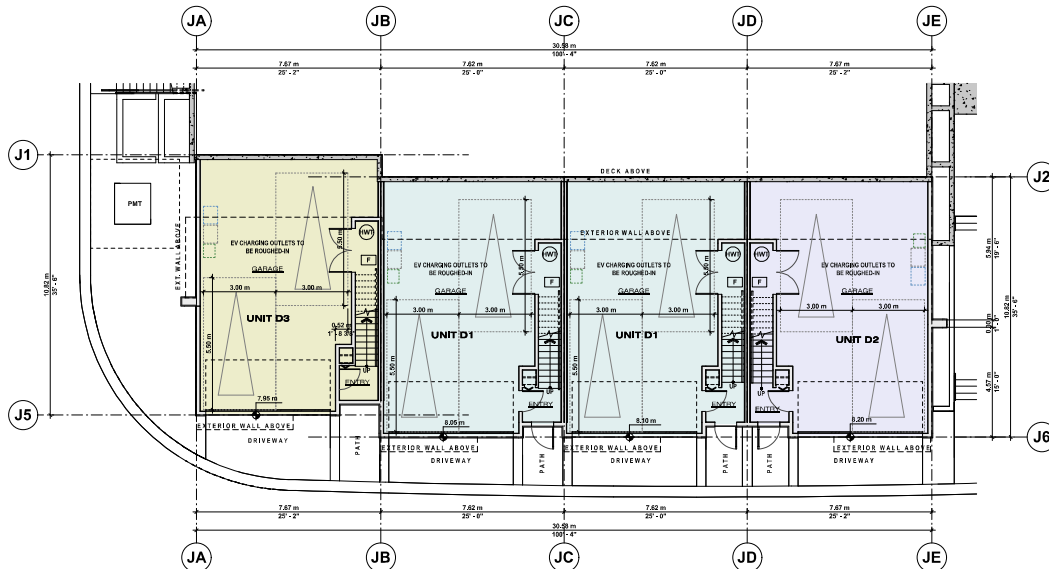
FLOOR PLANS

DP2.1-J

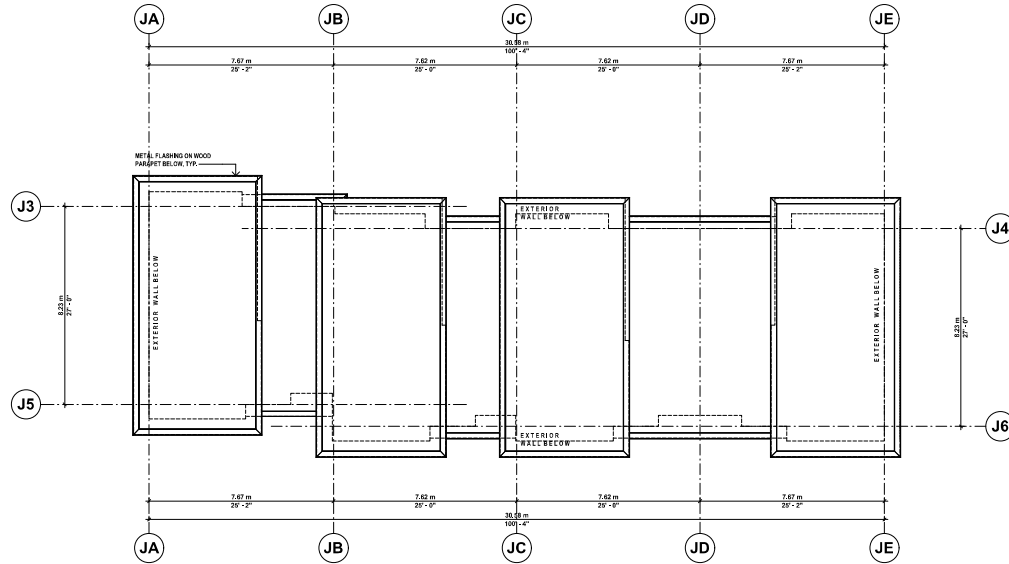
SCALE
1/8" = 1'-0"



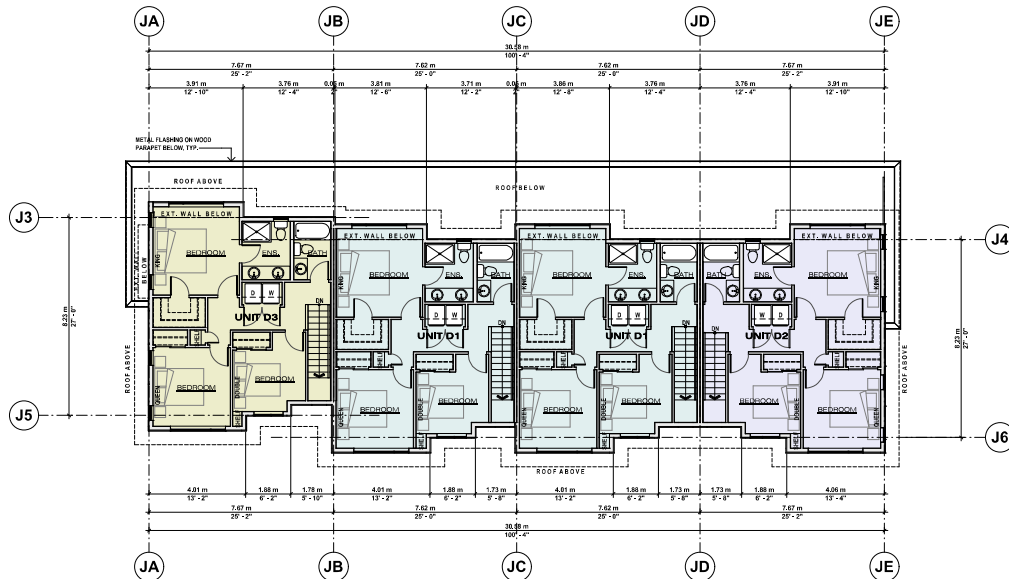
2ND FLOOR PLAN
1/8" = 1'-0"



1ST FLOOR PLAN
1/8" = 1'-0"



2ND FLOOR PLAN
1/8" = 1'-0"



3RD FLOOR PLAN
1/8" = 1'-0"

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

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FLOOR PLANS

DP2.2-J

SCALE
1/8" = 1'-0"

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FLOOR PLANS

DP2.1-L

SCALE
1/8" = 1'-0"



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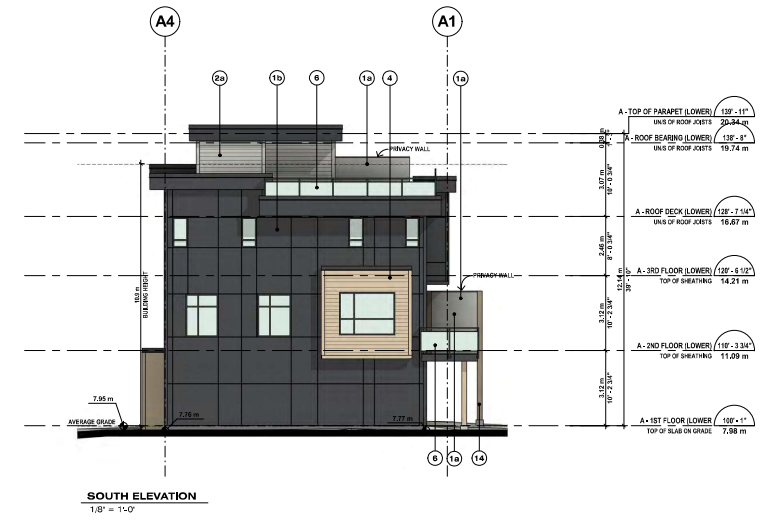
2024-02-06

NO.	DATE	DESCRIPTION
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ELEVATIONS

DP3.1-A

SCALE As Indicated



EXTERIOR FINISHES

1 - WALL - PAINTED CEMENTITIOUS PANELS a) BENJAMIN MOORE: OC-117 BRILLIANT WHITE b) BENJAMIN MOORE: 210-19 WROUGHT IRON	6 - GUARDRAIL - GLASS FACE MOUNT - PREFABRICATED ALUMINUM - GENTLE ALUMINUM, BLACK	11 - CAST-IN-PLACE CONCRETE a) METALLIC FINISH b) PAINTED TO MATCH BM: OC-117 BRILLIANT WHITE c) PAINTED TO MATCH BM: 210-19 WROUGHT IRON
2 - WALL - PAINTED CEMENTITIOUS LAP a) BENJAMIN MOORE: OC-117 BRILLIANT WHITE b) BENJAMIN MOORE: 210-19 WROUGHT IRON	7 - GUARDRAIL - FLUOROC POLYESTER FACE MOUNT - PREFABRICATED ALUMINUM - GENTLE ALUMINUM, BLACK	14 - TRIMMER COLUMN STAINED TO MATCH WOODSTONE SIAID CASTLE
3 - WALL - WOODSTONE TRUSTERSBEEF CEMENTITIOUS PANEL a) SAND CASTLE	8 - TRIM - PAINTED SIAID MESSING, CORNER TRIM & FASCIA: TO MATCH AS SHOWN	15 - METAL SOFFIT - FACTORY WHITE
4 - WALL - WOODSTONE TRUSTERSBEEF CEMENTITIOUS LAP a) SAND CASTLE	9 - GLAZING - VINYL TRIM - EXTERIOR FACTORY BLACK & INTERIOR FACTORY WHITE	
5 - PARAPET CAP FLASHING - ALUMINUM FACTORY BLACK	10 - GLAZING - ALL WEATHER DECK GLAZING - FACTORY BLACK FRAME	

NOTES

1. IMPERIAL FLOOR GRADE. ELEVATIONS ARE ASSUMED
2. METRIC GRADE ELEVATIONS ARE ACTUAL AND GEODETIC
3. MAIN FLOOR GEODETIC GRADE ELEVATIONS MUST BE CONFIRMED WITH CIVIL DRAWINGS PRIOR TO EXCAVATION

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE: 2024-02-06
JOB NO.: 2301

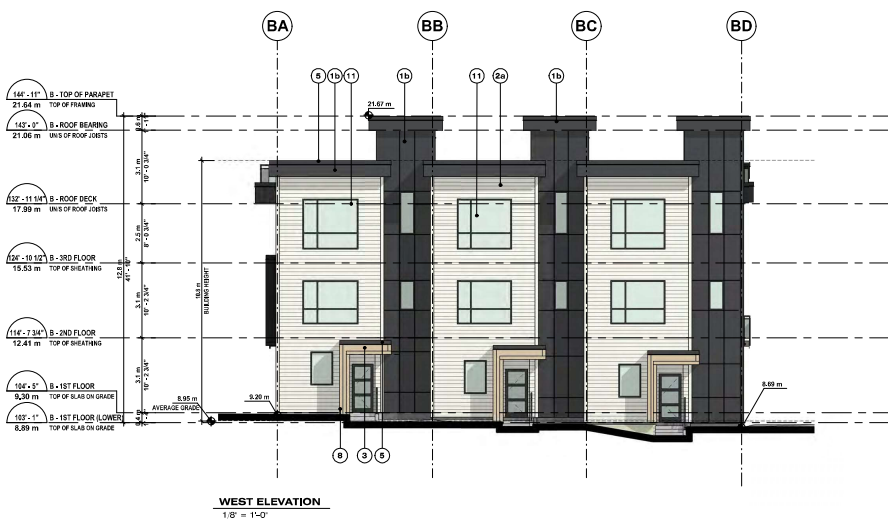


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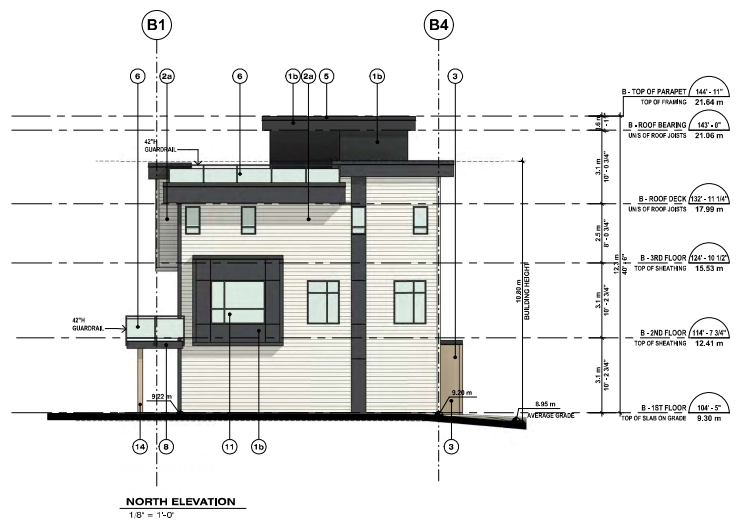
ELEVATIONS

DP3.1-B

SCALE As Indicated



WEST ELEVATION
1/8" = 1'-0"



NORTH ELEVATION
1/8" = 1'-0"



EAST ELEVATION
1/8" = 1'-0"



SOUTH ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES

1 - WALL - PAINTED CEMENTITIOUS PANELS (B) BENJAMIN MOORE: OC-115 SIMPLY WHITE (C) BENJAMIN MOORE: 2104-H WROUGHT IRON	2 - WALL - PAINTED CEMENTITIOUS LAP (B) BENJAMIN MOORE: OC-115 SIMPLY WHITE (C) BENJAMIN MOORE: 2104-H WROUGHT IRON	3 - WALL - WOODONE 'TRUSTSERIE' CEMENTITIOUS PANEL (B) SAND CASTLE	4 - WALL - WOODONE 'TRUSTSERIE' CEMENTITIOUS LAP (B) SAND CASTLE	5 - PARAPET CAP FLASHING - ALUMINUM FACTORY BLACK	6 - GUARDRAIL - GLASS FACE MOUNT - PREFABRICATED ALUMINUM - GENTEK ALUMINUM BLACK - GUARDRAIL - PICKET FACE MOUNT - PREFABRICATED ALUMINUM - GENTEK ALUMINUM BLACK	7 - TRIM - PAINTED IN MESSIAH, CORNER TRIM & FACIAE; TO MATCH AS SHOWN	8 - GLAZING - VINYL TRIM - EXTERIOR FACTORY BLACK & INTERIOR FACTORY WHITE	9 - GLAZING - ALL WEATHER DECK GLAZING - FACTORY BLACK FRAME	10 - CAST-IN-PLACE CONCRETE (B) METALLIC FINISH (C) PAINTED TO MATCH BR. 05-117 SIMPLY WHITE (D) PAINTED TO MATCH BR. 2104-H WROUGHT IRON	11 - TRIMMER COLUMN - STAINED TO MATCH WOODSTONE SAND CASTLE	12 - METAL SFFIT - FACTORY WHITE
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NOTES
1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED
2. METRIC FLOOR GRADE ELEVATIONS ARE ACTUAL AND GEODETIC
3. MAIN FLOOR GEODETIC GRADE ELEVATIONS MUST BE CONFIRMED WITH CIVIL ENGINEERS PRIOR TO EXCAVATION

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

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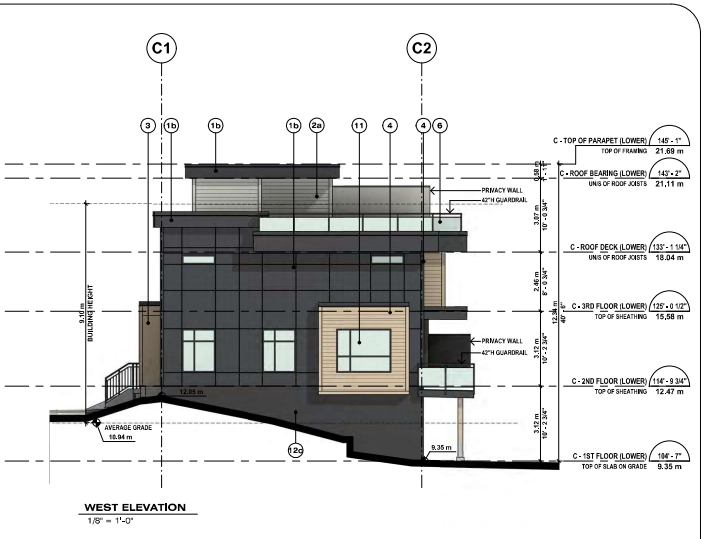
ELEVATIONS

DP3.1-C

SCALE
As Indicated



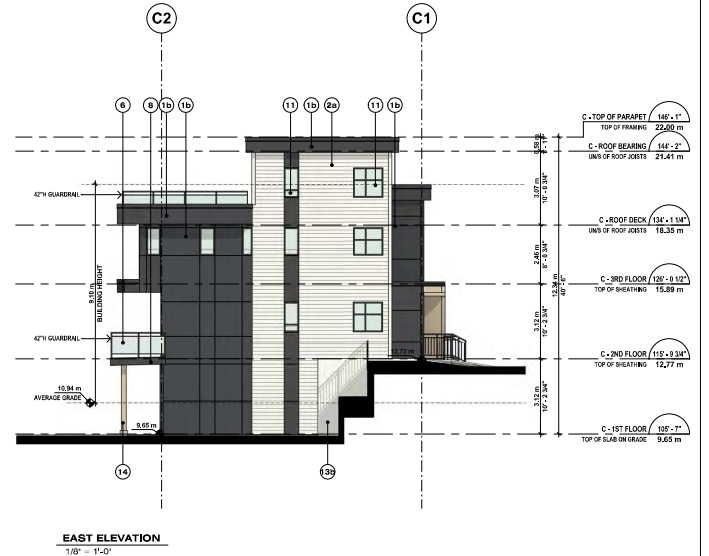
NORTH ELEVATION
1/8" = 1'-0"



WEST ELEVATION
1/8" = 1'-0"



SOUTH ELEVATION
1/8" = 1'-0"



EAST ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES					
1	WALL - PAINTED CEMENTITIOUS PANELS a) BENJAMIN MOORE, OC-117 SIMPLY WHITE b) BENJAMIN MOORE, 214-88 WROUGHT IRON	4	GUARDRAIL - GLASS FACE MOUNT - PREPARED ALUMINUM - CENTER ALUMINUM BLACK	13	CAST-IN-PLACE CONCRETE a) NATURAL FINISH b) PAINTED TO MATCH BM 05-117 SIMPLY WHITE c) PAINTED TO MATCH BM 214-88 WROUGHT IRON
2	WALL - PAINTED CEMENTITIOUS LAP a) BENJAMIN MOORE, OC-117 SIMPLY WHITE b) BENJAMIN MOORE, 214-88 WROUGHT IRON	7	GUARDRAIL - PROJECT FACE MOUNT - PREPARED ALUMINUM - CENTER ALUMINUM BLACK	14	TIMBER COLUMN STAINED TO MATCH WOODSTONE SAND CASTLE
3	WALL - WOODSTONE TRUSTERSERIES CEMENTITIOUS PANEL a) SAND CASTLE	8	TRIM - PAINTED TO MATCH, CORNER TRIM & FASCIA TO MATCH AS SHOWN	15	METAL BOFFIT - FACTORY WHITE
4	WALL - WOODSTONE TRUSTERSERIES CEMENTITIOUS LAP a) SAND CASTLE	11	GLAZING - VINYL TRIM - EXTERIOR FACTORY BLACK & INTERIOR FACTORY WHITE	NOTES	
5	PARAPET CAP FLASHING - ALUMINUM FACTORY BLACK	12	GLAZING - ALL WEATHER DECK GLAZING - FACTORY BLACK FRAME	1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED	
2. METRIC GRADE ELEVATIONS ARE ACTUAL AND GEODETIC					
3. MAIN FLOOR OLD DUCTILE IRON GRADE ELEVATIONS MUST BE CORRELATED WITH CIVIL DRAWINGS PRIOR TO EXCAVATION					

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE

2024-02-06

JOB NO.

2301



2024-02-06

REVISIONS
NO. DATE DESCRIPTION
A 2024-02-06 DP REVISION

ELEVATIONS

DP3.1-F

SCALE
As Indicated



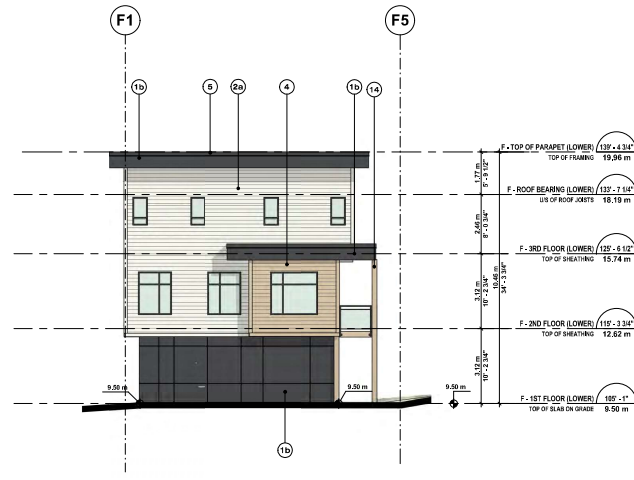
WEST ELEVATION
1/8" = 1'-0"



NORTH ELEVATION
1/8" = 1'-0"



EAST ELEVATION
1/8" = 1'-0"



SOUTH ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES		
1 - WALL - PAINTED CONCRETE/STUCCO PANEL a) BENJAMIN MOORE OC-111 SIMPLY WHITE b) BENJAMIN MOORE 214-19 WROUGHT IRON	6 - GLAZING - GLASS FACE MOUNT a) PREFABRICATED ALUMINUM - GENTLE ALUMINUM BLACK b) PAINTED TO MATCH SW: 214-15 WOODGUY IRON	13 - SCAFFOLD/CLASH CONCRETE a) NATURAL FINISH b) PAINTED TO MATCH SW: 214-15 WOODGUY IRON
2 - WALL - PAINTED CONCRETE/STUCCO LAP a) BENJAMIN MOORE OC-111 SIMPLY WHITE b) BENJAMIN MOORE 214-19 WROUGHT IRON	7 - GLAZING - FRICKET FACE MOUNT a) PREFABRICATED ALUMINUM - GENTLE ALUMINUM BLACK b) PAINTED TO MATCH SW: 214-15 WOODGUY IRON	14 - TIMBER COLUMN STAINED TO MATCH WOODSTONE SAND CASTLE
3 - WALL - WOODSTONE MUSTER/GENIE'S CONCRETE/STUCCO PANEL a) SAND CASTLE	8 - TRIM - PAINTED IN WEDGEMAN CORNER TRIM & FASCIA; TO MATCH SW: 214-15 WOODGUY IRON	15 - METAL SPLIT-T FACTORY WHITE
4 - WALL - WOODSTONE MUSTER/GENIE'S CONCRETE/STUCCO LAP a) SAND CASTLE	11 - GLAZING - HVEL TRIM - EXTERIOR FACTORY BLACK & INTERIOR FACTORY WHITE	NOTES
5 - PARAPET CARPENTRY - ALUMINUM FACTORY BLACK	12 - GLAZING - ALL WEATHER DECK GLAZING - FACTORY BLACK FRAME	1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED. 2. METRIC GRADE ELEVATIONS ARE ACTUAL AND GEODETIC. 3. MAIN FLOOR GEODETIC GRADE ELEVATIONS MUST BE CONFIRMED WITH CIVIL DRAWINGS PRIOR TO EXCAVATION.



EXTERIOR FINISHES			
1	WALL - PAINTED CEMENTITIOUS PANELS a) BENJAMIN MOORE: DQ-117 SMOKE WHITE b) BENJAMIN MOORE: 214-19 WINDOUTREON	6	GLASSING - GLASS FACE MOUNT - PREFABRICATED ALUMINUM - GEMTEK ALUMINUM: BLACK - GLASSING - SOCKET FACE MOUNT - PREFABRICATED ALUMINUM - GEMTEK ALUMINUM: BLACK
2	WALL - PAINTED CEMENTITIOUS LAP a) BENJAMIN MOORE: DQ-117 SMOKE WHITE b) BENJAMIN MOORE: 214-19 WINDOUTREON	7	GLASSING - SOCKET FACE MOUNT - PREFABRICATED ALUMINUM - GEMTEK ALUMINUM: BLACK
3	WALL - WOODTONE MULTI-COURED CEMENTITIOUS PANEL a) SAND CASTLE	8	TRIM - PAINTED TO MATCH MIRROR CORNER TRIM & FASCIA - EXTERIOR FACTORY BLACK & INTERIOR FACTORY WHITE
4	WALL - WOODTONE MULTI-COURED CEMENTITIOUS LAP a) SAND CASTLE	11	GLAZING - VINYL TRIM - EXTERIOR FACTORY BLACK & INTERIOR FACTORY WHITE
5	PARAPET CAP FLASHING - ALUMINUM FACTORY BLACK	12	GLAZING - ALL WEATHER DECK GLAZING - FACTORY BLACK FRAME
13	SCAFFOLD FACE CONCRETE a) NATURAL FINISH b) PAINTED TO MATCH FIN: DQ-117 SMOKE WHITE c) PAINTED TO MATCH FIN: 214-19 WINDOUTREON	14	TRUSS COLUMN STAINED TO MATCH WOODTONE SAND CASTLE
15	METAL SPOFF - FACTORY WHITE		

NOTES
1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED
2. METRIC GRADE ELEVATIONS ARE ACTUAL AND GEODETIC
3. MAIN FLOOR GEODETIC GRADE ELEVATIONS MUST BE CONFIRMED WITH CIVIL DRAWINGS PRIOR TO EXCAVATION

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

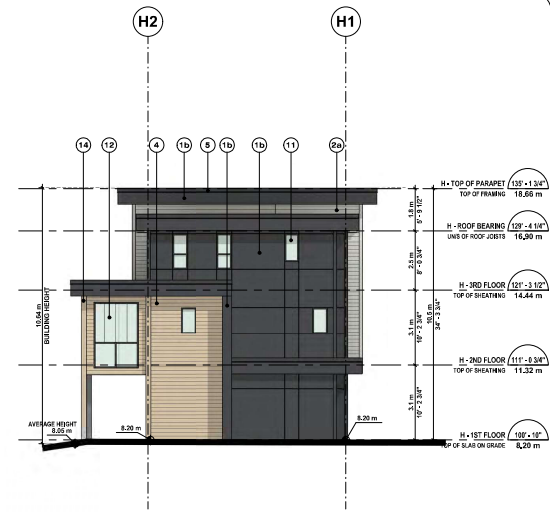
DATE: 2024-02-06
JOB NO.: 2301
REGISTERED ARCHITECT
BRITISH COLUMBIA
2024-02-06

REVISIONS
NO. DATE DESCRIPTION
A 2024-02-06 DP REVISION

ELEVATIONS
DP3.1-G
SCALE As Indicated



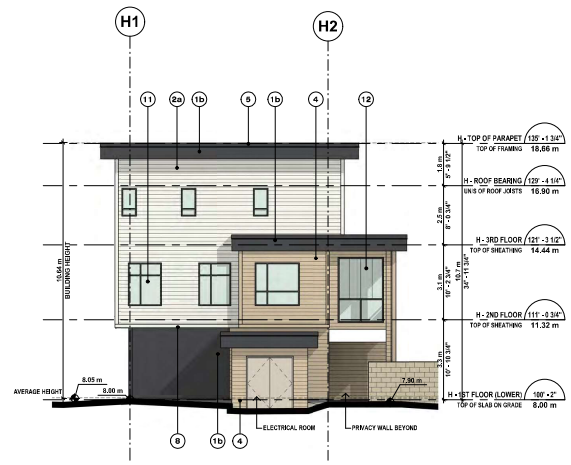
NORTH ELEVATION
1/8" = 1'-0"



EAST ELEVATION
1/8" = 1'-0"



SOUTH ELEVATION
1/8" = 1'-0"



WEST ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES		
1 - WALL - PAINTED CEMENTITIOUS PANELS a) BENJAMIN MOORE: OC-117 BRILL WHITE b) BENJAMIN MOORE: 214-16 WOODGROVE IRON	8 - GUARDRAIL - GLASS FACE MOUNT - PREFABRICATED ALUMINUM - GENTEK ALUMINUM BLACK	13 - CAST-IN-PLACE CONCRETE a) NATURAL FINISH b) PAINTED TO MATCH SW. OC-117 BRILL WHITE
2 - WALL - PAINTED CEMENTITIOUS LAP a) BENJAMIN MOORE: OC-117 BRILL WHITE b) BENJAMIN MOORE: 214-16 WOODGROVE IRON	17 - GUARDRAIL - PROTECT FACE MOUNT - PREFABRICATED ALUMINUM - GENTEK ALUMINUM BLACK	14 - TWISTED COLUMN - STAINING TO MATCH WOODTONE SAND CASTLE
3 - WALL - WOODTONE RUSTICISERIES CEMENTITIOUS PANEL a) SAND CASTLE	18 - TRIM - PAINTED TO MATCH CORNER TRIM & FASCIA - TO MATCH AS SHOWN	15 - METAL SPLIT - FACTORY WHITE
4 - WALL - WOODTONE RUSTICISERIES CEMENTITIOUS LAP a) SAND CASTLE	11 - GLAZING - VINYL TRIM - EXTERIOR FACTORY BLACK & INTERIOR FACTORY WHITE	NOTES
5 - PARAPET CANTYLLASHING - ALUMINUM FACTORY BLACK	12 - GLAZING - ALL WEATHER DECK GLAZING - FACTORY BLACK FRAME	1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED 2. METRIC GRADE ELEVATIONS ARE ACTUAL AND GEODETIC 3. MAIN FLOOR GEODETIC GRADE ELEVATIONS MUST BE CORRELATED WITH CIVIL DRAWINGS PRIOR TO EXCAVATION



THE USER HEREBY AGREES TO THE ASSUMPTION OF ALL PERMITS AND REGULATIONS AND TO BE RESPONSIBLE FOR ANY CHANGES TO THE DRAWINGS. THE ARCHITECT ASSUMES NO RESPONSIBILITY FOR THE QUALITY OF THE WORK OR THE SAFETY OF THE PROJECT. THE ARCHITECT'S LIABILITY IS LIMITED TO THE PROFESSIONAL SERVICES PROVIDED BY THE ARCHITECT.

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE: 2024-02-06
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BRITISH COLUMBIA
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REVISIONS NO.	DATE	DESCRIPTION
A	2024.02.06	DP REVISION

ELEVATIONS

DP3.1-H

SCALE As Indicated

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE

2024-02-06

JOB NO.

2301



2024-02-06

REVISIONS
NO. DATE DESCRIPTION
A 2024.02.06 DP REVISION

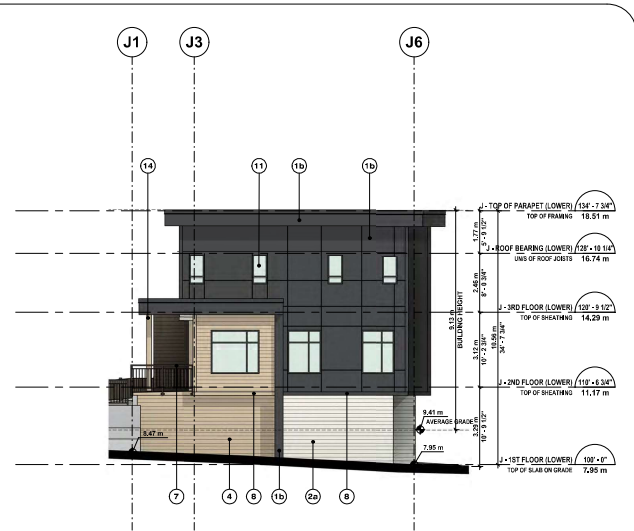
ELEVATIONS

DP3.1-J

SCALE
As Indicated



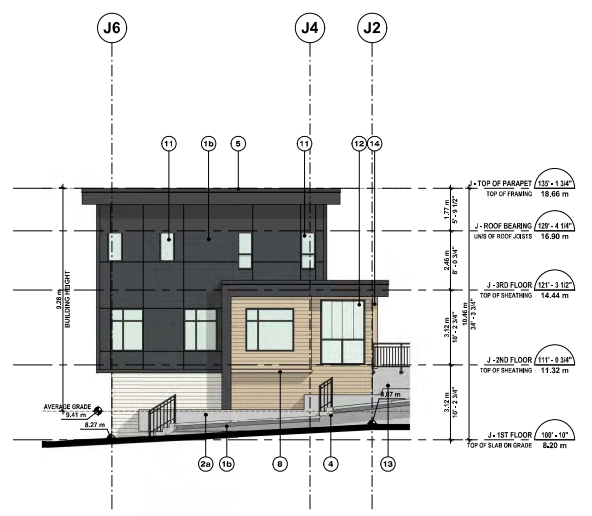
SOUTH ELEVATION
1/8" = 1'-0"



WEST ELEVATION
1/8" = 1'-0"



NORTH ELEVATION
1/8" = 1'-0"



EAST ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES

1 - WALL - PAINTED CEMENTITIOUS PANELS a) BELLAMUN WOOD: DC-117 EMPLOY WHITE	6 - GUARDRAIL - GLASS FACE MOUNT b) PREFABRICATED ALUMINUM - GENTEK ALUMINUM: BLACK	13 - CAST-IN-PLACE CONCRETE a) NATURAL FINISH b) PAINTED TO MATCH BRM 204-19 EMPLOY WHITE c) PAINTED TO MATCH BRM 204-19 WROUGHT IRON
2 - WALL - PAINTED CEMENTITIOUS LAP a) BELLAMUN WOOD: DC-117 EMPLOY WHITE b) BELLAMUN WOOD: 25-GRADE BRIDGECREST TRICK	7 - GUARDRAIL - FIXET FACE MOUNT c) PREFABRICATED ALUMINUM - GENTEK ALUMINUM: BLACK	14 - TRIMMER COLUMN STAINED TO MATCH WOODSTONE 25-GRADE SAND CASTLE
3 - WALL - WOODSTONE RUSTICSERIES CEMENTITIOUS PANEL a) SAND CASTLE	8 - TRIM - PAINTED 2x4 WOODSAND, CORNER TRIM & FASCIA TO MATCH AS SHOWN	15 - METAL SOFFIT - FACTORY WHITE
4 - WALL - WOODSTONE RUSTICSERIES CEMENTITIOUS LAP a) SAND CASTLE	9 - GLAZING - VINYL TRIM b) EXTERIOR FACTORY BLACK KINTERK FACTORY WHITE	
5 - PARAPET CAP/LASING - ALUMINUM FACTORY BLACK	10 - GLAZING - ALL WEATHER DECK GLAZING c) FACTORY BLACK FRAME	

NOTES
1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED
2. METRIC GRADE ELEVATIONS ARE ACTUAL AND GEODEIC
3. MAIN FLOOR GEODEIC GRADE ELEVATIONS MUST BE CORRELATED WITH CIVIL DRAWINGS PRIOR TO CONSTRUCTION

GREYSTONE

MARY HILL BYPASS, PITT RIVER RD & HARBOUR ST, PORT COQUITLAM, BC

DATE

2024.02.06

JOB NO.

2301



REVISONS NO.	DATE	DESCRIPTION
A	2024.02.06	DP REVISION

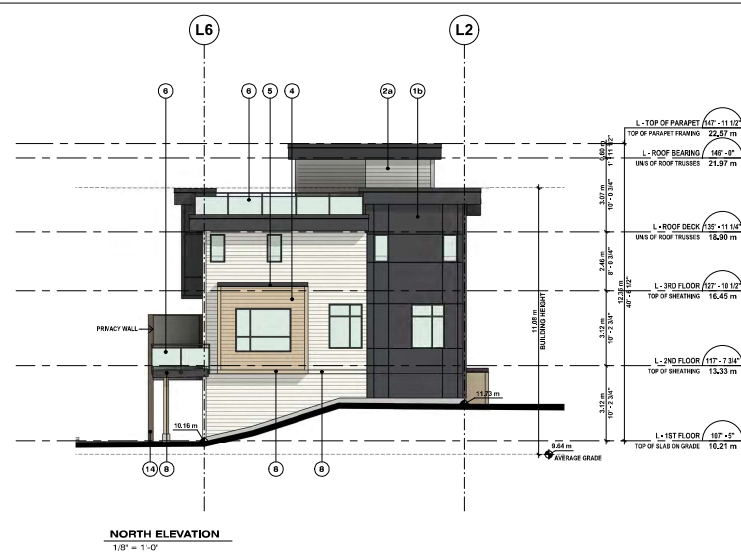
ELEVATIONS

DP3.1-L

SCALE As Indicated



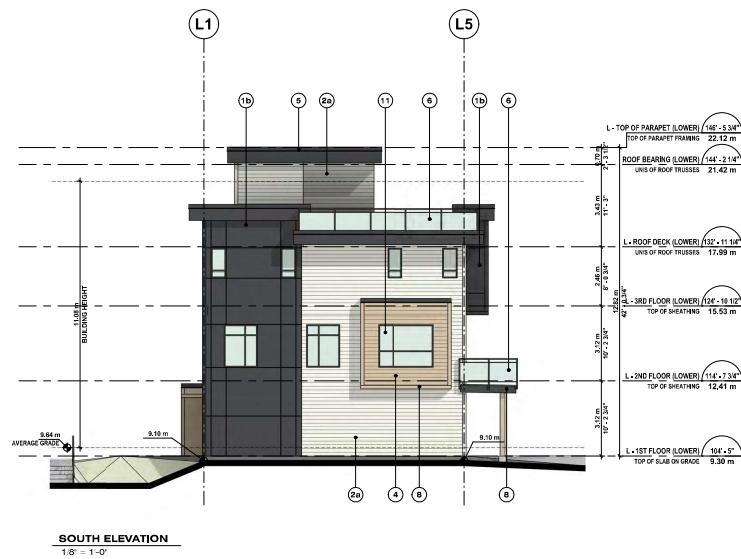
WEST ELEVATION
1/8" = 1'-0"



NORTH ELEVATION
1/8" = 1'-0"



EAST ELEVATION
1/8" = 1'-0"



SOUTH ELEVATION
1/8" = 1'-0"

EXTERIOR FINISHES		
1 - WALL - PAINTED CEMENTITIOUS PANELS a) SENLAWN MOORE: 05-111 SMPLY WHITE b) SENLAWN MOORE: 05-111 SMPLY WHITE c) SENLAWN MOORE: 05-111 SMPLY WHITE	6 - GUARDRAIL - GLASS FACE MOUNT - PREFABRICATED ALUMINUM - GENTEK ALUMINUM BLACK	13 - CAST-IN-PLACE CONCRETE a) NATURAL FINISH b) PAINTED TO MATCH SW: 05-111 SMPLY WHITE c) PAINTED TO MATCH SW: 125-110 WROUGHT IRON
2 - WALL - PAINTED CEMENTITIOUS LAP a) SENLAWN MOORE: 05-111 SMPLY WHITE b) SENLAWN MOORE: 05-111 SMPLY WHITE c) SENLAWN MOORE: 05-111 SMPLY WHITE	7 - GUARDRAIL - PICKET FACE MOUNT - PREFABRICATED ALUMINUM - GENTEK ALUMINUM BLACK	14 - TIMBER COLUMN STAINED TO MATCH WOODWORKERS SAND CASTLE
3 - WALL - WOODTONE "RUSTIC REBEL" CEMENTITIOUS PANEL a) SAND CASTLE	8 - TRIM - PAINTED OR MEDIAN, CORNER TRIM & FASCIA TO MATCH AS SHOWN	15 - METAL ROFFIT - FACTORY WHITE
4 - WALL - WOODTONE "RUSTIC REBEL" CEMENTITIOUS LAP a) SAND CASTLE	11 - GLAZING - VINYL TRIM - EXTERIOR FACTORY BLACK & INTERIOR FACTORY WHITE	
5 - PARAPET CAPSILING - ALUMINUM FACTORY BLACK	12 - GLAZING - ALL WEATHER DECK GLAZING - FACTORY BLACK FRAME	

NOTES
1. IMPERIAL FLOOR GRADE ELEVATIONS ARE ASSUMED
2. METRIC GRADE ELEVATIONS ARE ACTUAL AND GEODETIC
3. MAIN FLOOR GEODETIC GRADE ELEVATIONS MUST BE CORRECTED WITH CIVIL DRAWINGS PRIOR TO EXCAVATION

TABLE 1: GREYSTONE, PORT COQUITLAM

Typical Interior Noise Level Calculation
 (Analysis ref. NRC's IBANA-Calc)

#/UNIT ELEM	LOCATION / SUITE	ROOM TYPE	ROOM ABSORP (A) sq.m	FAÇADE(S)	EXTERIOR NOISE LEVEL (dB)	FAÇADE AREA (S) sq.m	S/A	S/A (dB)	TYPE OF FAÇADE	NOISE REDUCTION (dB)	INTERIOR LEVEL (dB)	TYP. CONST. / DESIGN CRIT. (dB)	MARGIN (dB)	
1.	Bldg. A Unit Type B2 3rd Floor	Master Bedroom (SW corner)	14.3	Pitt River (West, incl. return wall) South	70	Window:	4.9	0.34	-4.7	G36	40.7	29.3	OITC 32 rated window	
					72	Cladding:	13.0	0.91	-0.4	Wf41	41.4	28.6	Fibre-cem. w.res. chan. + 2x GWB	
						Windows:	1.5	0.10	-9.8	G36	45.8	26.2	OITC 32 rated window	
						Cladding:	11.0	0.77	-1.1	Wf41	42.1	29.9	Fibre-cem. w.res. chan. + 2x GWB	
						TOTAL Lp=					35	35	0	
2.	Bldg. A Unit Type B1 3rd Floor	Master Bedroom (West façade)	14.3 (sim. to B2)	Pitt River (West, incl. return walls)	70	Window:	4.9	0.34	-4.7	G36	40.7	29.3	OITC 32 rated window	
						Cladding:	17.5	1.22	0.9	Wf38	37.1	32.9	Fibre-cement w.2x GWB	
						TOTAL Lp=					34	35	1	
3.	Bldg. H Unit Type A3 3rd Floor	Master Bedroom (SW corner)	13.4	Mary Hill (South, incl. return wall) West	73	Window:	4.2	0.31	-5.1	G39	44.1	28.9	OITC 35 rated fenestration	
					71	Cladding:	10.5	0.78	-1.1	Wf41	42.1	30.9	Fibre-cem. w.res. chan. + 2x GWB	
						Window:	0.7	0.06	-12.6	G39	51.6	19.4	OITC 35 rated fenestration	
						Cladding:	11.5	0.86	-0.7	Wf41	41.7	29.3	Fibre-cem. w.res. chan. + 2x GWB	
						TOTAL Lp=					35	35	0	
4.	Bldg. H Unit Type A1 3rd Floor	Master Bedroom (South façade)	13.4 (sim. to A3)	Mary Hill (South, incl. return walls)	73	Window:	4.2	0.31	-5.1	G39	44.1	28.9	OITC 35 rated fenestration	
						Cladding:	13.0	0.97	-0.1	Wf41	41.1	31.9	Fibre-cem. w.res. chan. + 2x GWB	
						TOTAL Lp=					34	35	1	

-Bedrooms considered with 10 ft. clg. ht.

N.B.: Other windows may satisfy design criteria, subject to BSA analyses of final development drawings, façade details and manuf. fenestration acoustical test reports per ASTM E90.

Noise reduction data referencing NRC's IBANA-Calc, related validation studies and statistical traffic source data.

G29: Standard thermal glazing.

G33: OITC 29 rated window fenest. (typ. 6-13-4 or 6-13-6 thermal glazing)

G36: OITC 32 rated window (typ. 6Lam-13-6 glazing)

G39: OITC 35 rated fenestration (stringent design requirement)

G41: OITC 37 rated fenestration (very stringent design requirement)

W34: Standard lightweight ext. construction, e.g. vinyl siding, metal panel, etc.

W36: w. 2x interior GWB, or equivalent.

W39: w. 2x interior GWB on resilient channels.

W45: Heavy exterior finish, e.g. concrete, stone, brick, etc., or equiv. deep void construction.

Ws36: Spandrel assembly with furred GWB assembly, 1x GWB, or equivalent.

Ws38: Spandrel assembly with furred GWB assembly, 2x GWB, or equivalent.

Ws41: Spandrel assembly with furred GWB assembly, 2x GWB on resilient channels.

Wf36: Fibre-cement cladding, or equivalent.

Wf38: Fibre-cement cladding w. 2x interior GWB.

Wf41: Fibre-cement w. 2x interior GWB on resilient channels.

sID27: Standard sliding glass door with standard thermal glazing (OITC 24)

sID30: OITC 27 rated slider (typ. w.6/4 glazing)

sID31: OITC 28 rated slider (typ. w.6Lam/4 glazing)

sID32: OITC 29 rated slider (typ. w.6Lam/6 glazing)

sID35: OITC 32 rated slider (stringent design requirement).

sID38: OITC 35 rated slider (very stringent design requirement).

swD29: Standard exterior swing door with standard thermal glazing (OITC 26)

swD32: OITC 29 rated swing door (typ. w.6/4 glazing)

swD33: OITC 30 rated swing door (typ. w.6Lam/4 glazing)

swD35: OITC 32 rated swing door (stringent design requirement).

REVERBERATION TIME ANALYSIS

FILE:BA-B2MB1.RVB

Bldg. A: Unit Type B2, 3rd Floor, Master Bedroom (west).

FLOOR AREA 15.3 sq m. LENGTH 4.1 m. WIDTH 3.8 m.
 VOLUME 46.5 cu m. HEIGHT 3.0 m.

INDEX	MATERIAL	LIB#	AREA	125	250	500	1kHz	2kHz	4kHz	
N-S WALLS										
	11-Glass, double glazed	44	1	0.3	0.3	0.2	0.1	0.1	0.0	
	12-Interior Door	30	2	0.2	0.2	0.2	0.1	0.1	0.1	
	13-Drywall on Studs, 16"oc	18	20	6.0	2.4	1.2	1.0	1.0	1.0	
	GROUP 1 TOTALS		23	6.5	2.9	1.6	1.2	1.2	1.2	
E-W WALLS										
	21-Glass, double glazed	44	5	1.0	1.0	0.7	0.3	0.2	0.1	
	22-Interior Door	30	2	0.2	0.2	0.2	0.1	0.1	0.1	
	23-Drywall on Studs, 16"oc	18	18	5.5	2.2	1.1	0.9	0.9	0.9	
	GROUP 2 TOTALS		25	6.7	3.3	2.0	1.4	1.3	1.2	
FLOOR-CLG										
	31-Typical carpet.	1	11	1.0	1.1	2.3	2.8	3.4	4.0	
	32-Drywall ceiling.	23	15	2.3	1.5	0.8	0.8	0.8	0.8	
	GROUP 3 TOTALS		27	3.3	2.7	3.0	3.6	4.2	4.7	
COMMON										
	41-Double Bed 2m x 1.5m	17	1	4.5	5.0	5.5	6.0	6.0	6.0	
	42-Misc. Furn. 2m x 1m	33	1	1.5	1.8	2.0	2.0	2.0	2.0	
	43-AIR, 60% RH, per 1000 m ³		0	0.2	0.2	0.2	0.2	0.5	1.4	
	GROUP 4 TOTALS			6.2	6.9	7.7	8.2	8.5	9.4	
TOTAL ABSORPTION				74	22.7	15.8	14.3	14.3	15.0	16.4
ROOM CONSTANTS				74	32.6	20.0	17.6	17.7	18.9	21.1
SABINE REVERB TIME					0.33	0.47	0.53	0.52	0.50	0.46
FITZROY REVERB TIME					0.34	0.46	0.53	0.56	0.55	0.52
NORRIS E REVERB TIME					0.30	0.46	0.51	0.51	0.49	0.45
1-Typical carpet.					0.09	0.10	0.20	0.25	0.30	0.35
17-Double Bed 2m x 1.5m					4.50	5.00	5.50	6.00	6.00	6.00
18-Drywall on Studs, 16"oc					0.30	0.12	0.06	0.05	0.05	0.05
23-Drywall ceiling.					0.15	0.10	0.05	0.05	0.05	0.05
30-Interior Door					0.15	0.11	0.10	0.07	0.06	0.07
33-Misc. Furn. 2m x 1m					3.00	3.50	4.00	4.00	4.00	4.00
44-Glass, double glazed					0.20	0.20	0.15	0.07	0.05	0.03
101-AIR, 60% RH, per 1000 m ³					3.30	3.30	3.30	3.30	10.00	30.00



Brown Strachan Associates
 Consulting Engineers in Acoustics

REVERBERATION TIME ANALYSIS

FILE: BH-A3MB1.RVB

Bldg. H: Unit Type A3, 3rd Floor, Master Bedroom (SW corner).

FLOOR AREA 13.2 sq m. LENGTH 3.7 m. WIDTH 3.6 m.
 VOLUME 40.0 cu m. HEIGHT 3.0 m.

INDEX	MATERIAL	LIB#	AREA	125	250	500	1kHz	2kHz	4kHz	
N-S WALLS										
	11-Glass, double glazed	44	4	0.8	0.8	0.6	0.3	0.2	0.1	
	12-Interior Door	30	3	0.5	0.4	0.3	0.2	0.2	0.2	
	13-Drywall on Studs, 16"oc	18	14	4.3	1.7	0.9	0.7	0.7	0.7	
	GROUP 1 TOTALS		22	5.7	2.9	1.8	1.2	1.1	1.1	
E-W WALLS										
	21-Glass, double glazed	44	1	0.1	0.1	0.1	0.1	0.0	0.0	
	22-Interior Door	30	2	0.2	0.2	0.2	0.1	0.1	0.1	
	23-Drywall on Studs, 16"oc	18	20	6.0	2.4	1.2	1.0	1.0	1.0	
	GROUP 2 TOTALS		22	6.4	2.7	1.5	1.2	1.1	1.1	
FLOOR-CLG										
	31-Typical carpet.	1	9	0.8	0.9	1.8	2.3	2.8	3.2	
	32-Drywall ceiling.	23	13	2.0	1.3	0.7	0.7	0.7	0.7	
	GROUP 3 TOTALS		22	2.8	2.2	2.5	3.0	3.4	3.9	
COMMON										
	41-Double Bed 2m x 1.5m	17	1	4.5	5.0	5.5	6.0	6.0	6.0	
	42-Misc. Furn. 2m x 1m	33	1	1.5	1.8	2.0	2.0	2.0	2.0	
	43-AIR, 60% RH, per 1000 m ³	101	0	0.1	0.1	0.1	0.1	0.4	1.2	
	GROUP 4 TOTALS			6.1	6.9	7.6	8.1	8.4	9.2	
TOTAL ABSORPTION				67	21.0	14.8	13.4	13.5	14.1	15.3
ROOM CONSTANTS				67	30.6	19.0	16.8	16.9	17.9	19.8
SABINE REVERB TIME					0.31	0.44	0.48	0.48	0.46	0.42
FITZROY REVERB TIME					0.31	0.42	0.48	0.50	0.50	0.47
NORRIS E REVERB TIME					0.28	0.42	0.47	0.47	0.45	0.41
1-Typical carpet.					0.09	0.10	0.20	0.25	0.30	0.35
17-Double Bed 2m x 1.5m					4.50	5.00	5.50	6.00	6.00	6.00
18-Drywall on Studs, 16"oc					0.30	0.12	0.06	0.05	0.05	0.05
23-Drywall ceiling.					0.15	0.10	0.05	0.05	0.05	0.05
30-Interior Door					0.15	0.11	0.10	0.07	0.06	0.07
33-Misc. Furn. 2m x 1m					3.00	3.50	4.00	4.00	4.00	4.00
44-Glass, double glazed					0.20	0.20	0.15	0.07	0.05	0.03
101-AIR, 60% RH, per 1000 m ³					3.30	3.30	3.30	3.30	10.00	30.00



Brown Strachan Associates
 Consulting Engineers in Acoustics

BSA CMHC ROAD AND RAIL NOISE v4.3g
 RUN DATE: 04-MAR-24
 File: G-S

CALIBER PROJECTS / THE NERDY ARCHITECT PROJECT NUMBER: A04.182
 Bldg. G: Future traffic noise at south façade.

	Mary Hill WB	Mary Hill EB	Pitt River Rd	
POSTED SPEED..	70 kph 46.1	70 kph 46.1	50 kph 42.5	
VOLUME PER DAY	36100 45.6	41900 46.2	10800 40.3	
% OF TRUCKS...	5.0% 2.2	5.0% 2.2	1.0% 0.7	
DISTANCE.....	23.5m 1.1	36.0m -0.8	100.0m -5.2	
GROUND EFFECT.	(N) 0.0	(N) 0.0	(N) 0.0	
INCLUDED ANGLE	180 deg 0.0	180 deg 0.0	60 deg -4.8	-Eff. view to P.R.R
GRADIENT.....	1.0% 0.3	0.0% 0.0	5.0% 1.7	allows for refl.
INTERSECTION..	108.0m 1.0	108.0m 1.0	108.0m 1.0	effects.
BARRIER EFFECT	#1 (N) 0.0	#2 (N) 0.0	#3 (N) 0.0	
	-----	-----	-----	
	70.3dB	68.7dB	50.2dB	

 TOTAL TRAFFIC NOISE LEVEL: 73dB Leq (24hr)

BSA CMHC ROAD AND RAIL NOISE v4.3g
 RUN DATE: 04-MAR-24
 File: H-S-E

CALIBER PROJECTS / THE NERDY ARCHITECT PROJECT NUMBER: A04.182
 Bldg. H: Future traffic noise at south façade, east end.

	Mary Hill WB	Mary Hill EB	Pitt River Rd	
POSTED SPEED..	70 kph 46.1	70 kph 46.1	50 kph 42.5	
VOLUME PER DAY	36100 45.6	41900 46.2	10800 40.3	
% OF TRUCKS...	5.0% 2.2	5.0% 2.2	1.0% 0.7	
DISTANCE.....	24.0m 1.0	36.5m -0.9	70.0m -3.7	
GROUND EFFECT.	(N) 0.0	(N) 0.0	(N) 0.0	
INCLUDED ANGLE	180 deg 0.0	180 deg 0.0	60 deg -4.8	
GRADIENT.....	1.0% 0.3	0.0% 0.0	5.0% 1.7	
INTERSECTION..	80.0m 1.0	80.0m 1.0	80.0m 1.0	
BARRIER EFFECT	#1 (N) 0.0	#2 (N) 0.0	#3 (N) 0.0	
	-----	-----	-----	
	70.2dB	68.6dB	51.7dB	

 TOTAL TRAFFIC NOISE LEVEL: 73dB Leq (24hr)

BSA CMHC ROAD AND RAIL NOISE v4.3g
 RUN DATE: 04-MAR-24
 File: H-S-W

CALIBER PROJECTS / THE NERDY ARCHITECT PROJECT NUMBER: A04.182
 Bldg. H: Future traffic noise at south façade, west end.

	Mary Hill WB	Mary Hill EB	Pitt River Rd
POSTED SPEED..	70 kph 46.1	70 kph 46.1	50 kph 42.5
VOLUME PER DAY	36100 45.6	41900 46.2	10800 40.3
% OF TRUCKS...	5.0% 2.2	5.0% 2.2	1.0% 0.7
DISTANCE.....	26.0m 0.6	38.5m -1.1	37.0m -0.9
GROUND EFFECT.	(N) 0.0	(N) 0.0	(N) 0.0
INCLUDED ANGLE	180 deg 0.0	180 deg 0.0	60 deg -4.8
GRADIENT.....	1.0% 0.3	0.0% 0.0	5.0% 1.7
INTERSECTION..	53.0m 2.0	53.0m 2.0	53.0m 2.0
BARRIER EFFECT	#1 (N) 0.0	#2 (N) 0.0	#3 (N) 0.0
	-----	-----	-----
	70.8dB	69.4dB	55.5dB

 TOTAL TRAFFIC NOISE LEVEL: 73dB Leq (24hr)

BSA CMHC ROAD AND RAIL NOISE v4.3g
 RUN DATE: 04-MAR-24
 File: H-W

CALIBER PROJECTS / THE NERDY ARCHITECT PROJECT NUMBER: A04.182
 Bldg. H: Future traffic noise at west façade.

	Mary Hill WB	Mary Hill EB	Pitt River Rd	
POSTED SPEED..	70 kph 46.1	70 kph 46.1	50 kph 42.5	
VOLUME PER DAY	36100 45.6	41900 46.2	10800 40.3	
% OF TRUCKS...	5.0% 2.2	5.0% 2.2	1.0% 0.7	
DISTANCE.....	27.0m 0.5	39.5m -1.2	36.0m -0.8	
GROUND EFFECT.	(N) 0.0	(N) 0.0	(N) 0.0	
INCLUDED ANGLE	120 deg -1.8	120 deg -1.8	120 deg -1.8	-Allows for refl.
GRADIENT.....	1.0% 0.3	0.0% 0.0	5.0% 1.7	from Bldg. A.
INTERSECTION..	53.0m 2.0	53.0m 2.0	53.0m 2.0	
BARRIER EFFECT	#1 (N) 0.0	#2 (N) 0.0	#3 (N) 0.0	
	-----	-----	-----	
	68.9dB	67.5dB	58.6dB	

 TOTAL TRAFFIC NOISE LEVEL: 71dB Leq (24hr)

BSA CMHC ROAD AND RAIL NOISE v4.3g
 RUN DATE: 04-MAR-24
 File: A-S-W

CALIBER PROJECTS / THE NERDY ARCHITECT PROJECT NUMBER: A04.182
 Bldg. A: Future traffic noise at south façade, west end.

	Mary Hill WB	Mary Hill EB	Pitt River Rd
POSTED SPEED..	70 kph 46.1	70 kph 46.1	50 kph 42.5
VOLUME PER DAY	36100 45.6	41900 46.2	10800 40.3
% OF TRUCKS...	5.0% 2.2	5.0% 2.2	1.0% 0.7
DISTANCE.....	37.5m -1.0	50.0m -2.2	14.0m 3.3
GROUND EFFECT.	(N) 0.0	(N) 0.0	(N) 0.0
INCLUDED ANGLE	180 deg 0.0	180 deg 0.0	90 deg -3.0
GRADIENT.....	1.0% 0.3	0.0% 0.0	5.0% 1.7
INTERSECTION..	48.0m 2.0	48.0m 2.0	48.0m 2.0
BARRIER EFFECT	#1 (N) 0.0	#2 (N) 0.0	#3 (N) 0.0
	-----	-----	-----
	69.2dB	68.3dB	61.5dB

 TOTAL TRAFFIC NOISE LEVEL: 72dB Leq (24hr)

BSA CMHC ROAD AND RAIL NOISE v4.3g
 RUN DATE: 04-MAR-24
 File: A-W-S

CALIBER PROJECTS / THE NERDY ARCHITECT PROJECT NUMBER: A04.182
 Bldg. A: Future traffic noise at west façade, south end.

	Mary Hill WB	Mary Hill EB	Pitt River Rd
POSTED SPEED..	70 kph 46.1	70 kph 46.1	50 kph 42.5
VOLUME PER DAY	36100 45.6	41900 46.2	10800 40.3
% OF TRUCKS...	5.0% 2.2	5.0% 2.2	1.0% 0.7
DISTANCE.....	38.5m -1.1	51.0m -2.3	13.0m 3.6
GROUND EFFECT.	(N) 0.0	(N) 0.0	(N) 0.0
INCLUDED ANGLE	90 deg -3.0	90 deg -3.0	180 deg 0.0
GRADIENT.....	1.0% 0.3	0.0% 0.0	5.0% 1.7
INTERSECTION..	49.0m 2.0	49.0m 2.0	49.0m 2.0
BARRIER EFFECT	#1 (N) 0.0	#2 (N) 0.0	#3 (N) 0.0
	-----	-----	-----
	66.1dB	65.2dB	64.8dB

 TOTAL TRAFFIC NOISE LEVEL: 70dB Leq (24hr)

BSA CMHC ROAD AND RAIL NOISE v4.3g
 RUN DATE: 04-MAR-24
 File: B-W-S

CALIBER PROJECTS / THE NERDY ARCHITECT PROJECT NUMBER: A04.182
 Bldg. B: Future traffic noise at west façade, south end.

	Mary Hill WB	Mary Hill EB	Pitt River Rd
POSTED SPEED..	70 kph 46.1	70 kph 46.1	50 kph 42.5
VOLUME PER DAY	36100 45.6	41900 46.2	10800 40.3
% OF TRUCKS...	5.0% 2.2	5.0% 2.2	1.0% 0.7
DISTANCE.....	64.0m -3.3	76.5m -4.1	12.5m 3.8
GROUND EFFECT.	(N) 0.0	(N) 0.0	(N) 0.0
INCLUDED ANGLE	90 deg -3.0	90 deg -3.0	180 deg 0.0
GRADIENT.....	1.0% 0.3	0.0% 0.0	5.0% 1.7
INTERSECTION..	74.0m 1.0	74.0m 1.0	74.0m 1.0
BARRIER EFFECT	#1 (N) 0.0	#2 (N) 0.0	#3 (N) 0.0
	-----	-----	-----
	62.9dB	62.4dB	64.0dB

 TOTAL TRAFFIC NOISE LEVEL: 68dB Leq (24hr)

BSA CMHC ROAD AND RAIL NOISE v4.3g
 RUN DATE: 04-MAR-24
 File: C-W-N

CALIBER PROJECTS / THE NERDY ARCHITECT PROJECT NUMBER: A04.182
 Bldg. C: Future traffic noise at west façade, north end.

	Mary Hill E+W	Pitt River Rd	Harbour St.
POSTED SPEED..	70 kph 46.1	50 kph 42.5	50 kph 42.5
VOLUME PER DAY	78000 48.9	10800 40.3	1500 31.8
% OF TRUCKS...	5.0% 2.2	1.0% 0.7	1.0% 0.7
DISTANCE.....	110.0m -5.6	14.0m 3.3	11.5m 4.2
GROUND EFFECT.	(N) 0.0	(N) 0.0	(N) 0.0
INCLUDED ANGLE	90 deg -3.0	180 deg 0.0	90 deg -3.0
GRADIENT.....	1.0% 0.3	5.0% 1.7	4.0% 1.3
INTERSECTION..	115.0m 1.0	115.0m 1.0	20.0m 2.0
BARRIER EFFECT	#1 (N) 0.0	#2 (N) 0.0	#3 (N) 0.0
	-----	-----	-----
	63.9dB	63.5dB	53.5dB

 TOTAL TRAFFIC NOISE LEVEL: 67dB Leq (24hr)

CALIBER PROJECTS / THE NERDY ARCHITECT PROJECT NUMBER: A04.182
 Bldg. C: Future traffic noise at north façade, west end.

Mary Hill E+W Pitt River Rd Harbour St.

POSTED SPEED..	70 kph	46.1	50 kph	42.5	50 kph	42.5
VOLUME PER DAY	78000	48.9	10800	40.3	1500	31.8
% OF TRUCKS...	5.0%	2.2	1.0%	0.7	1.0%	0.7
DISTANCE.....	111.0m	-5.7	15.0m	3.0	10.5m	4.6
GROUND EFFECT.	(N)	0.0	(N)	0.0	(N)	0.0
INCLUDED ANGLE	30 deg	-7.8	90 deg	-3.0	180 deg	0.0
GRADIENT.....	1.0%	0.3	5.0%	1.7	4.0%	1.3
INTERSECTION..	116.0m	1.0	116.0m	1.0	21.0m	2.0
BARRIER EFFECT	#1 (N)	0.0	#2 (N)	0.0	#3 (N)	0.0
		-----		-----		-----
		59.0dB		60.2dB		56.9dB

 TOTAL TRAFFIC NOISE LEVEL: 64dB Leq (24hr)

BSA CMHC ROAD AND RAIL NOISE v4.3g
 RUN DATE: 29-FEB-24
 File: Predict1

CALIBER PROJECTS / THE NERDY ARCHITECT PROJECT NUMBER: A04.182
 Predicted traffic noise level at Test Location.

	Mary Hill WB		Mary Hill EB	
POSTED SPEED..	70 kph	46.1	70 kph	46.1
VOLUME PER DAY	38880	45.9	51000	47.1
% OF TRUCKS...	9.0%	3.4	7.0%	2.8
DISTANCE.....	16.5m	2.6	29.0m	0.1
GROUND EFFECT.	(N)	0.0	(N)	0.0
INCLUDED ANGLE	150 deg	-0.8	150 deg	-0.8
GRADIENT.....	1.0%	0.5	0.0%	0.0
INTERSECTION..	98.0m	1.0	98.0m	1.0
BARRIER EFFECT	#1 (N)	0.0	#2 (N)	0.0
		-----		-----
		72.7dB		70.3dB

 TOTAL TRAFFIC NOISE LEVEL: 75dB Leq (24hr)

BSA CMHC ROAD AND RAIL NOISE v4.3g
 RUN DATE: 29-FEB-24
 File: Predict2

CALIBER PROJECTS / THE NERDY ARCHITECT PROJECT NUMBER: A04.182
 Predicted traffic noise level at Test Location.

	Mary Hill WB		Mary Hill EB		
POSTED SPEED..	60 kph	44.5	70 kph	46.1	-Estimated traffic speed.
VOLUME PER DAY	38880	45.9	51000	47.1	W.Bnd slowing to light at
% OF TRUCKS...	9.0%	3.5	7.0%	2.8	Pitt River Road.
DISTANCE.....	16.5m	2.6	29.0m	0.1	
GROUND EFFECT.	(N)	0.0	(N)	0.0	
INCLUDED ANGLE	150 deg	-0.8	150 deg	-0.8	
GRADIENT.....	1.0%	0.5	0.0%	0.0	
INTERSECTION..	98.0m	1.0	98.0m	1.0	
BARRIER EFFECT	#1 (N)	0.0	#2 (N)	0.0	
		-----		-----	
		71.2dB		70.3dB	

 TOTAL TRAFFIC NOISE LEVEL: 74dB Leq (24hr)

Re: GREYSTONE - Mary Hill Bypass, Pitt River Road & Harbour Street, Port Coquitlam.

Measurement Test Location.

TABLE 2: Measured samples of daytime traffic noise level approximately 1.5m north of south property line and 28m west of east property line (11-Oct-2018, ~2:00pm).
 Conditions: Effective view to traffic on Mary Hill Bypass is ~150 deg. W.Bnd traffic moving slower than 70km/h posted speed limit (Est. 60 km/h).

2 min. Leq Samples



74.5	Dump truck c/w jake brake W.Bnd
72.9	
74.3	
73.8	
73.1	
74.2	
72.2	
76.2	Loud motorbike W.Bnd
74.0	
72.8	Aircraft overhead
72.7	
74.2	
73.5	
73.5	Bus W.Bnd (#791)
74.0	
73.3	

=====

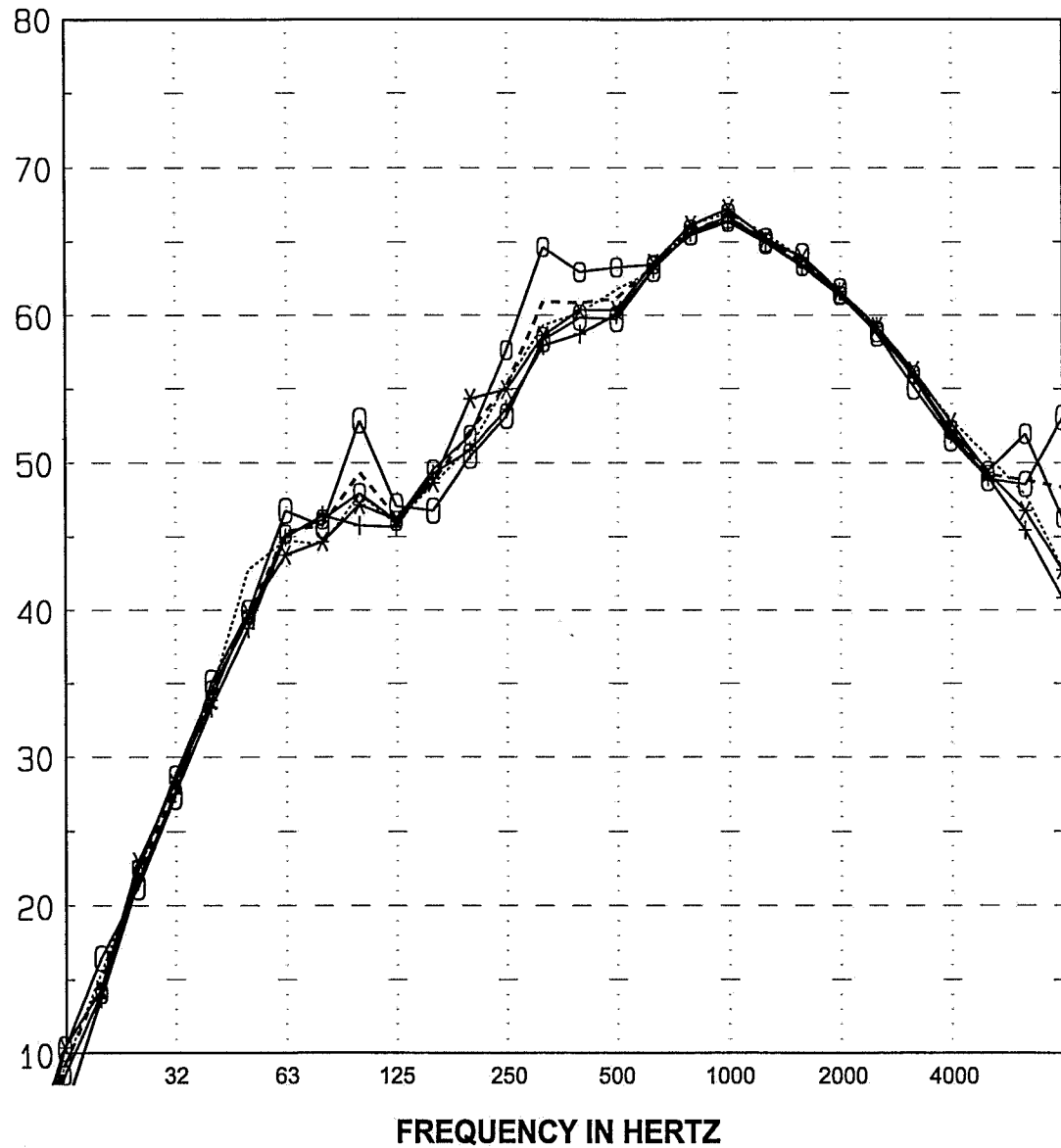
AVERAGE: 74 dBA

Traffic Counts on Mary Hill Bypass:

Direction	VPH	% Heavy Veh.	Equiv. 24 hour Volume
Westbound:	1620	9	38,880
Eastbound:	2125	7	51,000



A-Weighted Third Octave Band Levels (dB re 20 uPa)



LEGEND

- *—* Avg. Leq from Graph: 1.
Leq = 74 dBA.
- o—o Graph: 2, Leq = 74 dBA.
- +—+ Graph: 3, Leq = 73 dBA.
- o—o Graph: 4, Leq = 74 dBA.
- Overall Avg. Leq= 74 dBA
- Add. Leq (20min) = 74 dBA

Third octave band analysis of daytime traffic noise level at Test Location along Mary Hill Bypass, ~1.5m north of south property line & 28m west of the east P.L. (~2pm, 11-OCT-18).

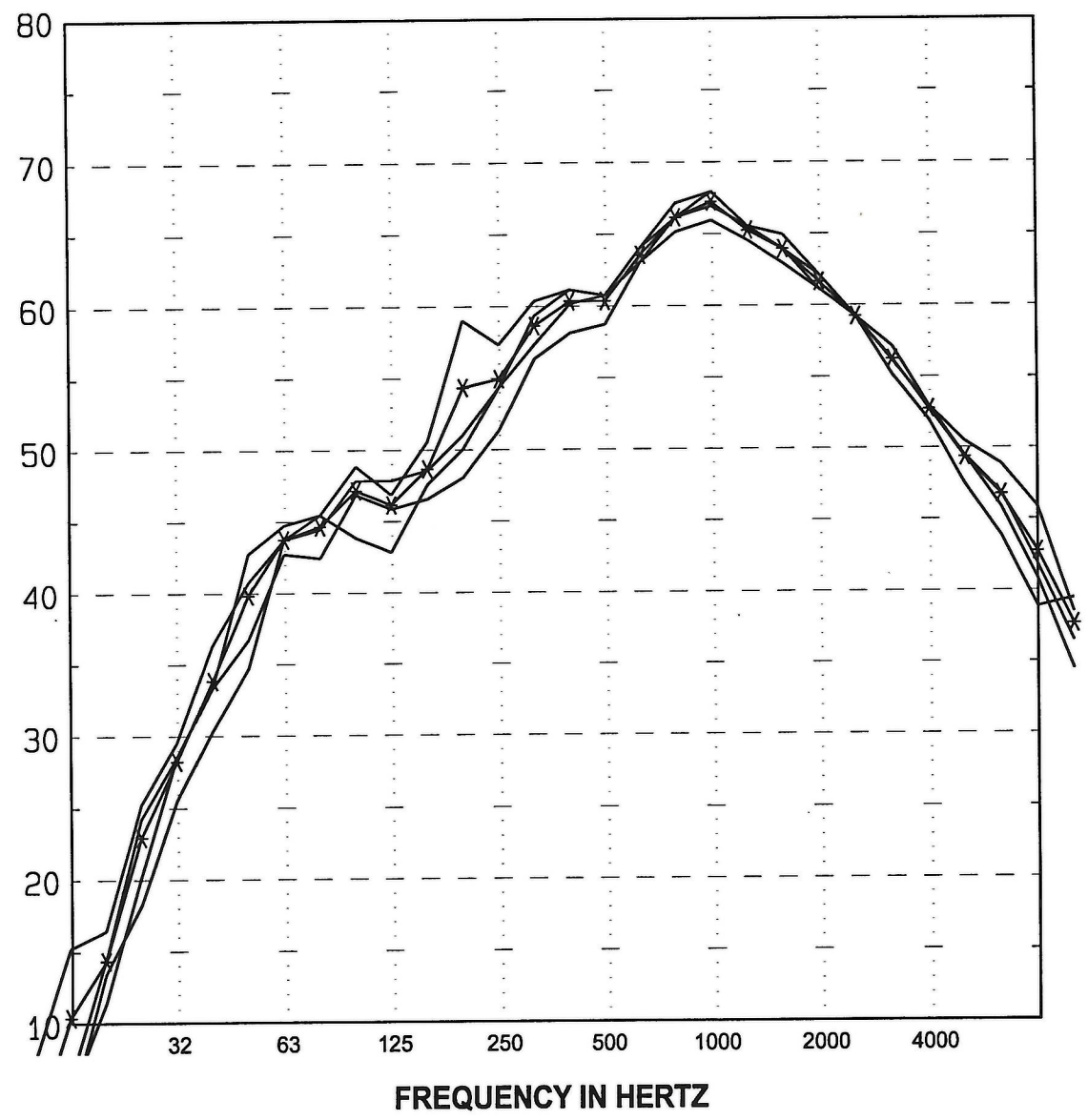
PROJECT
CALIBER / NERDY ARCHITECT
Re: Greystone, Port Coquitlam

GRAPH TITLE
Evaluation of Traffic Noise Levels at Test Location.

GRAPH NUMBER **SUMM**
FILE: SUMMARY

PROJECT NUMBER A04.182	DATE 29-FEB-24
---------------------------	-------------------

A-Weighted Third Octave Band Levels (dB re 20 uPa)



LEGEND

- Measured samples of daytime traffic noise level at Test Location.
- *—* Average Leq = 74 dBA. Leq (2min) samples.

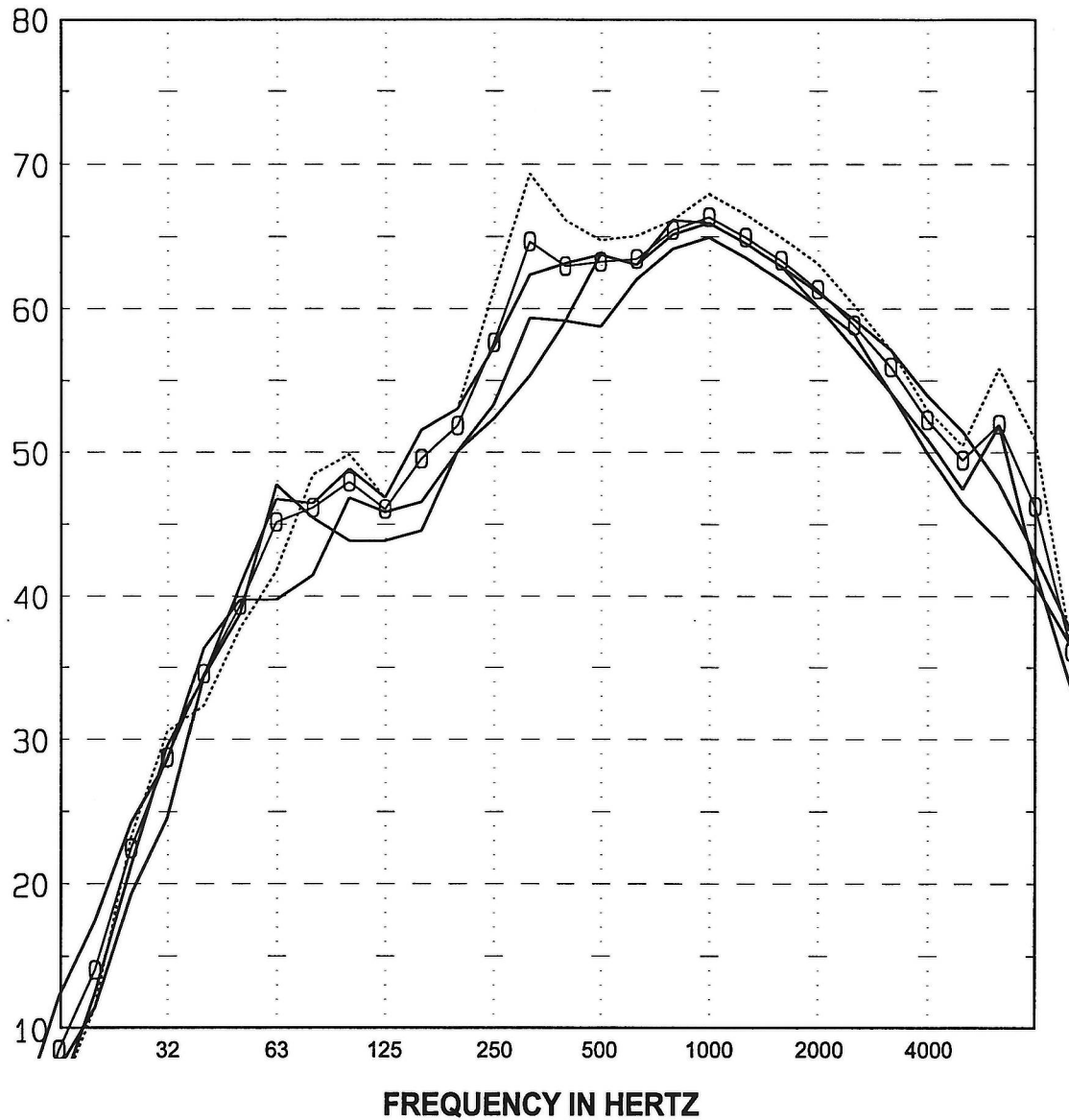
PROJECT
 CALIBER / NERDY ARCHITECT
 Re: Greystone, Port Coquitlam

GRAPH TITLE
 Evaluation of Traffic Noise Levels at Test Location.

GRAPH NUMBER **1**
 FILE: 1

PROJECT NUMBER A04.182	DATE 29-FEB-24
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A-Weighted Third Octave Band Levels (dB re 20 uPa)



LEGEND

- Additional samples of daytime traffic noise level at Test Location.
- Motorbike W.Bnd 76 dBA.
- Average Leq = 74 dBA.

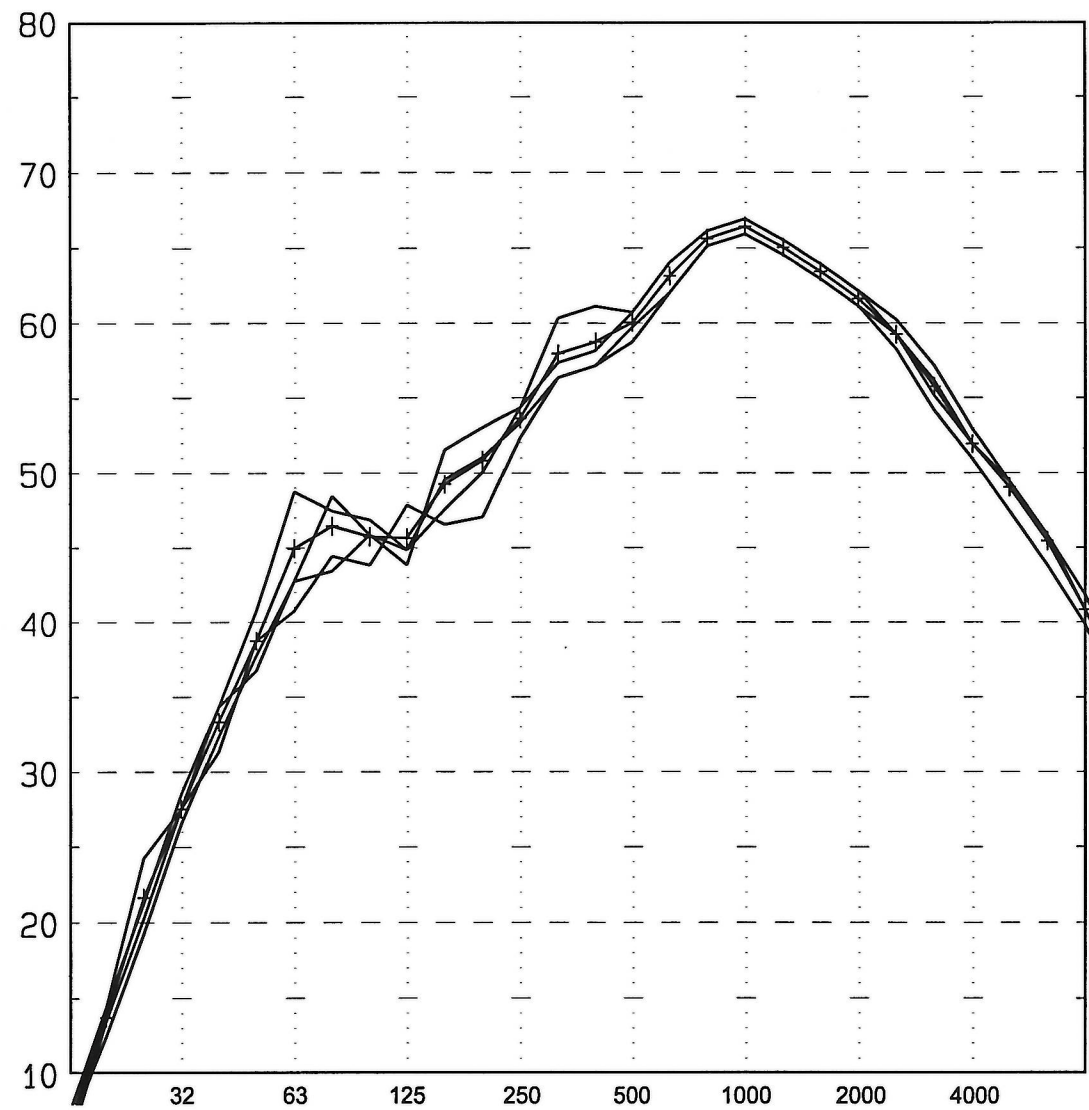
PROJECT
 CALIBER / NERDY ARCHITECT
 Re: Greystone, Port Coquitlam

GRAPH TITLE
 Evaluation of Traffic Noise Levels at Test Location.

GRAPH NUMBER **2**
 FILE: 2

PROJECT NUMBER A04.182	DATE 29-FEB-24
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A-Weighted Third Octave Band Levels (dB re 20 uPa)



LEGEND

— Additional samples of traffic noise level at Test Location.

+ Average Leq = 73 dBA.

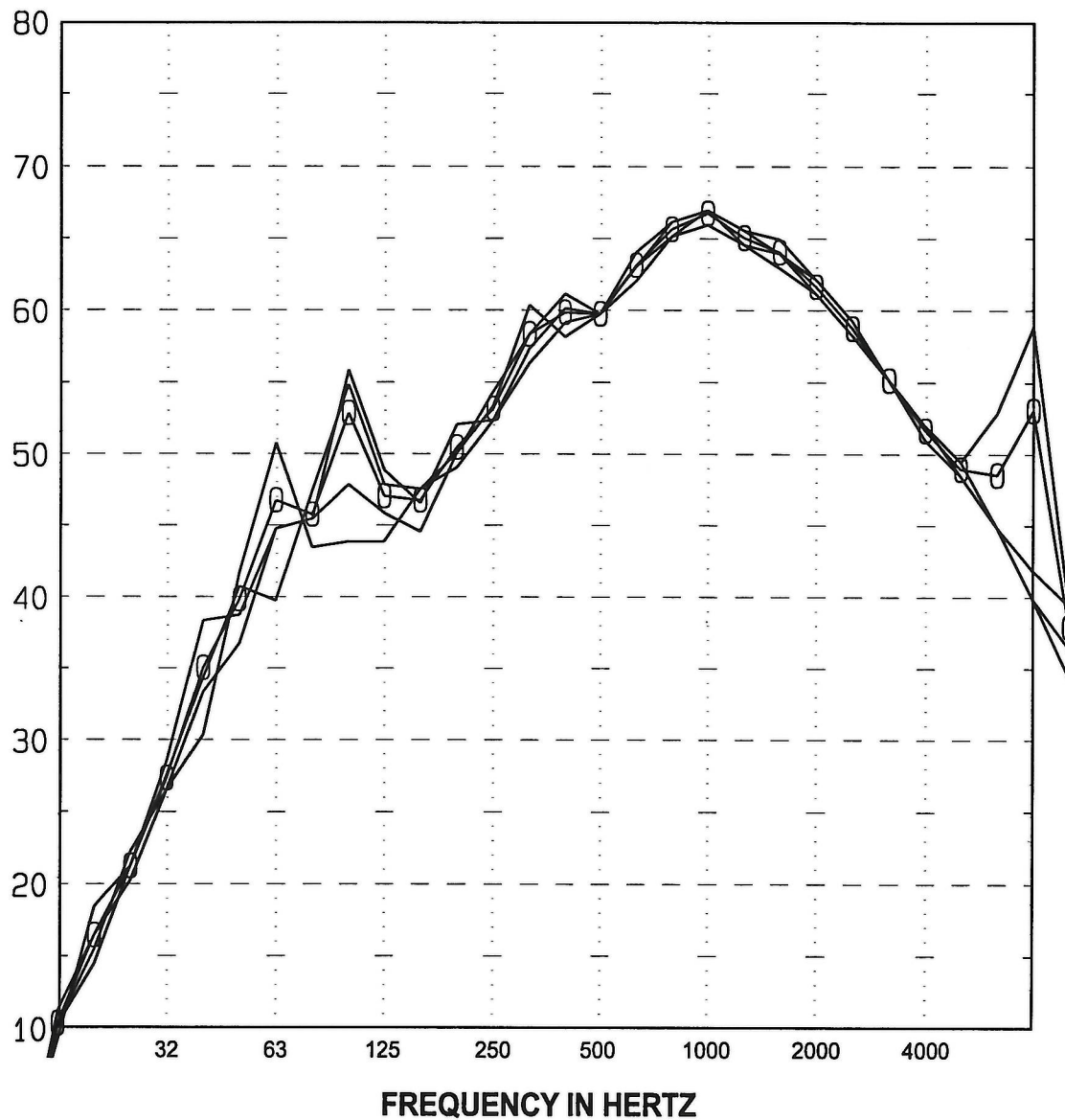
PROJECT
 CALIBER / NERDY ARCHITECT
 Re: Greystone, Port Coquitlam

GRAPH TITLE
 Evaluation of Traffic Noise Levels at Test Location.

GRAPH NUMBER **3**
 FILE: 3

PROJECT NUMBER A04.182	DATE 29-FEB-24
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A-Weighted Third Octave Band Levels (dB re 20 uPa)



LEGEND

— Additional samples of daytime traffic noise level at Test Location.

○ Average Leq = 74 dBA.

PROJECT
CALIBER / NERDY ARCHITECT
Re: Greystone, Port Coquitlam

GRAPH TITLE
Evaluation of Traffic Noise Levels at Test Location.

GRAPH NUMBER
FILE: 4 4

PROJECT NUMBER A04.182	DATE 29-FEB-24
---------------------------	-------------------

Greystone Development Traffic Impact Assessment



Prepared for:

Schmidt & Associates Development Planning Ltd.



Prepared by:



Date: April 5, 2018

Our File No: 5776-01

BY EMAIL

Laurie Schmidt
Schmidt & Associates Development Planning Ltd.
Suite 1440, 1166 Alberni Street
Vancouver, BC
V6E 3Z3

Dear Mr. Schmidt,

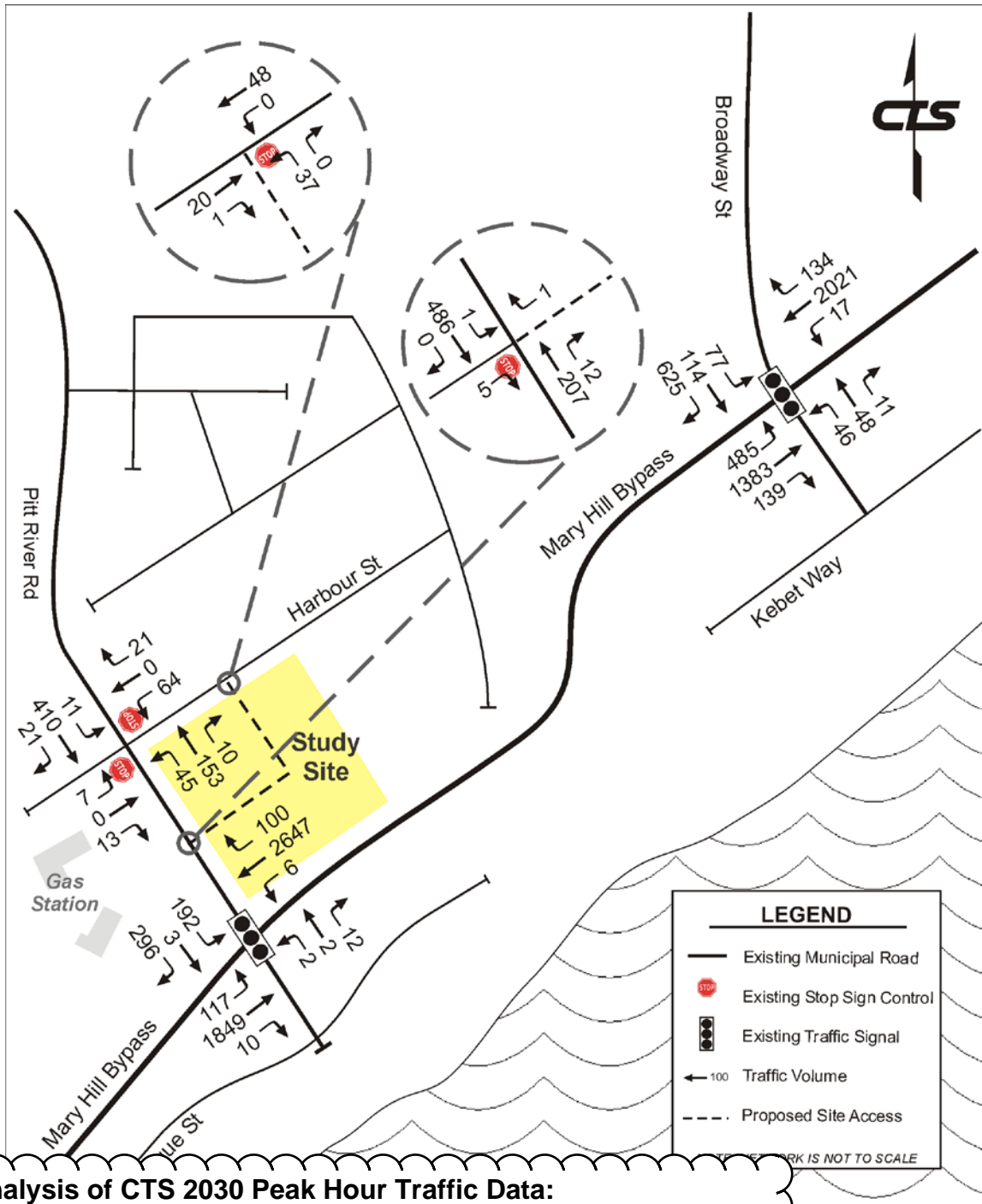
Re: Greystone Development, Port Coquitlam - FINAL Traffic Impact Assessment

Creative Transportation Solutions Ltd. (CTS) is pleased to submit this FINAL Traffic Impact Assessment for the proposed mixed-use Greystone Development in the City of Port Coquitlam.

The primary objectives of this assignment were:

1. To conduct a traffic impact assessment of the proposed mixed-use Greystone Development; and
2. To prepare a report that documents the technical analysis, key findings and recommendations (if any) to meet the transportation requirements of development as set out by the City of Port Coquitlam and Ministry of Transportation and Infrastructure (MOTI).

FIGURE 18
2030 (Build-out + 10 Years) Weekday Morning Peak Hour Base + Site Traffic Volumes



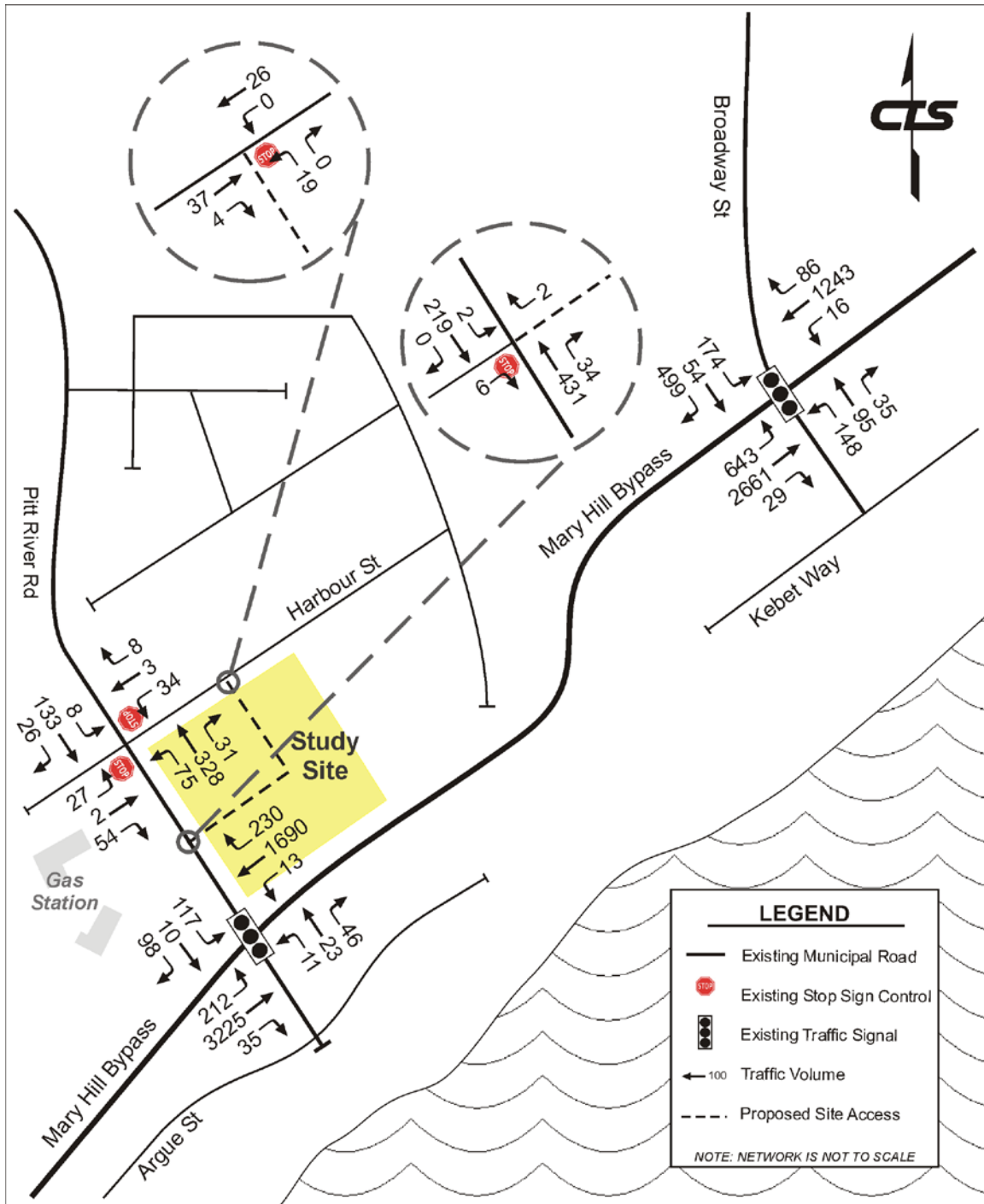
BSA Analysis of CTS 2030 Peak Hour Traffic Data:

	a.m.	p.m.	a.m. + p.m.	Est. Daily Volume*
Mary Hill W.Bnd:	2753	1933	4686	~36,100 vpd
Mary Hill E.Bnd:	2053	3388	5441	~41,900 vpd
Pitt River (total):	710	690	1400	~10,800 vpd
Harbour (total):	106	86	192	~1,500 vpd.

(p.m. next page)

*Est. based on a.m. + p.m. peak hour data being equivalent to 13% of the daily total traffic (data appended).

FIGURE 19
2030 (Build-out + 10 Years) Weekday Afternoon Peak Hour Base + Site Traffic Volumes



British Columbia Ministry of Transportation

Daily Volume from 08/18/2007 through 08/23/2007

Site Names: 16-191EW

County:

Funct. Class:

Location: Route 7B (Marv Hill Bypass), 0.8 Km East Of United Boulevard, Coquitlam

Seasonal Factor Type: Consistent

Daily Factor Type: Consistent

Axle Factor Type:

Growth Factor Type: Consistent

	08/19/2007			08/20/2007			08/21/2007			08/22/2007			08/23/2007			08/24/2007			08/25/2007		
	Road	Neg	Pos	Road	Neg	Pos	Road	Neg	Pos	Road	Neg	Pos	Road	Neg	Pos	Road	Neg	Pos	Road	Neg	Pos
00:00	1,139	401	738	608	201	407	550	174	376	658	207	451	680	205	475						
01:00	757	288	469	320	99	221	327	94	233	378	130	248	416	143	273						
02:00	517	188	329	231	102	129	285	128	157	289	128	161	334	171	163						
03:00	326	118	208	250	149	101	258	162	96	270	163	107	333	187	146						
04:00	294	161	133	598	438	160	594	425	169	584	432	152									
05:00	524	313	211	2,178	1,737	441	2,239	1,746	493	2,321	1,814	507									
06:00	782	472	310	3,145	2,072	1,073	3,068	1,948	1,120	3,060	1,966	1,094									
07:00	959	606	353	3,276	1,985	1,291	3,383	1,980	1,403	3,365	2,037	1,328									
08:00	1,158	723	435	3,069	1,819	1,250	3,068	1,847	1,221	3,274	1,924	1,350									
09:00	1,623	1,034	589	2,829	1,654	1,175	3,152	1,912	1,240	3,064	1,816	1,248									
10:00	2,183	1,416	767	3,068	1,793	1,275	2,970	1,729	1,241	3,165	1,871	1,294									
11:00	2,564	1,473	1,091	3,177	1,760	1,417	3,292	1,822	1,470	3,230	1,738	1,492									
12:00	2,874	1,613	1,261	3,198	1,718	1,480	3,404	1,772	1,632	3,326	1,773	1,553									
13:00	3,065	1,681	1,384	3,223	1,661	1,562	3,521	1,746	1,775	3,543	1,727	1,816									
14:00	3,206	1,604	1,602	3,744	1,676	2,068	3,926	1,712	2,214	4,119	1,780	2,339									
15:00	3,055	1,378	1,677	4,342	1,652	2,690	4,041	1,671	2,370	4,542	1,817	2,725									
16:00	3,054	1,372	1,682	4,549	1,612	2,937	4,558	1,623	2,935	4,631	1,581	3,050									
17:00	3,049	1,372	1,677	4,220	1,403	2,817	4,398	1,475	2,923	4,340	1,457	2,883									
18:00	2,601	1,138	1,463	3,272	1,142	2,130	3,708	1,299	2,409	3,658	1,336	2,322									
19:00	2,146	1,001	1,145	2,441	1,019	1,422	2,808	1,114	1,694	2,872	1,211	1,661									
20:00	2,036	1,014	1,022	1,899	756	1,143	2,210	959	1,251	2,402	958	1,444									
21:00	1,915	868	1,047	1,725	686	1,039	1,971	788	1,183	2,213	909	1,304									
22:00	1,474	606	868	1,395	530	865	1,513	569	944	1,704	652	1,052									
23:00	903	321	582	954	300	654	1,121	351	770	1,082	382	700									
Volume	42,204	21,161	21,048	57,711	27,964	29,747	60,365	29,046	31,318	62,090	29,809	32,281	1,763	706	1,057						
AM Peak Vol	2,564	1,473	1,090	3,281	2,085	1,419	3,383	2,053	1,479	3,516	2,131	1,492									
AM Peak Fct	0.87	0.93	0.79	0.94	0.96	0.91	0.92	0.97	0.92	0.93	0.92	0.90									
AM Peak Hr	11:00	11:00	11:00	6:30	5:45	11:00	7:00	6:15	11:00	7:30	7:15	11:00									
PM Peak Vol	3,206	1,689	1,718	4,577	1,730	2,978	4,615	1,776	3,020	4,706	1,817	3,050									
PM Peak Fct	0.97	0.98	0.97	0.95	0.92	0.98	0.97	0.95	0.97	0.97	0.95	0.95									
PM Peak Hr	14:00	12:30	15:30	16:30	12:30	16:30	16:30	14:15	16:15	15:15	15:00	16:00									
Seasonal Fct	0.940	0.940	0.940	0.940	0.940	0.940	0.940	0.940	0.940	0.940	0.940	0.940	0.940	0.940	0.940						
Daily Fct	1.168	1.168	1.168	1.041	1.041	1.041	0.984	0.984	0.984	0.960	0.960	0.960	0.941	0.941	0.941						
Axle Fct	0.500	0.500	0.500	0.500	0.500	0.500	0.500	0.500	0.500	0.500	0.500	0.500	0.500	0.500	0.500						
Pulse Fct	2.000	2.000	2.000	2.000	2.000	2.000	2.000	2.000	2.000	2.000	2.000	2.000	2.000	2.000	2.000						

*BSA Analysis of MoTI Traffic Data:
 AM + PM peak hour data are
 equivalent to 13% of daily traffic.*



Pitt River Rd & Mary Hill Bypass

Tuesday, January 09, 2018

Vehicle Classification Summary

Project: #5776: Greystone Mixed Use Traffic Impact Assessment
Municipality: Port Coquitlam
Weather: Cloudy, Sunny

Time Period	Entering Intersection	Vehicle Classification				Total
		Passenger Cars	Heavy Vehicles (3 or more axles)			
Morning (07:00 - 09:00)	Volume	7,531	302			7,833
	%	96.1%	3.9%			100.0%
Midday (11:00 - 13:00)	Volume	5,315	447			5,762
	%	92.2%	7.8%			100.0%
Afternoon (15:00 - 18:00)	Volume	12,665	270			12,935
	%	97.9%	2.1%			100.0%
Total (7 Hours)	Volume	25,511	1,019			26,530
	%	96.2%	3.8%			100.0%

Mary Hill Design = 5% heavy veh.



Pitt River Rd & Harbour St - Harbour Ave

Tuesday, January 09, 2018

Vehicle Classification Summary

Project: #5776: Greystone Mixed Use Traffic Impact Assessment
Municipality: Port Coquitlam
Weather: Cloudy, Sunny

Time Period	Entering Intersection	Vehicle Classification				Total
		Passenger Cars	Heavy Vehicles (3 or more axles)			
Morning (07:00 - 09:00)	Volume	1,115	5			1,120
	%	99.6%	0.4%			100.0%
Midday (11:00 - 13:00)	Volume	748	0			748
	%	100.0%	0.0%			100.0%
Afternoon (15:00 - 18:00)	Volume	1,957	0			1,957
	%	100.0%	0.0%			100.0%
Total (7 Hours)	Volume	3,820	5			3,825
	%	99.9%	0.1%			100.0%

Pitt River Road & Harbour Street Design = 1% heavy veh.

Date: January 3, 2024
Our File No: 5776-01

BY EMAIL

Riley Schmidt
Development Manager
Suite 205, 6360 202nd Street
Langley, BC
V2Y 1N2

Dear Mr. Schmidt:

Re: Greystone Development, Port Coquitlam – Updated Traffic Impact Assessment Rationale

The following comparatively analyses and rationalizes the lack of need for an updated Traffic Impact Assessment for the redevelopment of the Greystone site, in Port Coquitlam.

Site

The 2018 Greystone Development proposal comprised 132 condominium units and 12,000 ft² (1,114.84 m²) of commercial/retail.

The 2024 Greystone Development proposal comprises 43 townhome units and no commercial/retail. The 2024 Greystone Development proposal is substantially less in unit number and scale.

Traffic Generation

The 2018 Greystone Development proposal was expected to generate an additional 52 vehicle trips (14 inbound, 38 outbound) in the morning peak hour and 61 vehicle trips (40 inbound, 21 outbound) in the afternoon peak hour.

The 2024 Greystone Development proposal is expected to generate an additional 21 vehicle trips (5 inbound, 16 outbound) in the morning peak hour and 25 vehicle trips (15 inbound, 10 outbound) in the afternoon peak hour. The 2024 Greystone Development proposal is expected to generate between 41% and 48% fewer vehicle trips, than the 2018 Greystone Development proposal.

Note that new development generating less than 30 new vehicle trips generally does not warrant a Traffic Impact Assessment, by traffic engineering standards.

Capacity Analysis

The 2018 Greystone Development proposal included capacity analysis for the intersection of Harbour Road and Pitt River Road. The level of service for all current and future design conditions for the intersection, was LOS A (Excellent).

Given the 2024 Greystone Development proposal is expected to generate between 41% and 48% fewer vehicles trips, it can also be expected that the level of service for all current and future design conditions for the intersection, will be LOS A (Excellent).

Parking

The 2024 Greystone Development proposal will:

1. Meet the residential and visitor vehicle parking requirements per the City of Port Coquitlam *Parking and Development Management Bylaw No. 3525, Section 6 – Required Off-Street Parking Spaces.*
2. Meet the bicycle parking requirements per City of Coquitlam *Zoning Bylaw No. 3630, Section 10 – Bicycle Facilities.*
3. Meet the loading requirements per the City of Port Coquitlam *Parking and Development Management Bylaw No. 3525, Section 11 – Required Off-Street Loading Spaces.*

Access

The 2024 Greystone Development proposal intends on maintaining a single point of access on Harbour Road at the same location as that identified by the 2018 Greystone Development proposal. There will be no point of access on Pitt River Road.

The driveway crossing will be designed with sufficient width and throat length to accommodate turning movements for a garbage truck and/or fire truck and to ensure no queue spill back onto the adjacent street. Internally, the drive aisle shall be of sufficient width to accommodate turning movements for a garbage truck and/or fire truck.

Sight lines to/from the point of access on Harbour Road meet the minimum for a Stopping Sight Distance for a road posted at 50 km/h. That is 65 meters.


Summary

Given the preceding comparative analysis and rationalization, CTS confirms that an Updated Traffic Impact Assessment for the 2024 Greystone Development proposal, is not warranted.

In closing, please contact the undersigned should there be questions and/or comments concerning this Updated Traffic Impact Assessment Rationale.

Yours truly,

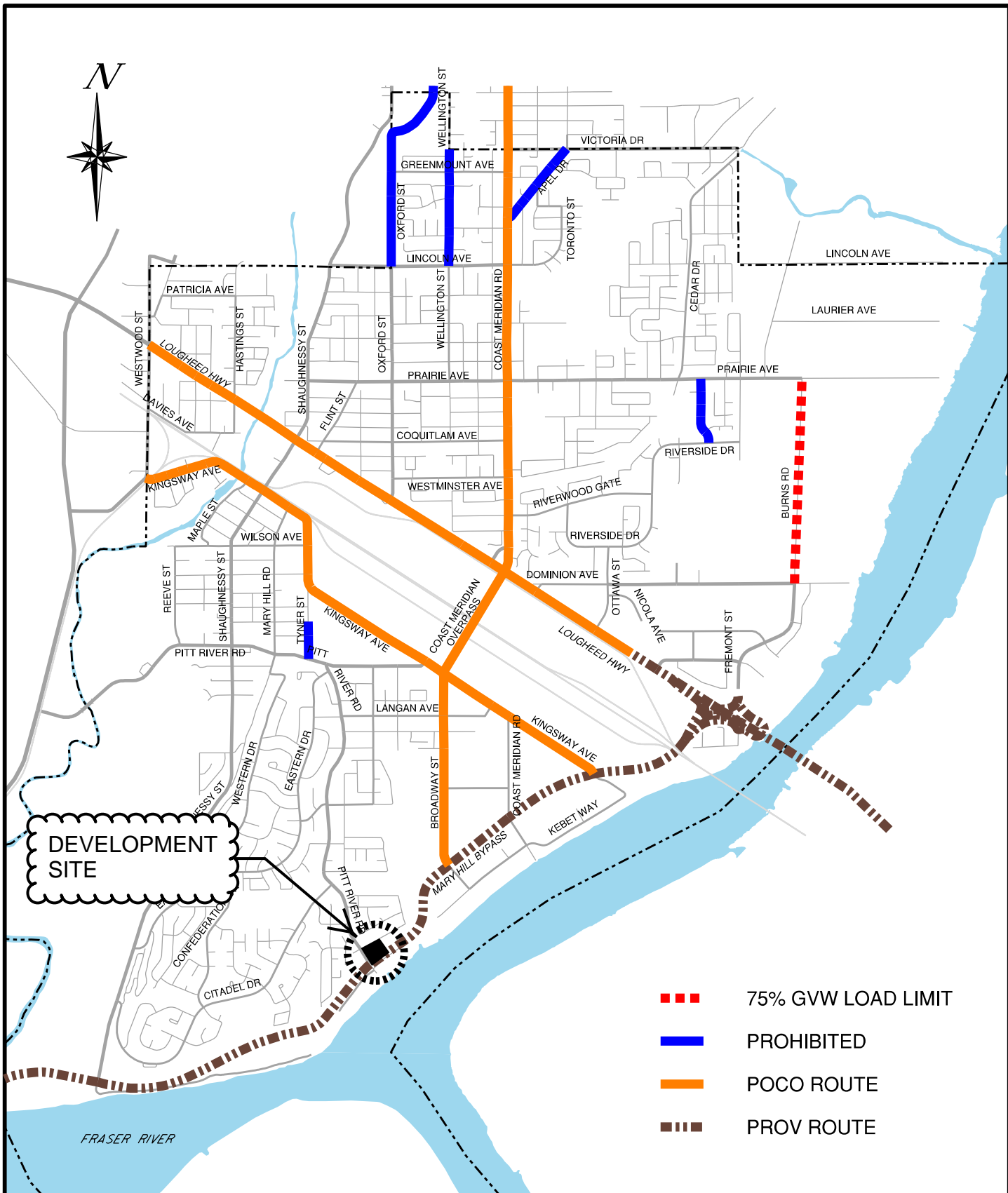
**CREATIVE TRANSPORTATION SOLUTIONS LTD.
PERMIT TO PRACTICE NO. 1000697**

A handwritten signature in black ink, appearing to read "BADOZZI". The signature is stylized with a large "B" and a long, sweeping underline.

Brent A. Dozzi, P. Eng.
Senior Traffic Engineer and Project Manager

Phone: (604) 936-6190 x 237

Email: bdozzi@cts-bc.com



- - - 75% GVW LOAD LIMIT
- PROHIBITED
- POCO ROUTE
- - - PROV ROUTE

TITLE:
TRUCK ROUTE MAP

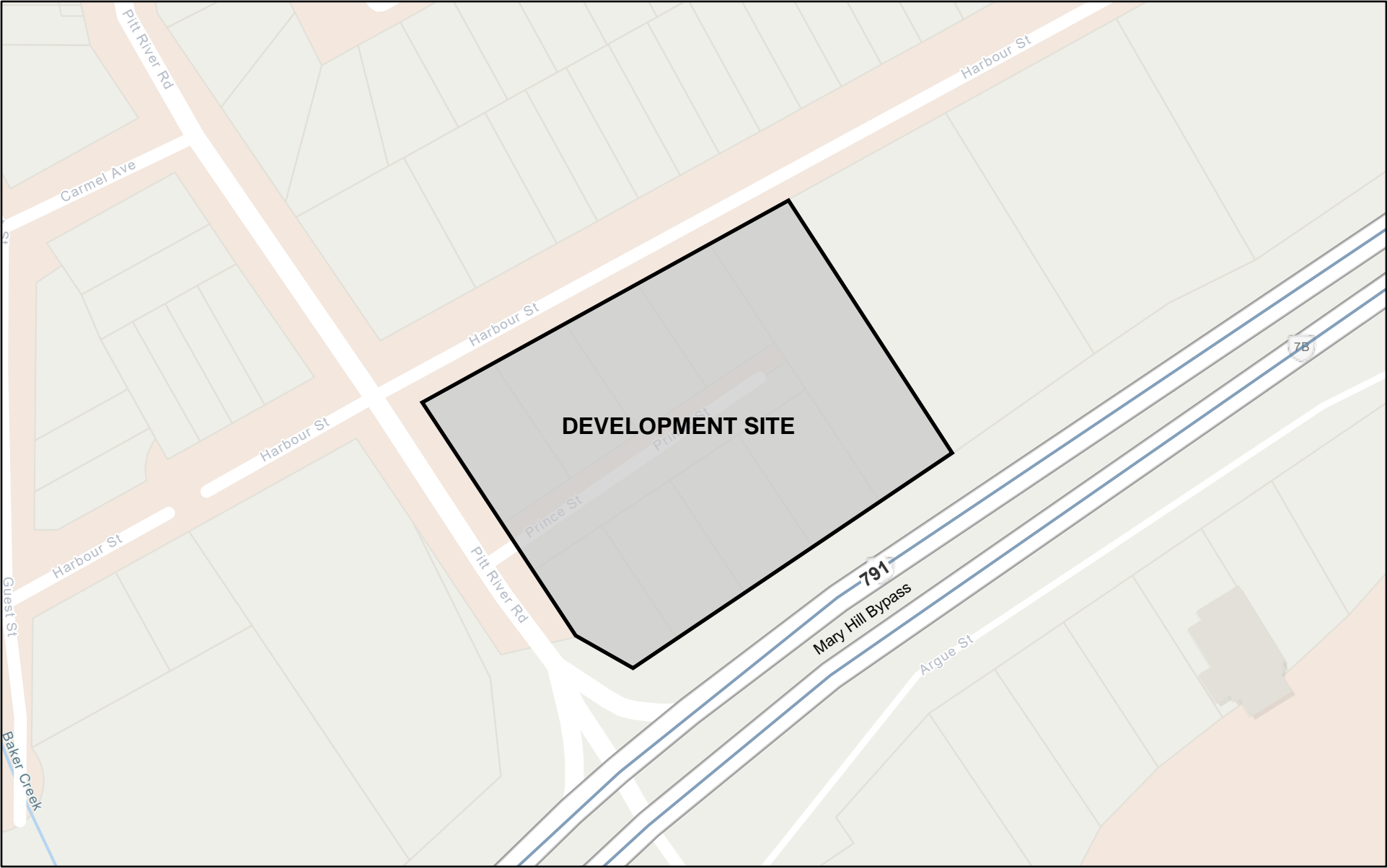
NOTE:
The City of Port Coquitlam makes no representations as to, and does not warrant, the accuracy or completeness of the data and the City of Port Coquitlam is not responsible for any errors or omissions in the data.
The User waives any right to claim damages or pursue any other remedy against the City of Port Coquitlam that arise out of or in connection with the delivery or use of the data.

DRAWING NO.:

SCALE:
NTS
DATE:
May 13, 2015



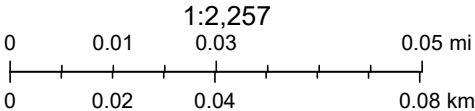
TransLink System Map



04/03/2024, 18:01:01

Lines

Basic



Esri Community Maps Contributors, Esri Canada, Esri, TomTom, Garmin, SafeGraph, GeoTechnologies, Inc, METI/NASA, USGS, EPA, NPS, US

TransLink

File Number: **RZ000261 & DP000551**

November 1, 2023

Prince Holding Ltd
c/o Jerry Pol
Caliber Projects
205-6360 202 Street
Langley, BC V2Y 1N2
jerry@caliberprojects.com

VIA EMAIL

Dear Jerry Pol,

RE: Application Review
Project Address: 1884, 1904, 1912, 1920, & 1930 Harbour Street, 1911, 1893, & 1887 Prince Street & 1155 Pitt River Road

We are pleased to advise that the City has completed its review of the rezoning and development permit applications received April 13, 2023 for a proposed residential townhouse (RTh3) development at 1884, 1904, 1912, 1920, & 1930 Harbour Street, 1911, 1893, & 1887 Prince Street & 1155 Pitt River Road. The applications have been reviewed to determine how the proposal complies to the policies of the Official Community Plan (OCP), OCP development permit area (DPA) objectives and guidelines and the zoning, parking and other city bylaws. Our detailed comments and requests for further information and revision are noted below.

Planning Division Comments (rezoning stage)

- 1. Proposed Use:** The OCP designates the site as Residential Townhouse which can allow for City Council consideration of rezoning to a townhouse zone. The application requests rezoning of the properties from Residential Single Dwelling 1 (RS1) to Residential Townhouse 3 (Rth3) to permit development of a 43-unit townhouse development; this is consistent with the sites Residential Townhouse OCP land use designation.
- 2. Community Consultation:** Thank you for hosting a community information meeting July 24, 2023 and providing a summary of input received. Staff also received some public input following the information meeting. While the summary you provide is generally supportive a few comments noted concern about transportation impact, parking management, desire for inclusion of some commercial uses, view impacts,

and loss of mature trees at the site. It is likely these matters may also be raised at the public hearing. If you have any further information on these matters you may want to provide it to staff for inclusion in the staff report.

3. **Road Closure and Land Purchase:** The application indicates a desire to include the Prince Street land area in the site development. This will require City Council approval of a road closure bylaw and purchase of the Prince Street land from the City. In determining the land value, the City will hire a qualified third-party appraiser. On September 28, 2023 you confirmed Caliber Projects would bare the appraisal cost and our Corporate Office will provide quotes from appraisal firms for your review and selection shortly. You will note in the Development Engineering comments below there is also a sanitary sewer located in Prince Street. If you wish the road to be closed you will need to reroute the sanitary sewer.
4. **Transportation Impact Assessment:** The 2018 CTS Traffic Impact Assessment was prepared for the previous mixed-use development. Please provide additional information from CTS that confirms the recommendations would be consistent for the revised townhouse proposal.
5. **MOTI Preliminary Approval:** As the site is within 800m of a Provincial controlled access highway, approval of the rezoning by the Ministry of Transportation and Infrastructure (MOTI) is required. With receipt of the rezoning application we referred the application to MOTI and have received their preliminary approval. Please note the Ministry strongly recommended sound-attenuation fencing along the High 7B frontage. Staff will likely include sound attenuation fencing as a rezoning condition.
6. **Restrictive Covenant:** A restrictive covenant (CA5778882) is registered on title, please confirm if this restriction impacts the proposed development.
7. **Statutory Right of Way:** The existing SRW (Plan 81834) on 1884 Harbour Street contains a municipal sanitary sewer that runs through Prince Street to Pitt River Road. To consider release of the SRW the City requires relocation of the service. Please see comments below from Development Engineering and confirm your intentions.
8. **Corner Cut:** Note that a 5m x 5m corner cut land dedication will be required (see comments below from Development Engineering) at the corner of Harbour Street and Pitt River Road. Please update architectural, landscape, and subdivision plans to reflect the corner cut. Note that structures such as retaining walls will not be permitted in the corner cut area.
9. **Common Amenities:** Policies of the OCP encourage quality housing design and recognizes the need for social and recreational opportunities. You have proposed an outdoor amenity area near the southwest corner of the site adjacent to the Mary Hill Bypass/Pitt River Road Intersection. This location does not seem well suited for an amenity area given that an average of 74 dBA is reported in the acoustic report adjacent Mary Hill Bypass. Staff recommend locating an amenity space away from the Mary Hill Bypass towards the interior of the site. We would also like to see the amenity space redesigned/expanded to not just provide seating but also provide a quality children's play area to support families.
10. **Acoustic Consultant Report:** The 2018 Brown Strachan Associates letter you provided was prepared for the previous mixed-use development. Please provide an updated and comprehensive acoustic report that assesses the current proposed townhouse development and provides clear and prescriptive acoustic mitigation measures to address highway noise. This report should consider the sound attenuation fence

recommend by MOTI and propose clear noise mitigation measures. This report and the mitigation measures will be the basis of a legal agreement noted below.

11. Potential Rezoning Conditions: Please note staff anticipate recommending to Council the following rezoning conditions:

- a. Installation of tree protection for off and on-site retained trees;
- b. Completion of road closure and sale of Prince Street;
- c. Site consolidation and dedication of corner cuts;
- d. Completion of design and submission of fees and securities for off-site works and services;
- e. Registration of a legal agreements:
 - i. to ensure the buildings are design to implement the recommendations of an acoustic study, and
 - ii. to ensure installation of the recommended sound attenuation fence.

Please note the above rezoning conditions are subject to change as the application progresses.

12. Flood Plain: Please note the southern corner of the site is located within the flood plain boundary. The habitable floor elevation for the site is 5.15m geodetic and the zoning bylaw restricts construction below the habitable floor elevation.

13. Zoning Regulations: Please confirm/clarify the following:

- a. Lot Area: Please update the lot area calculation to reflect the consolidated (or net) site area. This should reflect the final consolidated site with the addition of Prince Street and any required corner cuts or road dedication subtracted.
- b. Density of Development: Please ensure the density of development calculation reflects the consolidated lot area.
- c. Open Space: The zoning bylaw requires a minimum of 30m² of usable open space per townhouse dwelling unit. This typically includes semi-private attached yards, usable balconies and rooftop outdoor space. Please include a table confirming the open space per each dwelling unit in the project data.
- d. Height: You have proposed buildings with a flat roof. In the case of a flat roof the zoning bylaw defines height as the vertical distance from the finished grade adjacent to a building, structure, fence or wall to the highest point of a roof surface. The bylaw further clarifies stair towers may be excluded from the height calculation.
 - i. Please ensure the finished grade elevation is shown for each building corner.
 - ii. Please ensure the section drawings for each building indicate the height for each building measured from the average finished grade to the highest point of the flat roof.
- e. Setbacks/Projections/Variances:
 - i. Please note that for this site the yards and associated building setbacks are as follows:
 - Front yard (north/Harbour Street) – 7.5m min.
 - Exterior side yard (west/Pitt River Road) – 3.5m min.
 - Interior side yard (east/1872 Harbour Street) – 1.8m or 3.5m to habitable space with windows.
 - Rear yard (south/Mary Hill Bypass) – 7.5m min.

**Section C — Recommended Levels of
 Traffic Noise**

The acceptance of noise depends on both the characteristics of that noise and the activities of the listeners. The activities most affected by noise fall into two categories, corresponding to two different criteria. For activities similar to speech communication (including listening to radio and television), the first requirement is that the noise level does not interfere significantly with comfortable speech communication or with listening to soft music. The other important category is sleeping: noise, especially at night, should not interfere with normal sleep patterns.

To deal with the fluctuating noise level from road or rail traffic, it is convenient to describe it in terms of the equivalent level (L_{eq}). This is the level of a steady sound having the same energy, at a given time, as the fluctuating sound. For the purposes of this document, the A-weighted 24-hour equivalent sound level is used as the basic noise descriptor. This noise measure has been extensively tested in numerous social surveys. Of the commonly used noise descriptors, it is among the easiest to measure or to predict accurately, and no other descriptor has been shown to provide a significantly better prediction of the community response to noise.

Hereafter "noise level" expressed in decibels (dB) should be taken to mean the A-weighted 24-hour equivalent sound level.

The maximum equivalent level that will not impair sustained conversational speech is 40 dB. Noise above this level causes people to raise their voices and therefore is not acceptable for a quiet indoor environment. In order to hear quieter passages of music, a level of about 35 dB would be preferred. Communication in a slightly raised voice is acceptable in kitchens and bathrooms and usually in outdoor recreation areas.

Sleep arousal and interference with going to sleep depend on the level of noise and on the fluctuations in level or character that occur. A useful criterion is that the maximum levels should not exceed the indoor background level by more than about 5 dB. Quiet interior levels range from 25 to 35 dB. Normally night-time traffic is less than day-time traffic and the 24-hour average level provides a fair measure of maximum night-time levels. The maximum level acceptable in bedrooms is 35 dB.

Outdoor noise levels should be considered as well as indoor because residential areas ought to include some space for outdoor recreation, such as patios, balconies and play areas. Experience indicates that somewhat higher noise levels are generally more acceptable outside than inside. An appropriate outdoor noise level is 55 dB, which would correspond typically to an indoor level of 40 dB. These levels would permit conversation at close range or in a slightly raised voice most of the time. Such background noise may serve the purpose of masking more specific sounds, such as conversation on a neighbour's patio.

To meet these various criteria of acceptable noise levels, the levels given in Table 1 are recommended:

Table 1

Maximum acceptable levels of road and rail traffic noise in dwellings and in outdoor recreation areas.

Room	Noise Level
Bedrooms.....	35 dB
Living, dining, recreation rooms.....	40 dB
Kitchens, bathrooms, hallways, utility rooms.....	45 dB
Outdoor recreation area.....	55 dB

ACOUSTICAL EVALUATION REPORTS - BACKGROUND INFORMATION

Development applications for housing, hotels, childcare facilities and institutional projects often include a requirement to demonstrate that the proposed construction satisfies acoustical design criteria set by the municipality. The criteria are generally defined by By-laws, Guidelines or Restrictive Covenants, e.g. OCPs, CMHC, NRC, BS, HUD, etc. On the basis that subjective reaction to noise varies significantly, full disclosure should be made to prospective residents that the building is subject to noise and vibration which may be annoying to some individuals, as outlined below.

Municipal design criteria are inside noise levels based on an Leq acoustical analysis. Monitored site measurements are used to check the analysis and site conditions. BSA Acoustical Evaluation reports (reports) recommend facade upgrades necessary to satisfy the design criteria for sources such as future road traffic, rail and aircraft noise. An outline of the design process to determine the effect of design revisions is documented in the reports. To satisfy the criteria, an analysis of third octave acoustical data from a representative ASTM E90 test of the proposed fenestration is required (not an OITC rating only). Aircraft and train noise generally require a higher OITC rated facade than traffic. Increased glazing in a given room generally results in a higher OITC requirement. Fenestration performance can vary significantly between similar products. Unless significant design margin exists, a generic description of proposed fenestration is not adequate to demonstrate compliance with the design criteria.

Where commercial and industrial sources are a primary consideration, such sources are evaluated based on the maximum levels allowed under local By-laws unless measurements on site and/or other operational data indicate lower noise levels. Construction sources are generally covered under local By-laws and are not considered representative of the future noise environment.

The analysis appended to a report does not consider potential noise issues other than as described above, e.g. unusual traffic, rail or aircraft conditions, peak sound transmission from individual vehicles, ground or airborne transmitted vibration, changes to existing infrastructure other than as indicated on drawings evaluated, emergency signals, construction or maintenance related activity, public utilities noise, privacy between suites (e.g. Code 5.8 & 9.11), isolation of suites from CRUs, offices, common or amenity areas, music rooms, fitness rooms, pools, water features, parkades, garbage, recycling, building services such as HVAC or plumbing systems, elevators, mechanical or electrical equipment, terraces or balconies, subjective reaction, non-acoustical items (e.g. failure of facade or glazing from any cause, infiltration of precipitation, condensation, mould, mildew or other fungus), etc. BSA does not undertake unmonitored 24 hour measurements as a design basis because of the risk of design deficiencies introduced by unusual conditions such as traffic diversions, extraneous sources, etc.

BSA reports and related correspondence are supporting documents for registered professionals, as defined in the Code (BCBC Div. C, 2.3.1.2, 2018). A report and related documentation such as review of window and door shop drawings, fenestration acoustical data, covenants, disclosure statements, etc., are not a BSA certification of on-site noise levels or any aspect of the construction details. BSA does not undertake the responsibility of the Architect, Coordinating Registered Professional, Registered Professional of Record or Building Envelope Professional. BSA does not provide Schedule B or C services, etc., or field review services. Other professionals should be retained for overall project co-ordination, field review, Code related advice, co-ordination of BSA's recommendations with contractors, trades, suppliers, etc.



Utility Rate Relief – 2471 Gately Avenue

RECOMMENDATION:

That Council waive the Water utility and Sewer utility charges for 2471 Gately Avenue until the earlier of:

- a) December 21, 2025; or*
- b) Occupancy Permit Issuance by the City.*

PREVIOUS COUNCIL/COMMITTEE ACTION

At a Committee meeting held on April 1, 2025, the following motion was passed:

That Committee of Council:

- 1. recommend to Council that the Water utility and Sewer utility charges for 2471 Gately Avenue be waived until the earlier of:
 - a. December 31, 2025; or*
 - b. Occupancy Permit issuance by the City; and**
- 2. authorize the release of this resolution to the public.*

BACKGROUND

Westminster Junction is an affordable housing project being developed at Gately Avenue and Kingsway Avenue by Affordable Housing Societies. Section 11 of the City's *Waterworks Regulation Bylaw, 2016, No. 3935*, and Section 3 of the City's *Sewer Regulation Bylaw, 2016, No. 3936*, provide the established timelines for Water and Sewer utility billing to new build construction. The timeline for this project is to begin Water and Sewer utility charges 545 days from Building Permit issuance.

The project was issued its Building Permit on November 24, 2022, and is now eligible for Water and Sewer utility charges as per the above-noted bylaws for 2025. A formal request to defer the start of utility charges was received by the City in March 2025 as occupancy is expected in November 2025 and due to the nature of the housing project.

OPTIONS (✓ = Staff Recommendation)

	#	Description
<input checked="" type="checkbox"/>	1	That Council waive the Water utility and Sewer utility charges for 2025.
<input type="checkbox"/>	2	That Council provide alternate direction, including an extension for utility payment until April 30, 2025, if required.

Utility Rate Relief – 2442/2444/2446 Wilson Avenue

RECOMMENDATION:

That Council:

- 1. waive the Water utility and Sewer Utility charges for units in 2442, 2444, and 2446 Wilson Avenue, which are uninhabitable due to a structure fire; and*
- 2. the term of the waiver be from January 1, 2025, until the existing buildings are demolished.*

PREVIOUS COUNCIL/COMMITTEE ACTION

At a Committee meeting held on April 1, 2025, the following motion was passed:

That Committee of Council:

- 1. recommend to Council that the Water utility and Sewer utility charges be waived for units in 2442, 2444, and 2446 Wilson Avenue, which are uninhabitable due to a structure fire;*
- 2. recommend to Council that the term of the waiver be from January 1, 2025, until the existing buildings are demolished; and*
- 3. authorize the release of this resolution to the public.*

BACKGROUND

A structure fire occurred on June 18, 2024, affecting buildings 2442, 2444, and 2446 on Wilson Avenue. A total of 30 units had significant damage and have since been deemed uninhabitable. The enabling City Bylaws for municipal Water and Sewer services include sections requiring the imposition of utility fees on the property regardless of occupancy.

This is generally an acceptable requirement for these city-provided services and has served the City well in the past. However, in the circumstance where the building becomes uninhabitable, through structure fire or other natural disaster, there does not appear to be a provision to waive these fees without the expressed authorization of the Council.

OPTIONS (✓ = Staff Recommendation)

	#	Description
✓	1	That Council waive the Water utility and Sewer utility charges for 2442, 2444 and 2446 Wilson Avenue.
	2	That Council provide alternate direction, including an extension for utility payment until April 30, 2025, if required.